



“The City with a Heart”

Rico E. Medina, Mayor
Irene O’Connell, Vice Mayor
Laura Davis, Councilmember
Marty Medina, Councilmember
Michael Salazar, Councilmember

AGENDA

SAN BRUNO CITY COUNCIL

SPECIAL MEETING

September 24, 2019

5:00 p.m.

Meeting Location: San Bruno Senior Center, 1555 Crystal Springs Road, San Bruno, CA

City Council meetings are conducted in accordance with Roberts Rules of Order Newly Revised and City Council Rules of Procedure. All regular Council meetings are recorded and televised on CATV Channel 1 and replayed the following Thursday, at 2:00 pm. In compliance with the Americans with Disabilities Act, individuals requiring reasonable accommodations or appropriate alternative formats for notices, agendas and records for this meeting should notify us 48 hours prior to meeting. Please call the City Clerk’s Office 650-616-7061, or email your request to Melissa Thurman, City Clerk at mthurman@sanbruno.ca.gov.

1. CALL TO ORDER

2. ROLL CALL

3. PUBLIC COMMENT ON ITEMS NOT ON AGENDA

Individuals allowed three minutes, groups in attendance, five minutes. It is the Council’s policy to refer matters raised in this forum to staff for investigation and/or action where appropriate. The Brown Act prohibits the Council from discussing or acting upon any matter not agendized pursuant to State Law.

4. CLOSED SESSION:

- a. Conference with Labor Negotiators Pursuant to Gov’t Code Section 54957.6
Agency Designated Representatives: City Manager and Assistant City Manager Employee
Organizations: San Bruno Management Employee Association, Mid Management Unit, Public Safety
Mid Management Unit and Miscellaneous Unit.
- b. Conference with Legal Counsel – Anticipated Litigation
Significant Exposure to Litigation Pursuant to Government Code Section 54956.9(d)(2) - Four Cases

5. ADJOURNMENT

The next Regular City Council Meeting will be held on September 24, 2019 at 7:00 p.m. at the Senior Center, 1555 Crystal Springs Road, San Bruno.



“The City with a Heart”

Rico E. Medina, Mayor
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AGENDA

SAN BRUNO CITY COUNCIL

September 24, 2019

7:00 p.m.

Meeting Location: San Bruno Senior Center, 1555 Crystal Springs Road, San Bruno, CA

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Thanks to the San Bruno Garden Club for providing the floral arrangement.

1. CALL TO ORDER

2. ROLL CALL/PLEDGE OF ALLEGIANCE

3. PUBLIC COMMENT ON ITEMS NOT ON AGENDA:

Individuals allowed three minutes, groups in attendance, five minutes. It is the Council's policy to refer matters raised in this forum to staff for investigation and/or action where appropriate. The Brown Act prohibits the Council from discussing or acting upon any matter not agendized pursuant to State Law.

4. ANNOUNCEMENTS/PRESENTATIONS:

- a. The Community Services Department has several upcoming special events. The last Concert at the Rotary Pavilion in San Bruno City Park will be on Friday, September 27, 2019 at 6:00 p.m. The concert will be followed by the Family Overnight in the Park showing the free Movie in the Park, *Ralph Breaks the Internet*. For more information about these events please visit the City of San Bruno calendar, www.sanbruno.ca.gov/Calendar.
- b. The Friends of the San Bruno Library will hold their semi-annual book sale on Saturday, October 5, 2019 at 10:00 a.m.-3:00 p.m. in the Library’s downstairs community room. Thousands of books will be available for sale. Bring your own bag and fill it with books for just \$7. All proceeds benefit the Library.
- c. The League of Women Voters will host a Candidate Forum at the San Bruno Senior Center on Tuesday October 1, 2019, from 7:00-9:00 p.m.
- d. Present Proclamation declaring October 6 – 12, 2019 as National Fire Prevention Week in San Bruno.
- e. Receive Annual Presentation from the Senior Citizens Advisory Board.
- f. Receive Presentation from the 2019 San Bruno Sister City Exchange Delegation on their Visit to Narita, Japan in July 2019.

5. CONSENT CALENDAR:

All items are considered routine or implement an earlier Council action and may be enacted by one motion; there will be no separate discussion, unless requested.

- a. **Accept** Accounts Payable of September 3 and September 9, 2019.
- b. **Accept** Payroll of September 1, 2019.

- c. **Accept** Reconciliation of General Ledger to Bank Reports and Investment Reports Dated July 31, 2019
- d. **Approve** Draft Meeting Minutes for the Special and Regular Meetings of September 10, 2019.
- e. **Adopt** a Resolution Authorizing the Receipt and Expenditure of \$81,000 from the California Office of Traffic Safety Pursuant to the Federal Fiscal Year 2019-20 Selective Traffic Enforcement Program (STEP) Grant and Appropriating \$4,500 of Grant Revenue and Expenditure into the Federal/State Grants Fund.
- f. **Adopt** Resolution to Authorize Replacing the Fire Department Secretary Position with an Executive Assistant in the Fiscal Year 2019-20 Budget.
- g. **Adopt** Resolution Denying and Revoking Cardroom Work Permit

6. PUBLIC HEARING:

- a. **Hold** Public Hearing, Adopt a Resolution Adjusting the Affordable Housing Impact Fees for Residential and Nonresidential Development Projects.

7. CONDUCT OF BUSINESS:

- a. **Authorize** Modifications to the Florida Avenue Park Master Plan

8. STUDY SESSION:

- a. **Receive** Report on the San Mateo Avenue Conceptual Streetscape Plan, and Provide Direction to City Staff

9. COMMENTS FROM COUNCIL MEMBERS:

10. ADJOURNMENT:

The next Regular City Council Meeting will be held on October 8, 2019 at 7:00 p.m. at the Senior Center, 1555 Crystal Springs Road, San Bruno.



City Council Agenda Item
Staff Report

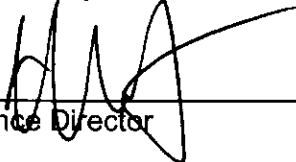
CITY OF SAN BRUNO

DATE: September 3, 2019
TO: Honorable Mayor and Members of the City Council
FROM: Jovan D. Grogan, City Manager
PREPARED BY: Keith DeMartini, Finance Director
Kathleen O'Malley, Accounting & Customer Service Representative
SUBJECT: Accounts Payable Warrant Register

This is to certify that the claims listed on pages 1 to 3 inclusive, and/or claims numbered from 183232 through 183389 inclusive, totaling \$233,030.81 have been checked in detail and approved by the proper officials, and in my opinion, represent fair and just charges against the City in accordance with their respective amounts. The table below summarizes the total paid by Fund.

Fund	Fund Name	Amount
001	General Fund	\$120,643.97
003	One-Time Revenue	\$2,903.52
111	Police Asset Forfeiture	\$1,283.47
122	Solid Waste/Recycle	\$866.98
132	Agency on Aging	\$932.45
133	Restricted Revenues	\$461.04
190	Disaster Recovery Fund	\$725.50
201	Parks and Facilities Capital	\$5,694.75
611	Water Fund	15,069.25
621	Stormwater Fund	1,289.49
631	Wastewater Fund	9,736.44
641	Cable TV Fund	42,141.04
701	Central Garage	9,976.02
702	Facility Maintenance Fund	7,633.17
707	Technology Development	12,773.72
880	Project Development Trust	900.00
TOTAL FOR APPROVAL		\$233,030.81

Respectfully submitted,



Finance Director

9/9/19.

Date

Document group: komalley Bank: apbank 432000438

Vendor Code & Name	Check #	Check Date	Amount
0107236 24/7 ROOTER & PLUMBING	183232	9/3/2019	1,000.00
0108580 ABHINAV MATHUR	183320	9/3/2019	29.33
0104680 ACCESS 24 COMMUNICATIONS INC.	183233	9/3/2019	502.10
0017053 ACCOUNTEMPS	183234	9/3/2019	4,956.25
0000858 ADECCO EMPLOYMENT SERVICES	183235	9/3/2019	3,701.20
0108563 ADRIANNA VERGARA	183381	9/3/2019	68.60
0017586 AIR EXCHANGE, INC	183237	9/3/2019	594.27
0000163 AIRPORT AUTO PARTS INC.	183238	9/3/2019	1,087.41
0108565 ALAN CHEN	183261	9/3/2019	28.38
0108532 ALEJANDRO AGUILAR	183236	9/3/2019	276.00
0102355 AMAZON	183240	9/3/2019	723.96
0098508 AMERICAN PUBLIC WORKS ASSOCIATION(APWA)	183346	9/3/2019	1,368.50
0108555 ANDREA DELGADO	183273	9/3/2019	88.95
0001202 ARAMARK UNIFORM SERVICES	183241	9/3/2019	844.89
0106185 ARIS KORON	183311	9/3/2019	395.00
0016123 AT&T	183242	9/3/2019	462.55
0017191 AT&T	183243	9/3/2019	55.89
0018363 AT&T LONG DISTANCE	183244	9/3/2019	21.86
0105649 ATLAS PLUMBING AND ROOTER	183245	9/3/2019	15,000.00
0000345 BAKER & TAYLOR BOOKS	183246	9/3/2019	2,946.26
0018688 BEST BEST & KRIEGER LLP	183248	9/3/2019	3,818.00
0017361 BOETHING TREELAND FARMS, INC.	183249	9/3/2019	461.04
0108566 BRUCE TREVITHICK	183375	9/3/2019	41.90
0100237 BULLSEYE PLUMBING	183251	9/3/2019	3,500.00
0014739 CAL-STEAM	183253	9/3/2019	32.56
0108554 CANADA COLLEGE	183254	9/3/2019	19.95
0017679 CDW GOVERNMENT, INC	183258	9/3/2019	6,244.70
0108571 CHARLES CLIFFORD	183266	9/3/2019	1,000.00
0017284 CHEMSEARCHFE	183260	9/3/2019	2,125.29
0103854 CHRISTINE HOPKINS	183296	9/3/2019	240.50
0016324 CINTAS CORPORATION #464	183262	9/3/2019	943.79
0098588 CITY OF BURLINGAME	183263	9/3/2019	536.40
0093561 CITY OF PACIFICA	183264	9/3/2019	50.00
0000508 CLEARLITE TROPHIES	183265	9/3/2019	88.34
0105187 CONCERN	183268	9/3/2019	702.66
0108543 CONSTANTINO PEREZ	183340	9/3/2019	150.00
0095644 CORTEZ BROTHERS CONCRETE CONSTRUCTION	183250	9/3/2019	552.00
0015857 COUNTY OF SAN MATEO	183269	9/3/2019	76.00
0002012 CPS HR CONSULTING	183270	9/3/2019	1,224.30
0106830 CRESTLINE SPECIALTIES INC.	183271	9/3/2019	1,283.47
0097934 CWEA-TCP	183272	9/3/2019	384.00
0108576 DAVID ELMATARI	183276	9/3/2019	29.33
0018673 DENNIS BOSCH	183274	9/3/2019	24.00
0101178 DISCOUNT PLUMBING	183275	9/3/2019	3,000.00
0108564 EMILY MORTAZAVI	183322	9/3/2019	37.84
0000073 ENGINEERING DATA SOFTWARE, INC	183278	9/3/2019	1,173.24
0108411 EURO SCHOOL OF TENNIS	183279	9/3/2019	3,006.25
0108574 EZEQUIEL VALENCIA	183379	9/3/2019	15.14
0102627 FASTRK VIOLATION PROCESSING DEPT.	183383	9/3/2019	31.00
0000944 FEDEX	183280	9/3/2019	43.04
0018117 FLYERS ENERGY, LLC	183282	9/3/2019	20,799.82
0096236 FOLGER GRAPHICS, INC.	183283	9/3/2019	5,683.04
0018272 GALE/CENGAGE LEARNING	183286	9/3/2019	34.08

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Vendor Code & Name	Check #	Check Date	Amount
0108557 GARY FLEMING	183281	9/3/2019	95.65
0108546 GEORGE READE	183347	9/3/2019	250.00
0017454 GOLDEN STATE FLOW MEASUREMENT	183289	9/3/2019	8,202.47
0000162 GRAINGER	183290	9/3/2019	1,217.18
0095966 GREAT AMERICA FINANCIAL SVC.	183291	9/3/2019	642.06
0017900 GREAT LAKES DATA SYSTEMS INC	183292	9/3/2019	2,100.00
0108579 GUIMING LIU	183317	9/3/2019	5.79
0108556 HENRY AMABLE	183239	9/3/2019	89.20
0105378 HOME MAID RAVIOLI COMPANY INC.	183295	9/3/2019	554.20
0096344 HUNT & SONS, INC.	183297	9/3/2019	109.21
0018838 INFOSEND, INC.	183298	9/3/2019	6,849.43
0108449 INNERWORKINGS, INC.	183299	9/3/2019	4,510.93
0105931 INNOTRANS	183300	9/3/2019	1,799.35
0108550 INTERFRESH, INC.	183301	9/3/2019	240.90
0018125 INTERNATIONAL CODE COUNCIL-ICC	183302	9/3/2019	2,192.65
0015531 INTERSTATE BATTERY SYS. OF SF	183303	9/3/2019	129.02
0099054 INTERSTATE TRS FUND	183304	9/3/2019	1,830.29
0108577 JASEN WALKER	183384	9/3/2019	85.14
0108578 JOEY NUTTER	183329	9/3/2019	26.49
0108562 JONATHAN PELTZ	183336	9/3/2019	66.65
0108551 JOY CANEDA	183255	9/3/2019	58.00
0106683 JUNIOR CHEF STARS	183306	9/3/2019	1,950.00
0000075 K-119 TOOLS OF CALIFORNIA INC.	183307	9/3/2019	119.52
0000132 KELLY-MOORE PAINT CO INC.	183309	9/3/2019	223.46
0108560 KERSTIN SJOSTROM	183360	9/3/2019	55.93
0108552 KEVIN LAU	183313	9/3/2019	64.95
0101866 KIDZ LOVE SOCCER	183310	9/3/2019	4,282.20
0105752 LEVEL 3 COMMUNICATIONS, LLC	183315	9/3/2019	10,578.00
0093274 LINDSTROM CO	183316	9/3/2019	2,000.00
0017924 LORAL LANDSCAPING INC.	183318	9/3/2019	4,735.00
0108587 LUCIA DELA PENA	183337	9/3/2019	400.00
0017026 LYNX TECHNOLOGIES, INC.	183319	9/3/2019	1,170.00
0090000 MARC CATALANO	183257	9/3/2019	373.76
0108547 MARIA HERNANDEZ	183294	9/3/2019	500.00
0097937 MARK BUNNELL	183252	9/3/2019	500.00
0108575 MARK WILLIAMS	183387	9/3/2019	64.55
0108573 MARY GALEA	183287	9/3/2019	40.07
0107928 MELISSA THURMAN	183373	9/3/2019	396.21
0100062 MIRELLA RAMIREZ	183321	9/3/2019	1,823.57
0000357 NATIONAL CABLE TV CO-OP, INC.	183323	9/3/2019	2,798.48
0018764 NATIONAL LEARNING CORP.	183324	9/3/2019	40.95
0095141 NFPA	183325	9/3/2019	729.61
0108545 NGUYEN MYPHUONG THI	183326	9/3/2019	283.00
0103301 NHL NETWORK US, LP	183327	9/3/2019	1,529.30
0105238 NORTHERN SERVICES INC.	183328	9/3/2019	797.19
0092263 OFFICE DEPOT INC	183330	9/3/2019	934.09
0097567 ONE HOUR DRY CLEANING	183331	9/3/2019	958.00
0018701 ORKIN PEST CONTROL	183332	9/3/2019	869.86
0103933 OWEN EQUIPMENT SALES	183333	9/3/2019	1,003.84
0104658 PACIFIC ACCESS	183334	9/3/2019	450.00
0000012 PACIFIC GAS & ELECTRIC	183335	9/3/2019	2,892.21
0108553 PATRICK GAGAN	183285	9/3/2019	121.55
0001154 PENINSULA LIBRARY SYSTEM	183338	9/3/2019	1,314.90
0106269 PENINSULA PET RESORT INC.	183339	9/3/2019	374.40

Document group: komalley Bank: apbank 432000438

Vendor Code & Name	Check #	Check Date	Amount	
0015163	PENINSULA SPORTS OFFICIALS ASSOC.INC.	183362	9/3/2019	936.00
0017260	PETERSON TRUCKS, INC.	183341	9/3/2019	120.11
0096538	PLAY-WELL TEKNOLOGIES	183342	9/3/2019	873.60
0016770	PRAXAIR DISTRIBUTION INC -192	183343	9/3/2019	145.66
0102915	PRECISE PRINTING & MAILING	183344	9/3/2019	748.24
0108125	PREMIUM HEALTH INSURANCE SERVICES, LLC	183293	9/3/2019	13.00
0098436	PROFESSIONAL LAND SERVICES	183345	9/3/2019	1,625.50
0107432	RED WING BRANDS OF AMERICA,INC	183348	9/3/2019	294.96
0108559	RINO BENENATI	183247	9/3/2019	62.74
0017987	RISO, INC.	183349	9/3/2019	399.30
0108548	ROBERT GEONZON	183288	9/3/2019	250.00
0016213	ROZZI REPRODUCTION&SUPPLY INC.	183350	9/3/2019	428.03
0108567	RYAN JONES	183305	9/3/2019	33.52
0018597	SAN MATEO DAILY JOURNAL	183352	9/3/2019	660.00
0018461	SERRAMONTE FORD, INC.	183353	9/3/2019	2,772.75
0106354	SEWER RAT PLUMBING, INC.	183354	9/3/2019	2,100.00
0097626	SHARP ELECTRONICS CORP.	183355	9/3/2019	1,067.32
0108561	SHERRIE CARNECER	183256	9/3/2019	45.42
0018962	SHOE DEPOT INC.	183356	9/3/2019	58.97
0098030	SHRED-IT USA	183357	9/3/2019	225.84
0001225	SIERRA PACIFIC TURF SUPPLY,INC	183358	9/3/2019	6.38
0108542	SILICON VALLEY COMFORT	183359	9/3/2019	1,573.56
0108268	SOLUTIONS II, INC.	183361	9/3/2019	11,687.84
0000102	SONITROL/PACIFIC WEST SECURITY, INC.	183386	9/3/2019	133.20
0108558	SOPHIE LAGACE	183312	9/3/2019	26.49
0097079	SPRINT	183363	9/3/2019	242.94
0016831	STAPLES CREDIT PLAN	183364	9/3/2019	293.72
0106866	STEPHEN KADASH	183308	9/3/2019	255.00
0105796	SUNRISE FOOD DISTRIBUTOR INC.	183365	9/3/2019	378.25
0102962	SWANK MOTION PICTURES, INC.	183366	9/3/2019	423.00
0107708	TELCORDIA TECHNOLOGIES	183367	9/3/2019	37.87
0002025	TELECOMMUNICATIONS ENGINEERING ASSOCIATE	183277	9/3/2019	2,923.00
0107098	TELSTAR INSTRUMENTS	183368	9/3/2019	1,137.66
0096616	TENNANT SALES AND SERVICE CO.	183369	9/3/2019	1,983.99
0000241	THE ADAM-HILL COMPANY	183370	9/3/2019	26.08
0018083	THE CROSSING SAN BRUNO PROPERTY OWNERS /	183351	9/3/2019	2,903.52
0108402	THE HOME DEPOT PRO	183371	9/3/2019	4,085.20
0103559	THE MLB NETWORK, LLC	183372	9/3/2019	1,678.50
0106116	TIAA COMMERCIAL FINANCE, INC.	183267	9/3/2019	395.47
0018493	TOWN OF PORTOLA VALLEY	183374	9/3/2019	120.00
0017133	TURBO DATA SYSTEMS INC	183376	9/3/2019	874.00
0103736	TURF STAR, INC.	183377	9/3/2019	196.84
0018618	UNITED SITE SERVICES INC.	183378	9/3/2019	194.05
0018248	VALUE LINE PUBLISHING INC.	183380	9/3/2019	1,205.02
0095749	VERIZON WIRELESS	183382	9/3/2019	4,643.90
0097611	WALTER LEE	183314	9/3/2019	20.97
0108541	WARDELL AUTO INTERIORS & TOPS	183385	9/3/2019	381.55
0017611	WEI-LUN CHANG	183259	9/3/2019	61.90
0093259	WILLIAM FORESTER	183284	9/3/2019	650.00
0104022	WORLD JOURNAL	183388	9/3/2019	650.00
0104033	ZCORUM, INC.	183389	9/3/2019	14,047.66

GrandTotal: 233,030.81

Total count: 158



**City Council Agenda Item
Staff Report**

CITY OF SAN BRUNO

DATE: September 9, 2019
TO: Honorable Mayor and Members of the City Council
FROM: Jovan D. Grogan, City Manager
PREPARED BY: Keith DeMartini, Finance Director
Kathleen O'Malley, Accounting & Customer Service Representative
SUBJECT: Accounts Payable Warrant Register

This is to certify that the claims listed on pages 1 to 2 inclusive, and/or claims numbered from 183390 through 183482 inclusive, totaling \$663,388.15 have been checked in detail and approved by the proper officials, and in my opinion, represent fair and just charges against the City in accordance with their respective amounts. The table below summarizes the total paid by Fund.

Fund	Fund Name	Amount
001	General Fund	\$139,649.46
122	Solid Waste/Recycle	\$8,275.25
131	In-Lieu Fees	\$37,500.00
201	Parks and Facilities Capital	\$11,709.75
611	Water Fund	43,448.69
621	Stormwater Fund	256,695.36
631	Wastewater Fund	29,835.50
641	Cable TV Fund	50,474.15
702	Facility Maintenance Fund	21,662.84
703	General Equipment Revolving	13,130.73
707	Technology Development	7,276.85
880	Project Development Trust	43,729.57
TOTAL FOR APPROVAL		\$663,388.15

Respectfully submitted,



Finance Director

9/12/19

Date

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Vendor Code & Name	Check #	Check Date	Amount
0107878 4LEAF, INC.	183390	9/9/2019	5,844.00
0107953 ACS SUPPORT	183391	9/9/2019	484.88
0108428 AJ FIBER CONSULTANT LLC	183392	9/9/2019	12,920.00
0000372 ALLIED SECURITY ALARMS	183393	9/9/2019	213.00
0108120 ARTISTS ON WHEELS	183395	9/9/2019	140.00
0016123 AT&T	183396	9/9/2019	1,757.75
0018465 AT&T MOBILITY	183397	9/9/2019	144.69
0105649 ATLAS PLUMBING AND ROOTER	183398	9/9/2019	7,500.00
0000345 BAKER & TAYLOR BOOKS	183399	9/9/2019	2,059.23
0015988 BLACKSTONE PUBLISHING	183401	9/9/2019	102.52
0000378 BROADMOOR LANDSCAPE SUPPLY	183403	9/9/2019	203.04
0102737 BURKE, WILLIAMS & SORENSEN,LLP	183404	9/9/2019	936.00
0108182 C2R ENGINEERING, INC.	183405	9/9/2019	234,602.50
0014739 CAL-STEAM	183406	9/9/2019	263.44
0099078 CAMINO RAMON ASSOCIATES, LLC	183407	9/9/2019	8,135.14
0105785 CELLEBRITE USA, INC.	183408	9/9/2019	3,700.00
0017843 CENTRAL COUNTY FIRE DEPT.	183409	9/9/2019	1,086.80
0106048 CIT	183411	9/9/2019	427.81
0015857 COUNTY OF SAN MATEO	183415	9/9/2019	1,449.63
0097071 CRESCO EQUIPMENT RENTALS	183416	9/9/2019	6,249.87
0018188 DAU PRODUCTS	183417	9/9/2019	1,340.18
0000197 DEMCO SUPPLY INC.	183418	9/9/2019	145.29
0101178 DISCOUNT PLUMBING	183419	9/9/2019	3,100.00
0108585 EMILY CHENG	183410	9/9/2019	320.00
0017152 ERLER & KALINOWSKI, INC.	183421	9/9/2019	22,658.22
0001782 FLOWERS ELECTRIC & SVC.CO.INC.	183424	9/9/2019	3,301.36
0017720 FOX TELEVISION STATIONS, INC.	183425	9/9/2019	13,468.91
0108475 FRACTURED HANS	183426	9/9/2019	605.00
0107011 FRANK J. BIANCHI	183400	9/9/2019	218.40
0104135 GLOBAL TRACKING COMMUNICATIONS, INC.	183475	9/9/2019	24.99
0000162 GRAINGER	183427	9/9/2019	10,738.00
0000385 HACH COMPANY	183428	9/9/2019	4,880.56
0091742 HIP HOUSING, INC.	183429	9/9/2019	30,000.00
0106864 INSIGHT PUBLIC SECTOR, INC.	183430	9/9/2019	10,773.75
0105884 JACKSON LEWIS P.C.	183431	9/9/2019	2,368.00
0016347 JEFFREY MADONICH	183439	9/9/2019	707.85
0000075 K-119 TOOLS OF CALIFORNIA INC.	183432	9/9/2019	162.37
0018050 KAISER FOUNDATION HEALTH PLAN	183433	9/9/2019	5,059.05
0096379 KAREN OJAKIAN	183452	9/9/2019	405.60
0000317 L.N. CURTIS & SONS	183434	9/9/2019	2,644.95
0018561 LANCE BAYER	183435	9/9/2019	375.00
0108010 LAURA FABBRI	183423	9/9/2019	400.00
0106460 LIFEMOVES	183436	9/9/2019	7,500.00
0017026 LYNX TECHNOLOGIES, INC.	183438	9/9/2019	6,110.00
0103231 MAINTSTAR INC.	183440	9/9/2019	47,245.36
0104916 MANDELL MUNICIPAL COUNSELING	183441	9/9/2019	928.00
0097081 MARYMOUNT PLACE, LLC	183442	9/9/2019	7,981.13
0102770 METLIFE	183443	9/9/2019	152.00
0018783 MICHAEL V FERRETTI	183444	9/9/2019	790.00
0016863 MIDWEST TAPE, LLC	183445	9/9/2019	43.05
0098530 MISAC	183446	9/9/2019	130.00
0000333 MOSS RUBBER & EQUIP. CORP.	183447	9/9/2019	473.09
0108583 NANCY ELLIS	183420	9/9/2019	50.00

Document group: komalley Bank: apbank 432000438

Vendor Code & Name	Check #	Check Date	Amount
0106753 NEXSTAR BROADCASTING, INC.	183448	9/9/2019	10,559.25
0105238 NORTHERN SERVICES INC.	183449	9/9/2019	17,616.64
0092263 OFFICE DEPOT INC	183451	9/9/2019	547.76
0016818 OUTDOOR CREATIONS INC	183453	9/9/2019	8,275.25
0000012 PACIFIC GAS & ELECTRIC	183454	9/9/2019	36,639.00
0106110 PACIFIC OFFICE AUTOMATION	183455	9/9/2019	1,273.57
0106156 PENGUIN RANDOM HOUSE LLC	183456	9/9/2019	40.97
0001154 PENINSULA LIBRARY SYSTEM	183457	9/9/2019	79,565.14
0106229 PET FOOD EXPRESS	183458	9/9/2019	41.15
0107711 PLATINUM CONSULTING AND INVESTIGATIONS	183414	9/9/2019	3,000.00
0097558 PURCHASE POWER	183460	9/9/2019	37.94
0108582 RAINEY CONSTRUCTION	183461	9/9/2019	2,500.00
0108584 REGINA AND MARTIN ROMERO	183463	9/9/2019	50.00
0103531 RICOH USA, INC.	183462	9/9/2019	324.11
0105690 ROBERT BLAIR	183402	9/9/2019	100.00
0000022 ROBERT LOUIE	183437	9/9/2019	219.70
0108581 SAGE VETERINARY CENTERS	183464	9/9/2019	670.02
0106393 SANDY ALVAREZ	183394	9/9/2019	1,932.90
0001225 SIERRA PACIFIC TURF SUPPLY, INC	183466	9/9/2019	2,743.06
0104548 SLOAN SAKAI YEUNG & WONG LLP	183467	9/9/2019	693.00
0093543 SMPCSA	183468	9/9/2019	158.08
0108268 SOLUTIONS II, INC.	183469	9/9/2019	3,246.85
0097079 SPRINT	183470	9/9/2019	202.61
0102991 STEVE SALAZAR	183465	9/9/2019	290.43
0108568 STUTZMAN, BROMBERG ESSERMAN & PLIFKA, A PRC	183422	9/9/2019	1,473.50
0017928 THE EDCCO GROUP, INC.	183471	9/9/2019	700.00
0108402 THE HOME DEPOT PRO	183472	9/9/2019	3,105.93
0018275 THE REGENTS OF THE UNIVERSITY OF CA	183450	9/9/2019	249.33
0000036 THOMSON REUTERS	183473	9/9/2019	204.30
0097449 THYSSENKRUPP ELEVATOR CORP.	183474	9/9/2019	461.04
0106116 TIAA COMMERCIAL FINANCE, INC.	183413	9/9/2019	1,182.52
0108586 TIMOTEO POLICARPIO	183459	9/9/2019	400.00
0106660 TRIDENT K9 CONSULTING INC.	183476	9/9/2019	375.00
0000665 TSQ SOLUTIONS INC.	183477	9/9/2019	375.00
0001362 TV GUIDE MAGAZINE, LLC	183478	9/9/2019	59.61
0106266 VIBO SIMFANI	183479	9/9/2019	600.00
0108590 WALTER LEE & BARBARA NG	183480	9/9/2019	2,450.08
0105955 WEST COAST CODE CONSULTANTS, INC.	183412	9/9/2019	812.50
0000612 WESTVALLEY CONSTRUCTION CO. INC	183481	9/9/2019	4,386.50
0108261 WOODARD & CURRAN	183482	9/9/2019	2,505.00
		GrandTotal:	663,388.15
		Total count:	93



City Council Agenda Item
Staff Report


CITY OF SAN BRUNO

DATE: September 16, 2019
TO: Honorable Mayor and Members of the City Council
FROM: Jovan D. Grogan, City Manager
PREPARED BY: Keith DeMartini, Finance Director
Kathleen O'Malley, Accounting & Customer Service Representative
SUBJECT: Accounts Payable Warrant Register

This is to certify that the claims listed on pages 1 to 2 inclusive, and/or claims numbered from 183483 through 183553 inclusive, totaling \$638,659.40 have been checked in detail and approved by the proper officials, and in my opinion, represent fair and just charges against the City in accordance with their respective amounts. The table below summarizes the total paid by Fund.

Fund	Fund Name	Amount
001	General Fund	\$94,005.65
111	Police Asset Forfeiture	\$2,322.79
132	Agency On Aging	\$6,461.37
133	Restricted Revenues	\$708.06
201	Parks and Facilities Capital	\$80,689.44
203	Street Improvement Projects	\$143,086.00
611	Water Fund	1,633.82
631	Wastewater Fund	129,677.34
641	Cable TV Fund	4,385.23
701	Central Garage	49.88
702	Facility Maintenance Fund	6,451.10
703	General Equipment Revolving	2,242.13
707	Technology Development	5,738.66
711	Self-Insurance	67,793.05
880	Project Development Trust	93,414.88
TOTAL FOR APPROVAL		\$638,659.40

Respectfully submitted,



Finance Director

9/17/19.

Date

Document group: komalley Bank: apbank 432000438

Vendor Code & Name	Check #	Check Date	Amount
0096852 ABAG PLAN CORPORATION	183483	9/16/2019	5,493.90
0017053 ACCOUNTEMS	183484	9/16/2019	1,560.00
0001170 AIRGAS USA, LLC	183485	9/16/2019	55.47
0000163 AIRPORT AUTO PARTS INC.	183486	9/16/2019	132.35
0108604 ALYSON SPRAGUE	183544	9/16/2019	13.24
0106185 ARIS KORON	183519	9/16/2019	325.00
0014617 AT&T	183488	9/16/2019	14.21
0016123 AT&T	183489	9/16/2019	1,227.34
0093371 B. BURDETTE	183490	9/16/2019	34.21
0108608 BING CHENG HO	183511	9/16/2019	9.46
0102737 BURKE, WILLIAMS & SORENSEN,LLP	183492	9/16/2019	33,399.20
0106246 CATHOLIC CHARITIES	183494	9/16/2019	2,381.00
0017679 CDW GOVERNMENT, INC	183495	9/16/2019	214.51
0000227 CITY OF SAN BRUNO	183496	9/16/2019	5,549.77
0095579 CLIFFORD COUTS JR	183498	9/16/2019	58.92
0108591 CORNERSTONE PATRIOT HOLDING,LLC	183535	9/16/2019	34,588.06
0108605 DAVID BURGOS	183491	9/16/2019	12.57
0093457 DEAN YAMAGUCHI	183553	9/16/2019	16.97
0018759 DENNIS MOLLOY	183529	9/16/2019	200.00
0108569 DERENIK LEVANTIAN	183521	9/16/2019	1,000.00
0108618 DERRELL MAGDANGAL	183525	9/16/2019	31.93
0001782 FLOWERS ELECTRIC & SVC.CO.INC.	183502	9/16/2019	295.95
0018117 FLYERS ENERGY, LLC	183503	9/16/2019	10,151.03
0108611 GIL SHVARTZ	183541	9/16/2019	30.27
0108415 GOLDEN BAY CONSTRUCTION, INC.	183504	9/16/2019	135,931.70
0000541 GRANITE ROCK COMPANY	183505	9/16/2019	374.36
0095966 GREAT AMERICA FINANCIAL SVC.	183506	9/16/2019	929.21
0016967 GROUP 4	183507	9/16/2019	80,474.93
0103293 HANLON'S TIRE SERVICE	183508	9/16/2019	25.00
0105966 HMTV TV DOMINICANA LLC	183510	9/16/2019	14.94
0108603 INNA POLOZOVA	183537	9/16/2019	115.20
0104018 INTERSTATE TRAFFIC CONTROL PRODUCTS,INC.	183549	9/16/2019	245.43
0106450 IRON MOUNTAIN	183513	9/16/2019	195.18
0097927 JAMES WYSYNSKI	183552	9/16/2019	12.58
0108551 JOY CANEDA	183493	9/16/2019	64.95
0000771 JT2 INTEGRATED RESOURCES	183515	9/16/2019	26,684.00
0018376 JT2 INTEGRATED RESOURCES	183516	9/16/2019	6,531.00
0108599 JUNFEI HUANG	183512	9/16/2019	16.76
0108598 KABOOM	183517	9/16/2019	52.12
0108529 KARANJIT DHILLON	183500	9/16/2019	117.00
0108617 KATHLEEN EVANGELIO	183501	9/16/2019	54.87
0104335 LEXIPOL LLC	183522	9/16/2019	10,072.00
0105979 LIFTOFF LLC	183523	9/16/2019	2,000.00
0108614 LOK KWAN LEUNG	183520	9/16/2019	49.19
0018177 LOWE'S	183524	9/16/2019	2,780.41
0108570 MAHMOUD MOHAMED	183528	9/16/2019	1,000.00
0107892 MANAGEMENT PARTNERS	183526	9/16/2019	28,503.94
0108606 MARINA SIDORKO	183542	9/16/2019	12.57
0108613 MARK O'GORMAN	183532	9/16/2019	64.95
0103091 MARY NUNNERY	183530	9/16/2019	150.00
0018397 MICHAEL KU	183527	9/16/2019	300.00
0092263 OFFICE DEPOT INC	183531	9/16/2019	682.64
0000012 PACIFIC GAS & ELECTRIC	183533	9/16/2019	9,729.89

Document group: komalley Bank: apbank 432000438

Vendor Code & Name	Check #	Check Date	Amount
0108607 PAUL KIM	183518	9/16/2019	5.68
0016085 PENINSULA CONFLICT RESOLUTION CENTER	183497	9/16/2019	10,032.75
0000294 PITNEY BOWES	183536	9/16/2019	1,869.94
0108602 RICHARD DART	183499	9/16/2019	58.00
0108601 ROSEMARIE THOMPSON	183548	9/16/2019	7.74
0106657 RYAN HARTMAN	183509	9/16/2019	1,035.00
0099047 SAN MATEO CTY SHERIFF'S OFFICE	183539	9/16/2019	3,232.00
0106393 SANDY ALVAREZ	183487	9/16/2019	209.00
0097626 SHARP ELECTRONICS CORP.	183540	9/16/2019	127.88
0104548 SLOAN SAKAI YEUNG & WONG LLP	183543	9/16/2019	29,084.15
0107640 TANNER PACIFIC, INC.	183545	9/16/2019	128,858.83
0018073 TEAMSTERS LOCAL 350	183546	9/16/2019	2,312.00
0015691 TEAMSTERS LOCAL 856	183547	9/16/2019	12,959.00
0107727 TRI COUNTIES BANK	183550	9/16/2019	38,793.18
0000019 U.S. POSTMASTER	183551	9/16/2019	6,000.00
0108600 VASUNDHARA PANDEY	183534	9/16/2019	5.15
0108612 XIAOWEI JIANG	183514	9/16/2019	51.08
0108615 YU QI	183538	9/16/2019	37.84
		GrandTotal:	638,659.40
		Total count:	71



**City Council Agenda Item
Staff Report**

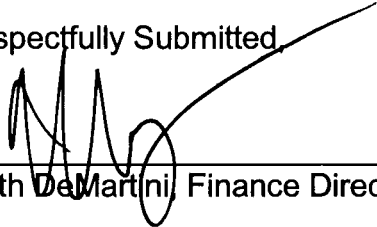
CITY OF SAN BRUNO

DATE: September 24, 2019
TO: Honorable Mayor and Members of the City Council
FROM: Jovan D. Grogan, City Manager
PREPARED BY: Benjie Lin, Payroll Specialist
SUBJECT: Payroll Acceptance

City Council acceptance of the City payroll distributed September 6, 2019 is recommended. The Labor Summary report reflecting the total payroll amount of \$1,571,827.95 for bi-weekly pay period ending September 1, 2019 by fund is shown below:

Fund	Amount
Fund: 001 - GENERAL FUND	\$1,207,410.28
Fund: 121 - FEDERAL/STATE GRANTS	6,716.71
Fund: 122 - SOLID WAIST/RECYCL.	1,759.50
Fund: 203 - STREET IMPROVE. PROJECTS	9,296.46
Fund: 611 - WATER FUND	86,313.06
Fund: 621 - STORMWATER FUND	18,545.12
Fund: 631 - WASTEWATER FUND	76,224.47
Fund: 641 - CABLE TV FUND	94,018.94
Fund: 701 - CENTRAL GARAGE	12,498.01
Fund: 702 - FACILITY MAINT.FUND	34,417.24
Fund: 707 - TECHNOLOGY DEVELOPMENT	17,516.81
Fund: 711 - SELF INSURANCE	7,111.35
Total:	\$1,571,827.95

Respectfully Submitted,



Keith DeMartini, Finance Director

9/16/19

Date

ITEM 5.b.

9/18/2019 12:40:03PM

Through period: 1

City of San Bruno
Through July 2019

	<u>Cash</u>	<u>Investments</u>	<u>Fund Total</u>
001	GENERAL FUND	57,275.72	666,659.12
002	GENERAL FUND RESERVE	0.00	11,298,263.17
003	ONE-TIME REVENUE	0.00	446,458.61
004	CAP IMPROV/ONE-TIME INITIATIVE RSRV	0.00	4,941,978.25
101	GAS TAX	0.00	824,535.39
102	MEASURE A TRANSPORTATION TAX	0.00	1,537,693.71
103	STREET SPECIAL REVENUE	0.00	323,406.09
104	TRAFFIC CONGESTION RELIEF	0.00	0.00
111	POLICE ASSET FORFEITURE	0.00	856,887.08
113	POLICE SPECIAL REVENUE	0.00	163,719.34
121	FEDERAL/STATE GRANTS	0.00	10,445.22
122	SOLID WASTE/RECYCL.	0.00	386,767.09
123	LIBRARY SPECIAL REVENUE	0.00	47,652.93
130	IMPACT FEES	0.00	43,676.00
131	IN-LIEU FEES	0.00	3,850,282.27
132	AGENCY ON AGING	0.00	42,145.75
133	RESTRICTED REVENUES	0.00	2,647,769.97
135	GLENVIEW FIRE DONATIONS	0.00	0.00
136	EMERGENCY DISASTER RESERVE	0.00	3,202,690.21
140	DEVELOPER IN-LIEU UNDERGROUNDING	0.00	75,000.00
151	SUCCESSOR AGENCY TO THE SB RDA - OPS	0.00	0.00
152	CITY OF SB AS SUCCESSOR HOUSING AGENCY	0.00	595,407.00
153	RDA OBLIGATION RETIREMENT FUND	8,515.79	1,847,589.59
190	DISASTER RECOVERY FUND	0.00	983,475.09
201	PARKS AND FACILITIES CAPITAL	0.00	3,537,936.41
203	STREET IMPROVE. PROJECTS	0.00	2,042,720.87
207	TECHNOLOGY CAPITAL	0.00	118,529.72
302	LEASE DEBT SERVICE	900.13	114,634.13
611	WATER FUND	9,979,309.48	38,076,173.27
621	STORMWATER FUND	0.00	1,873,128.45
631	WASTEWATER FUND	17,654,475.59	43,222,547.00
641	CABLE TV FUND	0.00	(13,030,337.85)
701	CENTRAL GARAGE	0.00	35,501.13
702	FACILITY MAINT.FUND	0.00	37,281.06
703	GENERAL EQUIPMENT REVOLVING	0.00	4,715,763.54
707	TECHNOLOGY DEVELOPMENT	0.00	235,717.28
711	SELF INSURANCE	91,118.50	732,753.03
870	SAN BRUNO COMMUNITY FOUNDATION	0.00	0.00
880	PROJECT DEVELOP. TRUST	0.00	1,080,390.46
891	RECOLOGY SAN BRUNO	0.00	294,375.90
	Grand Total:	90,088,021.07 *	117,879,616.28

* Reconciliation of Pooled Cash & Investments to Portfolio Book Value

Investment Portfolio Value	86,801,578.16
Cash on hand - Checking Account	5,607,919.15
Payroll and Accounts Payable Outstanding Checks	(2,447,268.52)
Deposits in Transit	125,792.28
General Ledger Cash Balance as of July 31, 2019	90,088,021.07

Totals are through period: 1

Page: 1

ITEM 5.c.



“The City with a Heart”

Rico E. Medina, Mayor
Irene O’Connell, Vice Mayor
Laura Davis, Councilmember
Marty Medina, Councilmember
Michael Salazar, Councilmember

MINUTES
SAN BRUNO CITY COUNCIL
SPECIAL MEETING
September 10, 2019
5:00 p.m.

Meeting Location: San Bruno Senior Center, 1555 Crystal Springs Road, San Bruno, CA

1. CALL TO ORDER

2. ROLL CALL – All Council Members were present.

3. PUBLIC COMMENT ON ITEMS NOT ON AGENDA
There were no public speakers during Public Comment.

4. CLOSED SESSION – 5:00 p.m.:

- a. Conference with Labor Negotiators Pursuant to Gov't Code Section 54957.6
Agency Designated Representatives: City Manager and Assistant City Manager Employee
Organizations: San Bruno Management Employee Association, Mid Management Unit, Public Safety
Mid Management Unit and Miscellaneous Unit.
- b. Conference with Legal Counsel – Anticipated Litigation
 - Significant Exposure to Litigation Pursuant to Government Code Section 54956.9(d)(2) - One Case

The Closed Session meeting adjourned at 5:55 p.m., with item 4.b. continued until after the September 10, 2019 regular meeting adjournment.

5. SPECIAL MEETING - 6:00 p.m.:

- a. Appeal from Denial of Application of Cardroom Work Permit.

Lance Bayer, City of San Bruno Outside Counsel presented the staff report.

Truc Ma, Appellant presented her report.

There were no public speakers.

M/S Salazar/Medina to deny the appeal of cardroom work permit. **Motion carried unanimously by roll call vote.**

6. ADJOURNMENT – The meeting adjourned at 6:26 p.m.

The next Regular City Council Meeting will be held on September 10, 2019 at 7:00 p.m. at the Senior Center, 1555 Crystal Springs Road, San Bruno.

ITEM 5.d.

Minutes were prepared by Melissa Thurman, City Clerk and will be presented to the City Council for approval at the meeting of September 24, 2019.

Melissa Thurman, CMC
City Clerk

Rico E. Medina
Mayor

DRAFT



“The City with a Heart”

Rico E. Medina, Mayor
Irene O’Connell, Vice Mayor
Laura Davis, Councilmember
Marty Medina, Councilmember
Michael Salazar, Councilmember

MINUTES
SAN BRUNO CITY COUNCIL
September 10, 2019
7:00 p.m.

Meeting Location: San Bruno Senior Center, 1555 Crystal Springs Road, San Bruno, CA

1. CALL TO ORDER

2. ROLL CALL/PLEDGE OF ALLEGIANCE – All Council Members were present.

3. PUBLIC COMMENT ON ITEMS NOT ON AGENDA:

The following members of the public spoke during public comment:

- Janet Lutus – Spoke regarding parking concerns.
- Jim Evangelist – Spoke regarding the Mills Park project.

4. ANNOUNCEMENTS/PRESENTATIONS:

- a. The Community Services Department will host Concerts at the Rotary Pavilion in San Bruno City Park on Fridays in September beginning at 6:00 p.m. Concerts are free for the community to enjoy. Movies in the Park occur every Friday following the concerts. “Mary Poppins Returns” will be shown on September 13, 2019 and “BumbleBee” will be shown on September 20, 2019.
- b. Family Overnight will be held in San Bruno City Park on Friday, September 27, 2019 from 6:00 p.m. to 9:00 a.m. Saturday, September 28, 2019. Visit www.sanbruno.ca.gov/Calendar for upcoming City activities.
- c. Present Proclamation Declaring September as National Preparedness Month.

Rico Medina, Mayor presented the proclamation to **Dave Cresta, Fire Chief**.

- d. Present Proclamation for Arbor Day 2019 in San Bruno.

Rico medina, Mayor presented the proclamation to **Joanne Magrini, Community Services Director**.

5. CONSENT CALENDAR:

All items are considered routine or implement an earlier Council action and may be enacted by one motion; there will be no separate discussion, unless requested.

The City Council thanked Trisha Howard for her service on the Community Preparedness Committee.

M/S O’Connell/Davis to approve the Consent Calendar. **Motion carried unanimously by voice vote.**

- a. **Accept** Accounts Payable of August 26, 2019.
- b. **Accept** Payroll of August 23, 2019.
- c. **Approve** Draft Meeting Minutes for the Special and Regular Meetings of July 31 and August 27, 2019.

- d. **Accept** Resignation from Community Preparedness Committee Member Effective August 31, 2019 and Direct the City Clerk to Post a Notice of Vacancy in Accordance with State Law.
- e. **Waive** Second Reading and Adopt an Ordinance Regarding Amendments to San Bruno Municipal Code Chapter 7.18, Residential Permit Parking.

6. CONDUCT OF BUSINESS:

- a. **Adopt** Resolution Increasing the City of San Bruno's Aggregate Credit Limit with Tri Counties Bank from \$125,000 to \$350,000.

Keith DeMartini, Finance Director presented the report.

The following members of the public spoke regarding this item:

- Maria – Spoke regarding various financial software suggestions.

M/S Salazar/O'Connell to adopt a resolution increasing the City of San Bruno's Aggregate Credit Limit with Tri Counties Bank from \$125,000 to \$350,000. **Motion carried unanimously by roll call vote.**

- b. **Adopt** Resolution Authorizing the City Manager to Execute a Contract with Water Works Engineers for Design Services for the Sneath Lane and Lake Drive Pump Stations Rehabilitation Project in an Amount Not to Exceed \$546,037 and Appropriating \$210,000 from the Water Fund.

Jimmy Tan, Public Works Director presented the report.

M/S Davis/O'Connell to adopt the resolution authorizing the City Manager to execute a contract with Water Works Engineers for design services for the Sneath Lane and Lake Drive Pump Stations Rehabilitation Project in an amount not to exceed \$546,037 and appropriating \$210,000 from the water fund. **Motion carried unanimously by roll call vote.**

- c. **Adopt** Resolution Authorizing the City Manager to Execute an Agreement with Consolidated CM, Inc. to Provide Construction Management and Inspection Services for the Cunningham Water Tank (Water Tank No. 1) Replacement Project in an Amount Not to Exceed \$564,000.

Jimmy Tan, Public Works Director presented the report.

M/S Davis/O'Connell to adopt the resolution authorizing the City Manager to execute an agreement with Consolidated CM, Inc. to provide construction management and inspection services for the Cunningham Water Tank (Water Tank No. 1) replacement project in an amount not to exceed \$564,000. **Motion carried unanimously by roll call vote.**

- d. **Adopt** Resolution Authorizing the City Manager to Execute an Agreement with West Yost Associates for the Design of the Sweeney Ridge Tank 7 Replacement Project in an Amount Not to Exceed \$842,150 and approve the appropriation of \$393,810 from the Water Fund.

Jimmy Tan, Public Works Director presented the report.

M/S O'Connell/Davis to adopt the resolution authorizing the City Manager to execute an agreement with West Yost Associates for the Design of the Sweeney Ridge Tank 7 Replacement Project in an amount not to exceed \$393,810 from the water fund. **Motion carried unanimously by roll call vote.**

- e. **Adopt** Resolution Authorizing the City Manager to Execute a Funding Agreement with the County of San Mateo for the Award of Measure K Funding in the Amount of \$150,000 to Rehabilitate the Grandstand Facility at the Tom Lara Field located at San Bruno City Park and Appropriate \$150,000 of Revenue and Expenditure into the Tom Lara Field Improvements Capital Project in Fiscal Year 2019-20.

Jovan Grogan, City Manager presented the report.

M/S Davis/O’Connell to adopt the resolution authorizing the City Manager to execute a funding agreement with the County of San Mateo for the award of Measure K funding in the amount of \$150,000 to rehabilitate the grandstand facility at the Tom Lara Field located at San Bruno City Park and appropriate \$150,000 of revenue and expenditure into the Tom Lara Field Improvements Capital Project in Fiscal Year 2019-20. **Motion carried unanimously by roll call vote.**

7. COMMENTS FROM COUNCIL MEMBERS:

- **Irene O’Connell, Vice Mayor** – Spoke regarding Coastal Cleanup Day on September 21, 2019 and a recent City Council candidate event.
- **Marty Medina, Council Member** – Spoke regarding the Belle Aire School Walk Audit on September 11, 2019.
- **Rico Medina, Mayor** – Spoke regarding the “Stand Up to Hate” initiative.

8. ADJOURNMENT – The meeting adjourned at 8:46 p.m.

The next Regular City Council Meeting will be held on September 24, 2019 at 7:00 p.m. at the Senior Center, 1555 Crystal Springs Road, San Bruno.

Minutes were prepared by Melissa Thurman, City Clerk and will be presented to the City Council for approval at the meeting of September 24, 2019.

Melissa Thurman, CMC
City Clerk

Rico E. Medina
Mayor



City Council Agenda Item Staff Report

CITY OF SAN BRUNO

DATE: September 24, 2019

TO: Honorable Mayor and Members of the City Council

FROM: Jovan D. Grogan, City Manager

PREPARED BY: Ed Barberini, Chief of Police

SUBJECT: Adopt a Resolution Authorizing the Receipt and Expenditure of \$81,000 from the California Office of Traffic Safety Pursuant to the Federal Fiscal Year 2019-20 Selective Traffic Enforcement Program (STEP) Grant and Appropriating \$4,500 of Grant Revenue and Expenditure into the Federal/State Grants Fund.

BACKGROUND

The City of San Bruno has a residential population of just over 43,000 people, as well as a substantial transitory population traveling through the City on a daily basis. San Bruno is also home to large employers, a regional shopping mall, community college, multiple transportation centers, and a busy transit corridor. These factors, in addition to three interstate and two state highways that intersect the City, result in a high volume of vehicular and pedestrian traffic within the City on a regular basis.

The San Bruno Police Department faces a significant challenge in ensuring safety on roadways within the City. The Traffic Section of the Police Department currently consists of only one Motor Traffic Enforcement Officer and one Traffic Sergeant. Conducting consistent education and enforcement efforts that are critical to traffic safety often requires additional officers working substantial overtime hours. This overtime creates a substantial expense for the City.

The purpose of the Office of Traffic Safety (OTS) Selective Traffic Enforcement Program (STEP) Grant is to provide much needed funds to address overtime and other expenses associated with education and enforcement efforts related to traffic safety within the City limits. Examples of grant funded activities include DUI checkpoints, DUI saturation patrols, traffic enforcement connected to the violations that cause the most collisions in the City, pedestrian safety details, bicyclist safety details, seatbelt enforcement, distracted driver enforcement, and the UDETER Program at Capuchino High School.

Upon receipt of OTS STEP grant funding for FY2019-20, the San Bruno Police Department will conduct various education and enforcement details with the goal of

ITEM 5.e.

reducing injuries and deaths occurring on San Bruno roadways. OTS has awarded the San Bruno Police Department grant funding in the amount of \$81,000 for the 2019-20 Federal Fiscal Year.

DISCUSSION

In 2015, 99 injury vehicle collisions occurred in San Bruno, causing injuries to 120 people. In 2016, there were 120 injury collisions in San Bruno, causing injuries to 138 people, with one fatality. In 2017, there were 124 injury collisions in San Bruno, causing injuries to 133 people, with two fatalities.

Current OTS collision rankings place San Bruno 9th out of 93 similar cities in the category of collisions involving injury or fatalities. San Bruno ranks 51st of 93 in the category of alcohol involved collisions and 35th of 93 for DUI collisions involving drivers between the ages of 21-34. The three primary collision factors (PCFs) responsible for injury collisions in San Bruno are unsafe speed, red light violations, and failure to yield to pedestrians.

The San Bruno Police Department continues to address traffic safety related issues on a continual basis; however, the existing staffing level of the Traffic Section of the Police Department is not sufficient to conduct enforcement at an adequate level. Therefore, assigning officers to conduct targeted enforcement on an overtime basis is essential to traffic safety. Additionally, DUI checkpoints, saturation patrols, and pedestrian safety details require many more staff members than the Traffic Section can accommodate.

For many years, the San Bruno Police Department has applied for, and been granted, OTS STEP Grant funds in an effort to better address the traffic safety issues facing the community. In FY2017-18, the department was denied its grant request. The Police Department observed a spike in nearly all of our collision and DUI statistics. In FY2018-19, the Police Department was awarded \$75,000 in OTS grant funding that enabled the Department to make some progress toward enhancing traffic safety. The San Bruno Police Department has been awarded \$81,000 in FY2019-20. These funds are critical to providing improved traffic safety in the community. These funds not only address overtime expenses associated with a variety of education and enforcement efforts, but also provide significant additional training to officers and much needed equipment for conducting traffic safety operations.

The goals of the OTS STEP Grant include:

1. Reducing the number of motorists, pedestrians and bicyclists killed or injured in traffic collisions
2. Reducing the number of motorists, pedestrians and bicyclists killed or injured in alcohol or drug related traffic collisions
3. Reducing the number of motorists, pedestrians and bicyclists killed in traffic collisions

4. Reducing hit and run fatality and injury collisions
5. Reducing nighttime collisions
6. Educating the community of the dangers of irresponsible driving practices

The objectives of the OTS STEP Grant include:

1. Participating in all National Highway Traffic Safety Administration (NHTSA) safety campaigns throughout the grant period
2. Developing and maintain a monthly “hot sheet” of repeat DUI offenders
3. Sending at least five officers through the NHTSA Standardized Field Sobriety Testing Course
4. Sending at least five officers through the NHTSA Advanced Roadside Impaired Driving Enforcement Course
5. Sending at least one officer through Drug Recognition Expert (DRE) training
6. Sending at least two officers through DRE Recertification Training
7. Sending at least one officer through SFST Instructor School
8. Sending at least one officer through DRE Instructor School
9. Conducting at least three DUI Checkpoints
10. Conducting at least twelve DUI Saturation Patrol operations
11. Conducting at least thirty traffic enforcement operations based on PCFs
12. Conducting at least six distracted driving enforcement operations
13. Conducting at least six pedestrian/bicycle safety enforcement operations
14. Conducting the UDETER program at Capuchino High School
15. Conducting at least two collaborative DUI enforcement operations
16. Conducting at least two collaborative traffic enforcement operations
17. Issuing press releases and social media posts to publicize all efforts
18. Purchasing a handheld Citation Data Collection Device
19. Purchasing supplies for running DUI checkpoints

FISCAL IMPACT

The training and operation for OTS STEP Grant initiatives is fully funded via the California Office of Traffic Safety, Selective Traffic Enforcement Program Grant in the amount of \$81,000 for the federal fiscal year. \$75,000 was included in the Federal/State Grants Fund in the City’s adopted FY2019-20 budget for this grant. The adoption of the resolution will increase the revenue and expenditures budget in the Federal/State Grants Fund by \$4,500 in FY2019-20.

ALTERNATIVES

1. Do not accept the grant funding.

RECOMMENDATION

Adopt a resolution authorizing the receipt and expenditure of \$81,000 from the California Office of Traffic Safety pursuant to the Fiscal Year 2019-20 Selective Traffic Enforcement Program Grant and Appropriating \$4,500 of Grant Revenue and Expenditure into the Federal/State Grants Fund.

ATTACHMENTS

1. Resolution
2. Agreement between the California Office of Traffic Safety and the San Bruno Police Department

DATE PREPARED:

September 4, 2019

RESOLUTION NO. 2019-_____

RESOLUTION AUTHORIZING THE RECEIPT AND EXPENDITURE OF \$81,000 IN THE CALIFORNIA OFFICE OF TRAFFIC SAFETY PURSUANT TO THE FEDERAL FISCAL YEAR 2019-20 SELECTIVE TRAFFIC ENFORCEMENT PROGRAM (STEP) GRANT AND APPROPRIATING \$4,500 OF GRANT REVENUE AND EXPENDITURE INTO THE FEDERAL/STATE GRANTS FUNDS

WHEREAS, the San Bruno Police Department is charged with working to reduce traffic related injuries and deaths within San Bruno City Limits; and

WHEREAS, upon receipt of \$81,000 in Office of Traffic Safety (OTS) grant funding for Federal Fiscal Year 2019-20, the San Bruno Police Department will continue to conduct traffic safety education and enforcement operations with the goals of reducing traffic related injuries and deaths within City limits; and

WHEREAS, The San Bruno Police Department and OTS have collaborated to establish the goals and objectives for the 2019-20 Federal Fiscal Year to achieve the intended outcomes of the grant funding.

NOW THEREFORE, BE IT RESOLVED that the San Bruno Police Department is authorized to execute the attached agreement, including any extensions or amendments thereof and any subsequent contract with the state in relation thereto.

IT IS AGREED that any liability arising out of the performance of this contract, including civil court actions for damages, shall be the responsibility of the grant recipient and the authorizing agency. The State of California and OTS disclaim responsibility for any such liability.

BE IT FURTHER RESOLVED that grant funds received hereunder shall not be used to supplant expenditures controlled by this body.

ATTEST:

Melissa Thurman, CMC
City Clerk

Date: _____

-000-

I, Melissa Thurman, City Clerk, do hereby certify that the foregoing is a true copy of the resolution adopted by the City Council of the City of San Bruno in a meeting thereof held on this 24th day of September 2019 by the following vote:

AYES: Councilmembers: _____
NOES: Councilmembers _____
ABSENT: Councilmembers: _____

10. PROJECTED EXPENDITURES						
FUND	CFDA	ITEM/APPROPRIATION	F.Y.	CHAPTER	STATUTE	PROJECTED EXPENDITURES
164 AL-20	20.608	0521-0890-101	2019	2019	BA/19	\$50,000.00
402PT-20	20.600	0521-0890-101	2019	2019	BA/19	\$25,000.00
405c TR-20	20.616	0521-0890-101	2019	2019	BA/19	\$6,000.00
					AGREEMENT TOTAL	\$81,000.00
					AMOUNT ENCUMBERED BY THIS DOCUMENT \$81,000.00	
<i>I CERTIFY upon my own personal knowledge that the budgeted funds for the current budget year are available for the period and purpose of the expenditure stated above.</i>					PRIOR AMOUNT ENCUMBERED FOR THIS AGREEMENT \$ 0.00	
					TOTAL AMOUNT ENCUMBERED TO DATE \$81,000.00	
OTS ACCOUNTING OFFICER'S SIGNATURE ✍			DATE SIGNED			

1. PROBLEM STATEMENT

The City of San Bruno has a population of just over 43,000 people living within its borders. San Bruno also has a substantial transient population that makes its way into the city each day, due largely to the following locations within its jurisdiction: Tanforan shopping mall, Skyline Community College, San Bruno BART station, San Bruno Cal-Train station, You-Tube campus and Walmart campus. The BART and Cal-Train Stations are located near downtown San Bruno and have created a busy transit corridor with a high volume of vehicle, pedestrian and bicycle traffic. This busy transit corridor presents the city with many traffic safety challenges, especially during commute times on weekdays. San Bruno also shares its eastern border with the San Francisco International Airport, which results in major travel-related traffic throughout the city.

Additionally, San Bruno has three major Interstates (US-101, I-280 and I-380), and two major state routes (SR-82 and SR-35) running directly through the city. These thoroughfares bring substantial additional traffic into the city, which results in increased traffic safety issues that require constant attention from the Police Department. As our in-house statistics indicate, the scope of the traffic safety problem facing San Bruno is substantial in comparison to cities of similar size.

2. PERFORMANCE MEASURES

A. Goals:

1. Reduce the number of persons killed in traffic collisions.
2. Reduce the number of persons injured in traffic collisions.
3. Reduce the number of pedestrians killed in traffic collisions.
4. Reduce the number of pedestrians injured in traffic collisions.
5. Reduce the number of bicyclists killed in traffic collisions.
6. Reduce the number of bicyclists injured in traffic collisions.
7. Reduce the number of persons killed in alcohol-involved collisions.
8. Reduce the number of persons injured in alcohol-involved collisions.
9. Reduce the number of persons killed in drug-involved collisions.
10. Reduce the number of persons injured in drug-involved collisions.
11. Reduce the number of persons killed in alcohol/drug combo-involved collisions.
12. Reduce the number of persons injured in alcohol/drug combo-involved collisions.
13. Reduce the number of motorcyclists killed in traffic collisions.
14. Reduce the number of motorcyclists injured in traffic collisions.
15. Reduce hit & run fatal collisions.
16. Reduce hit & run injury collisions.
17. Reduce nighttime (2100 - 0259 hours) fatal collisions.
18. Reduce nighttime (2100 - 0259 hours) injury collisions.

B. Objectives:

	Target Number
1. Issue a press release announcing the kick-off of the grant by November 15. The kick-off press releases and media advisories, alerts, and materials must be emailed to the OTS Public Information Officer at pio@ots.ca.gov , and copied to your OTS Coordinator, for approval 14 days prior to the issuance date of the release.	1
2. Participate and report data (as required) in the following campaigns, National Walk to School Day, National Teen Driver Safety Week, NHTSA Winter Mobilization, National Distracted Driving Awareness Month, National Motorcycle Safety Month, National Bicycle Safety Month, National Click it or Ticket Mobilization, NHTSA Summer Mobilization, National Child Passenger Safety Week, and California's Pedestrian Safety Month.	10
3. Develop (by December 31) and/or maintain a “HOT Sheet” program to notify patrol and traffic officers to be on the lookout for identified repeat DUI offenders with a suspended or revoked license as a result of DUI convictions. Updated HOT sheets should be distributed to patrol and traffic officers monthly.	12
4. Send law enforcement personnel to the NHTSA Standardized Field Sobriety Testing (SFST) (minimum 16 hours) POST-certified training.	5

5. Send law enforcement personnel to the NHTSA Advanced Roadside Impaired Driving Enforcement (ARIDE) 16 hour POST-certified training.	5
6. Send law enforcement personnel to the Drug Recognition Expert (DRE) training.	1
7. Send law enforcement personnel to the DRE Recertification training.	2
8. Send law enforcement personnel to SFST Instructor training.	1
9. Send law enforcement personnel to DRE Instructor training.	1
10. Conduct DUI/DL Checkpoints. A minimum of 1 checkpoint should be conducted during the NHTSA Winter Mobilization and 1 during the Summer Mobilization. To enhance the overall deterrent effect and promote high visibility, it is recommended the grantee issue an advance press release and conduct social media activity for each checkpoint. For combination DUI/DL checkpoints, departments should issue press releases that mention DL's will be checked at the DUI/DL checkpoint. Signs for DUI/DL checkpoints should read "DUI/Driver's License Checkpoint Ahead." OTS does not fund or support independent DL checkpoints. Only on an exception basis and with OTS pre-approval will OTS fund checkpoints that begin prior to 1800 hours. When possible, DUI/DL Checkpoint screeners should be DRE- or ARIDE-trained.	3
11. Conduct DUI Saturation Patrol operation(s).	12
12. Conduct Traffic Enforcement operation(s), including but not limited to, primary collision factor violations.	30
13. Conduct highly publicized Distracted Driving enforcement operation(s) targeting drivers using hand held cell phones and texting.	6
14. Conduct highly publicized pedestrian and/or bicycle enforcement operation(s) in areas or during events with a high number of pedestrian and/or bicycle collisions resulting from violations made by pedestrians, bicyclists, and drivers.	6
15. Conduct Traffic Safety educational presentation(s) with an effort to reach community members. Note: Presentation(s) may include topics such as distracted driving, DUI, speed, bicycle and pedestrian safety, seat belts and child passenger safety.	1
16. Conduct Know Your Limit campaigns with an effort to reach members of the community.	1
17. Conduct highly visible collaborative DUI Enforcement operations	2
18. Conduct highly visible collaborative Traffic Enforcement operations	2
3. METHOD OF PROCEDURE	
A. Phase 1 – Program Preparation (1st Quarter of Grant Year)	
<ul style="list-style-type: none"> The department will develop operational plans to implement the “best practice” strategies outlined in the objectives section. All training needed to implement the program should be conducted this quarter. All grant related purchases needed to implement the program should be made this quarter. In order to develop/maintain the “Hot Sheets,” research will be conducted to identify the “worst of the worst” repeat DUI offenders with a suspended or revoked license as a result of DUI convictions. The Hot Sheets may include the driver’s name, last known address, DOB, description, current license status, and the number of times suspended or revoked for DUI. Hot Sheets should be updated and distributed to traffic and patrol officers at least monthly. Implementation of the STEP grant activities will be accomplished by deploying personnel at high collision locations.<u>Media Requirements</u> Issue a press release announcing the kick-off of the grant by November 15, but no earlier than October 1. If unable to meet the November 15 date, communicate reasons to your OTS Coordinator. The kick-off press releases and any related media advisories, alerts, and materials must be emailed for approval to the OTS Public Information Officer at pio@ots.ca.gov, and copied to your OTS Coordinator, 14 days prior to the issuance date of the release. 	
B. Phase 2 – Program Operations (Throughout Grant Year)	
The department will work to create media opportunities throughout the grant period to call attention to the innovative program strategies and outcomes.	
<u>Media Requirements</u>	
<ul style="list-style-type: none"> Send all grant-related activity press releases, media advisories, alerts and general public materials to the OTS Public Information Officer (PIO) at pio@ots.ca.gov, with a copy to your OTS Coordinator. The 	

following requirements are for grant-related activities and are different from those regarding any grant kick-off release or announcement.

- If an OTS-supplied, template-based press release is used, there is no need for pre-approval, however, the OTS PIO and Coordinator should be copied when at the same time as the release is distributed to the press.
- If an OTS-supplied template is not used, or is substantially changed, a draft press release shall be sent to the OTS PIO for approval. Optimum lead-time would be 10 days prior to the release distribution date, but should be no less than 5 working days prior to the release distribution date.
- Press releases reporting the immediate and time-valued results of grant activities such as enforcement operations are exempt from the recommended advance approval process, but still should be copied to the OTS PIO and Coordinator when the release is distributed to the press.
- Use the following standard language in all press, media, and printed materials: Funding for this program was provided by a grant from the California Office of Traffic Safety, through the National Highway Traffic Safety Administration.
- Email the OTS PIO at pio@ots.ca.gov and copy your OTS Coordinator at least 30 days in advance, a short description of any significant grant-related traffic safety event or program so OTS has sufficient notice to arrange for attendance and/or participation in the event.
- Submit a draft or rough-cut of all printed or recorded material (brochures, posters, scripts, artwork, trailer graphics, etc.) to the OTS PIO at pio@ots.ca.gov and copy your OTS Coordinator for approval 14 days prior to the production or duplication.
- Space permitting, include the OTS logo, on grant-funded print materials; consult your OTS Coordinator for specifics and format-appropriate logos.
- Contact the OTS PIO or your OTS Coordinator, sufficiently far enough in advance of need, for consultation when deviation from any of the above requirements might be contemplated

C. Phase 3 – Data Collection & Reporting (Throughout Grant Year)

- Invoice Claims (due January 30, April 30, July 30, and October 30)
- Quarterly Performance Reports (due January 30, April 30, July 30, and October 30)
 - Collect and report quarterly, appropriate data that supports the progress of goals and objectives.
 - Provide a brief list of activity conducted, procurement of grant-funded items, and significant media activities. Include status of grant-funded personnel, status of contracts, challenges, or special accomplishments.
 - Provide a brief summary of quarterly accomplishments and explanations for objectives not completed or plans for upcoming activities.
 - Collect, analyze and report statistical data relating to the grant goals and objectives.

4. METHOD OF EVALUATION

Using the data compiled during the grant, the Grant Director will complete the “Final Evaluation” section in the fourth/final Quarterly Performance Report (QPR). The Final Evaluation should provide a brief summary of the grant’s accomplishments, challenges and significant activities. This narrative should also include whether goals and objectives were met, exceeded, or an explanation of why objectives were not completed.

5. ADMINISTRATIVE SUPPORT

This program has full administrative support, and every effort will be made to continue the grant activities after grant conclusion.

FUND NUMBER	CATALOG NUMBER (CFDA)	FUND DESCRIPTION	TOTAL AMOUNT
164 AL-20	20.608	Minimum Penalties for Repeat Offenders for Driving While Intoxicated	\$50,000.00
402PT-20	20.600	State and Community Highway Safety	\$25,000.00
405c TR-20	20.616	State Traffic Safety Information System Improvements	\$6,000.00

COST CATEGORY	CFDA	TOTAL COST TO GRANT
A. PERSONNEL COSTS		
<u>Straight time</u>		\$0.00
<u>Overtime</u>		
DUI/DL Checkpoints	20.608	\$27,000.00
DUI Saturation Patrols	20.608	\$19,200.00
Know Your Limit	20.608	\$2,400.00
Traffic Enforcement	20.600	\$15,000.00
Distracted Driving	20.600	\$3,000.00
Pedestrian and Bicycle Enforcement	20.600	\$3,000.00
Category Sub-Total		\$69,600.00
B. TRAVEL EXPENSES		
In State Travel	20.600	\$4,000.00
		\$0.00
Category Sub-Total		\$4,000.00
C. CONTRACTUAL SERVICES		
		\$0.00
Category Sub-Total		\$0.00
D. EQUIPMENT		
Handheld Citation Data Collection Devices	20.616	\$6,000.00
Category Sub-Total		\$6,000.00
E. OTHER DIRECT COSTS		
DUI Checkpoint Supplies	20.608	\$1,400.00
Category Sub-Total		\$1,400.00
F. INDIRECT COSTS		
		\$0.00
Category Sub-Total		\$0.00
GRANT TOTAL		\$81,000.00

BUDGET NARRATIVE	
PERSONNEL COSTS DUI/DL Checkpoints - Overtime for grant funded law enforcement operations conducted by appropriate department personnel.	QUANTITY 3
DUI Saturation Patrols - Overtime for grant funded law enforcement operations conducted by appropriate department personnel.	12
Know Your Limit - Overtime for grant funded traffic safety presentations or campaigns conducted by appropriate department personnel.	1
Traffic Enforcement - Overtime for grant funded law enforcement operations conducted by appropriate department personnel.	30
Distracted Driving - Overtime for grant funded law enforcement operations conducted by appropriate department personnel.	6
Pedestrian and Bicycle Enforcement - Overtime for grant funded law enforcement operations conducted by appropriate department personnel.	6
TRAVEL EXPENSES In State Travel - Costs are included for appropriate staff to attend conferences and training events supporting the grant goals and objectives and/or traffic safety. Local mileage for grant activities and meetings is included. All conferences, seminars or training not specifically identified in the Budget Narrative must be approved by OTS. All travel claimed must be at the agency approved rate. Per Diem may not be claimed for meals provided at conferences when registration fees are paid with OTS grant funds.	1
CONTRACTUAL SERVICES -	
EQUIPMENT Handheld Citation Data Collection Devices - Handheld devices for traffic officer use to collect citation data, print the citation in the field, and transfer the information electronically to the agency RMS system and to the courts. The system will be used by traffic officers to improve the efficiency and accuracy of writing traffic citations. Costs include the purchase of electronic citation device, with integral mag-strip reader, thermal printer, audio recorder, camera, docking/charging station, software, licenses, accessories, training, and associated shipping and taxes.	1
OTHER DIRECT COSTS DUI Checkpoint Supplies - On-scene supplies needed to conduct sobriety checkpoints. Costs may include 28" traffic cones, MUTCD compliant traffic signs, MUTCD compliant high visibility vests (maximum of 10), traffic counters (maximum of 2), generator, gas for generators, lighting, reflective banners, electronic flares, PAS device supplies, heater, propane for heaters, fan, anti-fatigue mats, and canopies. Additional items may be purchased if approved by OTS. The cost of food and beverages will not be reimbursed.	1
INDIRECT COSTS -	
STATEMENTS/DISCLAIMERS Program Income default statement: There will be no program income generated from this grant. Enforcement Grant Quota Disclaimer:	

Nothing in this “agreement” shall be interpreted as a requirement, formal or informal, that a particular law enforcement officer issue a specified or predetermined number of citations in pursuance of the goals and objectives here under.

CERTIFICATIONS AND ASSURANCES
HIGHWAY SAFETY GRANTS
(23 U.S.C. CHAPTER 4 AND SEC. 1906, PUB. L. 109-59, AS AMENDED)

Failure to comply with applicable Federal statutes, regulations, and directives may subject Grantee Agency officials to civil or criminal penalties and/or place the State in a high-risk grantee status in accordance with 49 CFR §18.12.

The officials named on the grant agreement, certify by way of signature on the grant agreement signature page, that the Grantee Agency complies with all applicable Federal statutes, regulations, and directives and State rules, guidelines, policies and laws in effect with respect to the periods for which it receives grant funding. Applicable provisions include, but are not limited to, the following:

- 23 U.S.C. Chapter 4—Highway Safety Act of 1966, as amended
- 49 CFR Part 18—Uniform Administrative Requirements for Grants and Cooperative Agreements to State and Local Governments
- 23 CFR Part 1300—Uniform Procedures for State Highway Safety Grant Programs

NONDISCRIMINATION

(applies to subrecipients as well as States)

The State highway safety agency will comply with all Federal statutes and implementing regulations relating to nondiscrimination (“Federal Nondiscrimination Authorities”). These include but are not limited to:

- Title VI of the Civil Rights Act of 1964 (42 U.S.C. 2000d et seq., 78 stat. 252), (prohibits discrimination on the basis of race, color, national origin) and 49 CFR part 21;
- The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, (42 U.S.C. 4601), (prohibits unfair treatment of persons displaced or whose property has been acquired because of Federal or Federal-aid programs and projects);
- Federal-Aid Highway Act of 1973, (23 U.S.C. 324 et seq.), and Title IX of the Education Amendments of 1972, as amended (20 U.S.C. 1681-1683 and 1685-1686) (prohibit discrimination on the basis of sex);
- Section 504 of the Rehabilitation Act of 1973, (29 U.S.C. 794 et seq.), as amended, (prohibits discrimination on the basis of disability) and 49 CFR part 27;
- The Age Discrimination Act of 1975, as amended, (42 U.S.C. 6101 et seq.), (prohibits discrimination on the basis of age);
- The Civil Rights Restoration Act of 1987, (Pub. L. 100-209), (broadens scope, coverage and applicability of Title VI of the Civil Rights Act of 1964, The Age Discrimination Act of 1975 and Section 504 of the Rehabilitation Act of 1973, by expanding the definition of the terms "programs or activities" to include all of the programs or activities of the Federal aid recipients, subrecipients and contractors, whether such programs or activities are Federally-funded or not);
- Titles II and III of the Americans with Disabilities Act (42 U.S.C. 12131-12189) (prohibits discrimination on the basis of disability in the operation of public entities, public and private transportation systems, places of public accommodation, and certain testing) and 49 CFR parts 37 and 38;

- Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations (prevents discrimination against minority populations by discouraging programs, policies, and activities with disproportionately high and adverse human health or environmental effects on minority and low-income populations); and
- Executive Order 13166, Improving Access to Services for Persons with Limited English Proficiency (guards against Title VI national origin discrimination/discrimination because of limited English proficiency (LEP) by ensuring that funding recipients take reasonable steps to ensure that LEP persons have meaningful access to programs (70 FR 74087-74100)).

The State highway safety agency—

- Will take all measures necessary to ensure that no person in the United States shall, on the grounds of race, color, national origin, disability, sex, age, limited English proficiency, or membership in any other class protected by Federal Nondiscrimination Authorities, be excluded from participation in, be denied the benefits of, or be otherwise subjected to discrimination under any of its programs or activities, so long as any portion of the program is Federally-assisted;
- Will administer the program in a manner that reasonably ensures that any of its subrecipients, contractors, subcontractors, and consultants receiving Federal financial assistance under this program will comply with all requirements of the Non-Discrimination Authorities identified in this Assurance;
- Agrees to comply (and require its subrecipients, contractors, subcontractors, and consultants to comply) with all applicable provisions of law or regulation governing US DOT's or NHTSA's access to records, accounts, documents, information, facilities, and staff, and to cooperate and comply with any program or compliance reviews, and/or complaint investigations conducted by US DOT or NHTSA under any Federal Nondiscrimination Authority;
- Acknowledges that the United States has a right to seek judicial enforcement with regard to any matter arising under these Non-Discrimination Authorities and this Assurance;
- Agrees to insert in all contracts and funding agreements with other State or private entities the following clause:

“During the performance of this contract/funding agreement, the contractor/funding recipient agrees—

- To comply with all Federal nondiscrimination laws and regulations, as may be amended from time to time;
- Not to participate directly or indirectly in the discrimination prohibited by any Federal non-discrimination law or regulation, as set forth in appendix B of 49 CFR part 21 and herein;
- To permit access to its books, records, accounts, other sources of information, and its facilities as required by the State highway safety office, US DOT or NHTSA;
- That, in event a contractor/funding recipient fails to comply with any nondiscrimination provisions in this contract/funding agreement, the State highway safety agency will have the right to impose such contract/agreement sanctions as it or NHTSA determine are appropriate, including but not limited to withholding payments to the contractor/funding

recipient under the contract/agreement until the contractor/funding recipient complies; and/or cancelling, terminating, or suspending a contract or funding agreement, in whole or in part; and

- To insert this clause, including paragraphs (a) through (e), in every subcontract and sub agreement and in every solicitation for a subcontract or sub-agreement, that receives Federal funds under this program.

POLITICAL ACTIVITY (HATCH ACT)

(applies to subrecipients as well as States)

The State will comply with provisions of the Hatch Act (5 U.S.C. 1501-1508), which limits the political activities of employees whose principal employment activities are funded in whole or in part with Federal funds.

CERTIFICATION REGARDING FEDERAL LOBBYING

(applies to subrecipients as well as States)

Certification for Contracts, Grants, Loans, and Cooperative Agreements

The undersigned certifies, to the best of his or her knowledge and belief, that:

1. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement;
2. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions;
3. The undersigned shall require that the language of this certification be included in the award documents for all sub-award at all tiers (including subcontracts, subgrants, and contracts under grant, loans, and cooperative agreements) and that all subrecipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by section 1352, title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

RESTRICTION ON STATE LOBBYING

(applies to subrecipients as well as States)

None of the funds under this program will be used for any activity specifically designed to urge or influence a State or local legislator to favor or oppose the adoption of any specific legislative proposal pending before any State or local legislative body. Such activities include both direct and indirect (e.g., "grassroots") lobbying activities, with one exception. This does not preclude a State official whose salary is supported with NHTSA funds from engaging in direct communications with State or local legislative officials, in accordance with customary State practice, even if such communications urge legislative officials to favor or oppose the adoption of a specific pending legislative proposal.

CERTIFICATION REGARDING DEBARMENT AND SUSPENSION

(applies to subrecipients as well as States)

Instructions for Primary Tier Participant Certification (States)

1. By signing and submitting this proposal, the prospective primary tier participant is providing the certification set out below and agrees to comply with the requirements of 2 CFR parts 180 and 1200.

2. The inability of a person to provide the certification required below will not necessarily result in denial of participation in this covered transaction. The prospective primary tier participant shall submit an explanation of why it cannot provide the certification set out below. The certification or explanation will be considered in connection with the department or agency's determination whether to enter into this transaction. However, failure of the prospective primary tier participant to furnish a certification or an explanation shall disqualify such person from participation in this transaction.

3. The certification in this clause is a material representation of fact upon which reliance was placed when the department or agency determined to enter into this transaction. If it is later determined that the prospective primary tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department or agency may terminate this transaction for cause or default or may pursue suspension or debarment.

4. The prospective primary tier participant shall provide immediate written notice to the department or agency to which this proposal is submitted if at any time the prospective primary tier participant learns its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

5. The terms covered transaction, civil judgment, debarment, suspension, ineligible, participant, person, principal, and voluntarily excluded, as used in this clause, are defined in 2 CFR parts 180 and 1200. You may contact the department or agency to which this proposal is being submitted for assistance in obtaining a copy of those regulations.

6. The prospective primary tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is proposed for debarment under 48 CFR part 9, subpart 9.4, debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency entering into this transaction.

7. The prospective primary tier participant further agrees by submitting this proposal that it will include the clause titled "Instructions for Lower Tier Participant Certification" including the "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion—Lower Tier Covered Transaction," provided by the department or agency entering into this covered transaction, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions and will require lower tier participants to comply with 2 CFR parts 180 and 1200.

8. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that it is not proposed for debarment under 48 CFR part 9, subpart 9.4, debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant is responsible for ensuring that its principals are not suspended, debarred, or otherwise ineligible to participate in covered transactions. To verify the eligibility of its principals, as well as the eligibility of any prospective lower tier participants, each participant may, but is not required to, check the System for Award Management Exclusions website (<https://www.sam.gov/>).

9. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

10. Except for transactions authorized under paragraph 6 of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is proposed for debarment under 48 CFR part 9, subpart 9.4, suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal government, the department or agency may terminate the transaction for cause or default.

Certification Regarding Debarment, Suspension, and Other Responsibility Matters-Primary Tier Covered Transactions

- (1) The prospective primary tier participant certifies to the best of its knowledge and belief, that it and its principals:
- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participating in covered transactions by any Federal department or agency;

(b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;

(c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or Local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and

(d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State, or local) terminated for cause or default.

(2) Where the prospective primary tier participant is unable to certify to any of the Statements in this certification, such prospective participant shall attach an explanation to this proposal.

Instructions for Lower Tier Participant Certification

1. By signing and submitting this proposal, the prospective lower tier participant is providing the certification set out below and agrees to comply with the requirements of 2 CFR parts 180 and 1200.

2. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal government, the department or agency with which this transaction originated may pursue available remedies, including suspension or debarment.

3. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

4. The terms covered transaction, civil judgment, debarment, suspension, ineligible, participant, person, principal, and voluntarily excluded, as used in this clause, are defined in 2 CFR parts 180 and 1200. You may contact the person to whom this proposal is submitted for assistance in obtaining a copy of those regulations.

5. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is proposed for debarment under 48 CFR part 9, subpart 9.4, debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

6. The prospective lower tier participant further agrees by submitting this proposal that it will include the clause titled "Instructions for Lower Tier Participant Certification" including the "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transaction," without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions and will require lower tier participants to comply with 2 CFR parts 180 and 1200.

7. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that it is not proposed for debarment under 48 CFR part 9, subpart 9.4, debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant is responsible for ensuring that its principals are not suspended, debarred, or otherwise ineligible to participate in covered transactions. To verify the eligibility of its principals, as well as the eligibility of any prospective lower tier participants, each participant may, but is not required to, check the System for Award Management Exclusions website (<https://www.sam.gov/>).

8. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

9. Except for transactions authorized under paragraph 5 of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is proposed for debarment under 48 CFR part 9, subpart 9.4, suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal government, the department or agency with which this transaction originated may pursue available remedies, including suspension or debarment.

Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion -- Lower Tier Covered Transactions:

1. The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participating in covered transactions by any Federal department or agency.
2. Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

BUY AMERICA ACT

(applies to subrecipients as well as States)

The State and each subrecipient will comply with the Buy America requirement (23 U.S.C. 313) when purchasing items using Federal funds. Buy America requires a State, or subrecipient, to purchase with Federal funds only steel, iron and manufactured products produced in the United States, unless the Secretary of Transportation determines that such domestically produced items would be inconsistent with the public interest, that such materials are not reasonably available and of a satisfactory quality, or that inclusion of domestic materials will increase the cost of the overall project contract by more than 25 percent. In order to use Federal funds to purchase foreign produced items, the State must submit a waiver request that provides an adequate basis and justification for approval by the Secretary of Transportation.

PROHIBITION ON USING GRANT FUNDS TO CHECK FOR HELMET USAGE

(applies to subrecipients as well as States)

The State and each subrecipient will not use 23 U.S.C. Chapter 4 grant funds for programs to check helmet usage or to create checkpoints that specifically target motorcyclists.

LAW ENFORCEMENT AGENCIES

All subrecipient law enforcement agencies shall comply with California law regarding profiling. Penal Code section 13519.4, subdivision (e), defines “racial profiling” as the “practice of detaining a suspect based on a broad set of criteria which casts suspicion on an entire class of people without any individualized suspicion of the particular person being stopped.” Then, subdivision (f) of that section goes on to provide, “A law enforcement officer shall not engage in racial profiling.”



**City Council Agenda Item
Staff Report**

CITY OF SAN BRUNO

DATE: September 24, 2019

TO: Honorable Mayor and Members of the City Council

FROM: Jovan D. Grogan, City Manager

PREPARED BY: Dave Cresta, Fire Chief

SUBJECT: Adopt Resolution to Authorize Replacing the Fire Department Secretary Position with an Executive Assistant in the Fiscal Year 2019-20 Budget

BACKGROUND:

With the recent departure of the Fire Department Secretary, the Fire Department conducted an evaluation of the administrative needs of the organization. The San Bruno Fire Department has 35 fulltime sworn positions and one non-sworn (Secretary) position. The sworn positions include the Fire Chief, four Battalion Chiefs, nine Fire Captains and twenty-one Firefighters. The Department operates on 24 hours a day, 7 days a week basis. Three of the Battalion Chiefs are assigned to the 56-hour work schedule that comprise the three separate shifts that make up the operations division. The fourth Battalion Chief (Administrative) is assigned as the City's Fire Marshal.

After the review of the administrative needs of the Fire Department as it relates to current operations, programs, responsibilities and anticipated future challenges, the Department is recommending replacing the current budgeted Secretary position with an Executive Assistant position. While the previous Secretary position provided routine clerical and administrative work, the Department requests a higher level of administrative support to enhance operations and support the administrative functions.

DISCUSSION:

The Fire Department responsibilities are divided into four separate work programs: Operations, Administrative, Training and Prevention/Emergency Management. Each program is overseen by one of the four Battalion Chiefs. Additionally, the Fire Department's nine Fire Captains have varying responsibilities in each program. The 56-hour operational work schedule that is necessary to operate on a 24 hour a day, 7 days a week basis possesses challenges for administrative continuity between staff responsible for the various programs. A higher-level administrative position with more complex responsibilities like that of an Executive Assistant would improve the administrative cohesiveness between these programs.

ITEM 5.f.

The Fire Prevention and Life Safety Division is responsible for enforcement of codes and ordinances. Additionally, the Division works with the Community Development Department to review and approve development projects proposed in San Bruno. The Division manages the inspection program that includes mandatory inspections that are conducted by the engine and truck companies. Overall, the Division handles approximately 900 reoccurring inspections on an annual basis. An additional administrative responsibility has been placed on the Department with the passage of Senate Bill 1205 which will require the Department to submit an annual report to the governing body regarding the jurisdictions mandated inspections. The Division recently added an additional full-time Fire Prevention Firefighter position authorized in the FY2018-19 Operating Budget. The Executive Assistant will support the Fire Prevention and Life Safety Division by managing the schedule for the Fire Marshal and Firefighter assigned to the Division, as well as interact with residents, builders and business owners who are seeking inspections. Additional support will include compiling and summarizing the inspection data and preparing the necessary reports.

The Department provides several public education programs that would benefit from administrative support. CPR/AED Training, Community Emergency Response Team (CERT), Car Safety Seat Inspection, and Smoke Alarm Installation are a few of the programs that the Department offers. Residents sign up for these programs on the San Bruno Responds Application and administrative coordination is needed to plan, schedule and coordinate with the appropriate Fire Captain or Firefighter providing the service. These programs will be enhanced with increased attention and oversight from an Executive Assistant.

The previous Secretary position provided limited administrative support to the Fire Chief. The Executive Assistant will be able to provide administrative assistance with the preparation and execution of the budget. Additionally, the position will assist with the preparation of various reports and presentations that are the responsibility of the Fire Chief. The Battalion Chiefs have significant administrative responsibilities as well and need increased administrative support.

To summarize the addition of the Executive Assistant Position will improve Department operations in the following manner:

- Provide administrative and executive assistant support to the Fire Chief and also support Battalion Chief's, who are responsible for managing Fire Department Programs which include Operations, Administrative, Training and Prevention/Emergency Management.
- Provide administrative support and continuity for all the Fire Department programs and services by conducting research and compiling and summarizing a variety of information and statistical material from various sources and preparing reports for decision making purposes.

- Provide administrative support for the Fire Prevention Bureau by making appointments and maintaining a calendar, scheduling and arranging meetings.
- Represent the Fire Department on projects and citywide initiatives.
- Provide administrative support for implementation and use of the Departments Records Management System.

FISCAL IMPACT:

The current full loaded cost (salary and benefits) for the Secretary position is budgeted at approximately \$110,000. The annual fully loaded cost for the Executive Assistant position at Step 3 is \$109,423, which is cost neutral with the current budgeted Secretary position. This is partially due to the fact that the prior incumbent of the Secretary position was at Step 5 in the salary schedule and the proposed Executive Assistant position will be budgeted at Step 3. Therefore, no additional funds are being requested at this time. At top step (Step 5), the Executive Assistant position costs \$118,363, which is \$4,177 more than the top step of the Secretary position.

ALTERNATIVES:

1. Do not approve the Executive Assistant Position.

RECOMMENDATION:

Adopt Resolution to Authorize Replacing the Fire Department Secretary Position with an Executive Assistant in the Fiscal Year 2019-20 Budget.

DISTRIBUTION:

None.

ATTACHMENTS:

1. Resolution
2. Executive Assistant Job Description

RESOLUTION NO. 2019 -

RESOLUTION AUTHORIZING REPLACING THE FIRE DEPARTMENT SECRETARY POSITION WITH AN EXECUTIVE ASSISTANT IN THE FISCAL YEAR 2019-20 BUDGET

WHEREAS, The San Bruno Fire Department has 35 fulltime sworn positions and one non-sworn (Secretary) position. These positions include the Fire Chief, four Battalion Chiefs, nine Fire Captains and twenty-one Firefighters. The Department operates on 24 hours a day, 7 days a week basis; and

WHEREAS, After the review of the administrative needs of the Fire Department as it relates to the current operations, programs, responsibilities and anticipated future challenges, the Department is recommending replacing the current budgeted Secretary position with an Executive Assistant position; and

WHEREAS, the addition of the Executive Assistant Position will improve Department operations by providing analytical and technical support to the Fire Chief and Battalion Chief's responsible for managing Fire Department Programs which include Operations, Administrative, Training and Prevention/Emergency Management; and

WHEREAS, the addition of the executive Assistant Position will provide administrative support for the Fire Prevention Bureau by making appointments and maintaining a calendar, scheduling and arranging meetings; and

WHEREAS, the annual fully loaded cost for the Executive Assistant position at Step 3 is \$109,423, which is cost neutral with the current budgeted Secretary position. Therefore, no additional funds are being requested at this time; and

NOW THEREFORE, BE IT that the San Bruno City Council hereby adopts replacing the Secretary Position with an Executive Assistant.

---oOo---

I hereby certify that foregoing **Resolution No. 2019 -**
was introduced and adopted by the San Bruno City Council at a regular meeting on
September 24, 2019, by the following vote:

AYES: Councilmembers:

NOES: Councilmembers:

ABSENT: Councilmembers:

Melissa Thurman, CMC
City Clerk



Position Description

EXECUTIVE ASSISTANT

DEFINITION

Performs a variety of highly responsible, complex and confidential clerical, technical administrative and secretarial duties for a department or division.

This position is the advanced journey-level class in the City's clerical support job series. Positions at this level are distinguished from other positions within the clerical support series by the level of responsibility assumed and the complexity of duties assigned. Employees perform the most difficult and responsible types of duties assigned to classes within this job series, including providing administrative support to a department or division in areas such as budget, personnel or a department program or function, as well as providing responsible secretarial support to management and professional staff which requires frequent use of tact, discretion, initiative and independent judgment. Employees at this level are required to be fully trained in all procedures related to assigned areas of responsibility, may be required to perform department-related special administrative assignments or projects, and may act as the Office Manager within the department.

SUPERVISION RECEIVED

Works under the general supervision of assigned division or department head.

SUPERVISION EXERCISED

May exercise direct supervision over other clerical and technical personnel including, temporary or other staff, as assigned.

ESSENTIAL DUTIES AND RESPONSIBILITIES (Illustrative only)

Performs a wide variety of complex, responsible and confidential secretarial and administrative duties for department heads, division heads and other management personnel, including providing routine analytical and technical support.

Provides administrative support to the department or division head including the organization and maintenance of various administrative, reference, and follow up files; makes appointments and maintains a calendar, schedules and arranges for meetings

and makes travel and meeting related arrangements; receives and screens visitors and telephone calls; organizes meetings by notifying participants, makes room arrangements, and prepares and posts agendas and required informational materials, attends meetings and prepares minutes; follows up on projects, transmits information, and keeps informed of pertinent activities.

Works closely with the department or division head to maintain an in-depth awareness and knowledge of the departments and City's issues, philosophies, goals and objectives and assists in developing related strategies; works collaboratively with other City staff.

Provides information and resolves moderately complex complaints which regularly requires the use of judgment and the interpretation and application of policies and procedures.

Uses current computer software programs to prepare drafts of documents and finished documents; reviews finished materials for completeness, accuracy, format, compliance with policies and procedures, and appropriate English usage.

Independently responds to letters and general correspondence not requiring the attention of professional personnel.

Researches, compiles and summarizes a variety of informational or statistical materials from multiple sources; prepares a variety of studies, reports and related information for decision-making purposes.

Provides assistance to department or division head in meeting management; assembles background materials, prepares agendas, and records action items from various meetings.

Composes drafts of correspondence, confidential communications, speeches, presentations, resolutions, ordinances, contracts, administrative policies, and other materials independently or from brief notes.

Investigates and follows-up on citizen requests for service, complaints, and requests for information.

Performs basic accounting and bookkeeping functions related to ordering supplies, equipment and services. Orders and purchases supplies for department or division; tracks projects and prepares invoices from file documents.

Performs clerical duties related to department or division activities such as typing, filing, and distributing mail.

Organizes and maintains various administrative, reference, and follow-up files; purges files as necessary; organizes and maintains data base files.

Plans, assigns, reviews trains, and evaluates the work of assigned clerical personnel; provides input on and conduct employee performance evaluations.

Organizes own work, sets priorities and meets critical deadlines.

Performs related duties as assigned.

PERIPHERAL DUTIES

Attends seminars and workshops related to administrative duties and responsibilities.

May serve as a member of employee focus groups.

May represent the City at various meetings and functions.

Operates a vehicle to carry out assigned duties.

May perform cashier function including receipt of payments and various other payments, and post monies to appropriate accounts.

May issue routine non-technical permits as authorized.

MINIMUM QUALIFICATIONS

Knowledge of:

Office administrative and secretarial practices and procedures such as business letter writing; organization and function of city government, including role of elected officials and appointed board and commission members; correct English usage, including spelling, grammar, punctuation and vocabulary; office management techniques and budgeting principles and practices.

Skill in:

Operation of listed tools and equipment;

Ability to:

Provide varied, responsible and confidential secretarial and administrative support for a department or division head and administrative staff; interpret, apply and explain complex policies and procedures; use tact, discretion, initiative and independent judgment within established guidelines; research, compile and summarize a variety of informational materials; compose correspondence independently or from brief instructions; accurately record and maintain records; establish and maintain effective working relationships with employees, supervisors, other departments, officials and the public; recommend solutions and adopt effective courses of action; plan, organize, and

schedule priorities in the office; communicate effectively verbally and in writing; and some positions may be required to type accurately at speeds up to 60 words per minute.

Education and Experience:

Any combination of experience and training that would provide the required knowledge and abilities is qualifying. A typical way to obtain the required knowledge and abilities would be:

Graduation from a high school or equivalent; an Associate's Degree or advanced clerical training is highly desirable but not required, and a minimum of at least three (3) years of related experience in a high level secretarial position with responsibility for providing administrative support to executive and/or political personnel.

SPECIAL REQUIREMENT

Possession of and maintenance of a California Driver's License.

TOOLS AND EQUIPMENT USED

Typewriter, personal computer, including word processing, spreadsheet and data base software; personal computer or computer terminal; 10-key calculator; phone; copy machine; fax machine and other modern office equipment.

PHYSICAL DEMANDS

The physical demands described here are representative of those that must be met by an employee to successfully perform the essential functions of this job. Reasonable accommodations may be made to enable individuals with disabilities to perform the essential functions.

While performing the duties of this job, the employee is frequently required to sit and talk and hear, use hands to finger, handle, feel or operate objects, tools, or controls; and reach with hands and arms. The employee is occasionally required to walk.

The employee must occasionally lift and/or move up to 25 pounds. Specific vision abilities required by this job include close vision and the ability to adjust focus.

WORK ENVIRONMENT

The work environment characteristics described here are representative of those an employee encounters while performing the essential functions of this job. Reasonable accommodations may be made to enable individuals with disabilities to perform the essential functions.

The noise level in the work environment is usually quiet, but can be moderately noisy depending on specific location. The work environment can be quite hectic and requires an ability to perform multiple tasks and maintain considerable tact when dealing with others, regardless of how they treat you.

SELECTION GUIDELINES

Formal application, rating of education and experience; oral interview and reference check; job related tests may be required.

The duties listed above are intended only as illustrations of the various types of work that may be performed. The omission of specific statements of duties does not exclude them from the position if the work is similar, related or a logical assignment to the position.

The job description does not constitute an employment agreement between the employer and employee and is subject to change by the employer as the needs of the employer and requirements of the job change.

Effective Date: July 28, 2009
Resolution Number: 2009-86
Revision History:



**City Council Agenda Item
Staff Report**

CITY OF SAN BRUNO

DATE: September 24, 2019
TO: Honorable Mayor and Members of the City Council
FROM: Marc Zafferano, City Attorney
PREPARED BY: Lance Bayer, Special Counsel
SUBJECT: Adopt Resolution Denying and Revoking Cardroom Work Permit

BACKGROUND:

San Bruno Municipal Code section 4.36 generally regulates the business of card rooms in the City. Section 4.36.100 requires employees of such card rooms to obtain a work permit from the City, which is issued following an investigation by the police department. Work permits must be renewed periodically, and the Chief of Police may revoke or deny a permit under circumstances specified in the code.

On June 25, 2019, San Bruno Police Chief Ed Barberini notified Truc Xuan Ma in writing of the denial of her application for a regular cardroom work permit. Ms. Ma submitted an appeal of Chief Barberini's decision. On September 10, 2019, the City Council heard the appeal of Truc Ma from the denial of a cardroom work permit. Ms. Ma presented the appeal. The City Council considered the staff report presented by Special Counsel Lance Bayer, testimony by Ms. Ma, and oral argument. At the conclusion of the appeal hearing, the Council voted to deny the appeal.

DISCUSSION:

In January of 2018, Truc Ma applied for a cardroom work permit as a chip runner. She was issued a temporary cardroom work permit pending a Police Department background check.

In 2015, Truc Ma was arrested for selling a pipe used to smoke a controlled substance. At the time of her arrest, she provided false information to a peace officer regarding her date of birth. This conduct violated the provisions of Penal Code section 148.9. In addition, at the time of her application, Ms. Ma failed to inform the San Bruno Police Department of the arrest and charge of providing false information to a peace officer. The San Bruno Police Department investigated and reviewed Ms Ma's conduct resulting in her arrest and determined that her conduct involved acts of dishonesty and moral turpitude. This conduct subjected Ms. Ma to denial of her application for a cardroom work permit per sections 4.36.065, 4.36.100, and 4.36.110 of the San Bruno Municipal Code.

On September 10, 2019, the City Council heard the appeal of Truc Ma from the denial of her application for cardroom work permit. Ms. Ma presented the appeal. The City Council considered the staff report presented by Special Counsel Lance Bayer, testimony by Ms. Ma,

ITEM 5.g.

and oral argument. Ms. Ma admitted to providing false information to a peace officer. At the conclusion of the appeal hearing, the Council voted to deny the appeal.

RECOMMENDATION:

Staff recommends that the City Council adopt a resolution denying Truc Ma's appeal.

ATTACHMENTS:

1. Resolution Denying Revocation of Cardroom Work Permit of Truc Ma.

RESOLUTION NO. 2019 - ____

RESOLUTION DENYING AND REVOKING CARDROOM WORK PERMIT

WHEREAS, In January of 2018, Truc Ma applied for a cardroom work permit as a chip runner.

WHEREAS, In 2015, Truc Ma lied to a peace officer at the time of her arrest for possession of a device used for smoking controlled substances and provided the officer with a false date of birth.

WHEREAS, Truc Ma engaged in conduct that violated Penal Code section 148.9 and involved acts of dishonesty and moral turpitude.

WHEREAS, On June 25, 2019, San Bruno Police Chief Ed Barberini notified Truc Ma in writing of the denial of her application for cardroom work permit for the reasons stated in the staff report dated September 10, 2019 and attachments thereto, which include the fact that Ms. Ma provided false information to a peace officer when she was arrested in 2015, conduct that involved dishonesty and moral turpitude, in violation of section 4.36 of the San Bruno Municipal Code;

WHEREAS, On September 10, 2019, the City Council heard the appeal of Truc Ma from the denial of her application for cardroom work permit. Ms. Ma presented the appeal on her behalf; special counsel Lance Bayer presented the appeal on behalf of the Chief of Police. Ms. Ma admitted that she had engaged in the offending conduct in 2015 specified above.

WHEREAS, At the conclusion of the appeal hearing, the Council voted to deny the appeal.

NOW, THEREFORE, BE IT RESOLVED that the appeal of Truc Ma from the denial her application for cardroom work permit is denied.

ATTEST:

Melissa Thurman, CMC
City Clerk

-o0o-

I, Melissa Thurman, City Clerk, do hereby certify that the foregoing Resolution was duly and regularly passed and adopted by the City Council of the City of San Bruno this 24th day of September 2019 by the following vote:

AYES: Councilmembers: _____
NOES: Councilmembers: _____
ABSENT: Councilmembers: _____



City Council Agenda Item Staff Report

CITY OF SAN BRUNO

DATE: September 24, 2019

TO: Honorable Mayor and Members of the City Council

FROM: Jovan D. Grogan, City Manager

PREPARED BY: Keith DeMartini, Finance Director

SUBJECT: Hold Public Hearing, Adopt a Resolution Adjusting the Affordable Housing Impact Fees for Residential and Nonresidential Development Projects

BACKGROUND

On December 13, 2016, the City Council adopted Resolution No. 2016-115 establishing affordable housing impact fees for residential and nonresidential development projects and establishing a standardized list of uses and exemptions with respect to the payment of nonresidential housing impact fees. These impact fees were established to implement the City's adopted 2015-2023 Housing Element. The Housing Element includes programs 3-J and 5-J that required the City to adopt new strategies to increase the supply of affordable housing in the City, including the adoption of affordable housing impact fees for residential and non-residential development projects.

The related ordinance (Chapter 12.230 of the Municipal Code) imposes affordable housing impact fees and rules for construction of affordable units, alternatives to comply with the ordinance, and the use of the fees. The ordinance applies to residential ownership or rental developments of five (5) new units or more, and all nonresidential developments throughout the City. The ordinance excludes fees for public uses such as hospitals and community facilities and quasi-public uses such as child care centers, community facilities, churches and schools.

Residential rental and commercial projects are required to pay impact fees, with the possibility of an alternative compliance method, such as the provision of affordable units on- or off-site, with the approval of the City Council. Residential for-sale projects are required to include affordable units within the project, with the possibility of an alternative compliance method, such as the payment of residential impact fees, with the approval of the City Council.

ITEM 6.a.

Table 1 shows the fee levels for residential impact fees and commercial linkage fees for each development type that were adopted in the original resolution and referred to in the associated ordinance. All fees are assessed per square foot of net new residential/gross floor area.

Table 1. Adopted Impact Fees

Unit Type	Fees per Square Foot	Minimum Project Size
Single Family	\$27.00	Residential projects with 5 or more net new units
Condominium	\$25.00	
Apartment	\$25.00	
Office	\$12.50	All new commercial development
Retail	\$6.25	
Hotel	\$12.50	

DISCUSSION

All City fees, including the affordable housing impact fees, are included in the Master Fee Schedule posted on the City’s website. Section 12.230.040A of the Municipal Code allows the City Council to consider a resolution to adjust the affordable housing impact fees from time to time. The adjustment shall be based on the percentage increase in the Engineering News-Record Construction Cost Index for San Francisco, California. The affordable housing impact fees have not been adjusted since the resolution was adopted on December 13, 2016.

According to the Engineering News-Record Construction Cost Index for San Francisco, California, the rate increase from 2016 to 2017 was 2.74%. And the rate increase from 2017 to 2018 was 2.02%. The cumulative, effective rate increase from 2016 to 2018 is 4.82%.

Table 2 shows the adjusted fee levels for residential impact fees and commercial linkage fees for each development type to bring the fees in line with the indexing as of the end of 2018 which is 4.82%.

Table 2. Adjusted Impact Fees

Unit Type	Adopted Fees	Adjusted Fees	Change
Single Family	\$27.00	\$28.30	\$1.30
Condominium	\$25.00	\$26.21	\$1.21
Apartment	\$25.00	\$26.21	\$1.21
Office	\$12.50	\$13.10	\$0.60
Retail	\$6.25	\$6.55	\$0.30
Hotel	\$12.50	\$13.10	\$0.60

Resolution No. 2016-115 adopted in 2016 including the ability to automatically index the fees if no City Council action was taken. The attached resolution eliminates the

automatic indexing so that the City Council can receive a report and recommended rate increase for the affordable housing and all other development impact fees during the annual budget cycle going forward.

Additionally, the City has initiated participation in an update to the commercial linkage fee feasibility (or nexus) study. This is being completed with the County's 21 Elements, a multi-jurisdictional planning collaborative. The study will examine the feasibility of modifying the fees for new commercial development to further support increasing the supply of affordable housing in cities throughout San Mateo County. The City participated in a similar effort that started back in 2015 and was completed in 2016 and provided the required legal basis for the adoption of the current fees.

The adjusted fees will be applicable for all development projects that are currently in the development pipeline, where a building permit has not been issued and the fee has not yet been paid. As of the date of this staff report, there are no pending building permit applications submitted with the City where the development project is subject to paying the affordable housing impact fee. However, the current development pipeline suggests the City could receive several million dollars in affordable housing impact fees in the upcoming 5 fiscal years in the current economic cycle. These amounts will vary widely over time based on the level of development activity.

The adopting Resolution states that the "fees shall be paid that are in effect at the time of issuance of the building permit for the residential development, based on the effective rate at the time of building permit issuance." in accordance with Government Code Section 66019, a public hearing is required in order to receive the fee adjustment resolution to adopt. If the City Council adopts the resolution to adjust the fees, the new fees would take effect 60 days after the adoption date.

FISCAL IMPACT

The exact amount of the fiscal impact is unknown due to the fact that it is dependent on the amount and type of development projects that are submitted to the City and ultimately approved and constructed. As stated above, the current development pipeline suggests the City could receive several million dollars in affordable housing impact fees in the upcoming 5 fiscal years in the current economic cycle. These amounts will vary widely over time based on the level of development activity.

ALTERNATIVES

1. Do not adopt the resolution and provide direction to staff.
2. Adopt a resolution adjusting the fee by a rate lower than prescribed in the Engineering News Record Construction Cost Index.

RECOMMENDATION

Hold Public Hearing, Adopt a Resolution Adjusting the Affordable Housing Impact Fees for Residential and Nonresidential Development Projects

DISTRIBUTION

None

ATTACHMENTS

1. Resolution

DATE PREPARED

August 10, 2019

RESOLUTION NO. 2019-_____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SAN BRUNO ADJUSTING THE AFFORDABLE HOUSING IMPACT FEES FOR RESIDENTIAL AND NONRESIDENTIAL DEVELOPMENT PROJECTS

WHEREAS, on April 14, 2015 the City Council of the City of San Bruno adopted its 2015-2023 Housing Element. The Housing Element includes programs 3-J and 5-J that required the City to adopt new strategies to increase the supply of affordable housing in the City, including the adoption of affordable housing impact fees for residential and non-residential development projects;

WHEREAS, to implement the affordable housing goals, policies and programs of the City's 2015-2023 Housing Element, the City Council has adopted an Ordinance (Chapter 12.230 of the Municipal Code) and the related Resolution No. 2016-115 that, among other things, authorizes the imposition of affordable housing impact fees for certain residential and nonresidential development projects to mitigate the impact of such projects on the need for affordable housing in the City (the "Affordable Housing Ordinance");

WHEREAS, the Affordable Housing Ordinance authorizes the City Council to adopt by Resolution affordable housing impact fees for residential and nonresidential development, and the City Council desires to adjust these fees for the first time since their original adoption date on December 13, 2016. The adjustment is calculated based on the Engineering News-Record Construction Cost Index for San Francisco, California, as required Resolution No. 2016-115. The effective rate increase from 2016 to 2018 is 4.82%;

WHEREAS, at least ten days prior to the date this resolution is being heard, data was made available to the public indicating the amount of cost, or estimated cost, required to provide the service for which the fee or service charge is levied and the revenue sources anticipated to provide the service, including general fund revenues, in accordance with Government Code Section 66019;

WHEREAS, at least fourteen days prior to the date this resolution is being heard, notice was provided to any persons or organizations who had requested notice, in accordance with Government Code Section 66019;

WHEREAS, notice of the hearing on the proposed fee was published twice in the manner set forth in Government Code Section 6062a as required by Government Code Sections 66004 and 66018; and

WHEREAS, the City Council has reviewed the information contained in this Resolution and the accompanying staff report and any attachments at a meeting held on September 24, 2019.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of San Bruno as follows:

1. The foregoing recitals are true and correct and incorporated into this Resolution by this reference.

2. The City Council hereby adopts those affordable housing impact fees for residential development projects shown on Exhibit "A", attached hereto and incorporated by reference herein.

3. The City Council hereby adopts those affordable housing impact fees for nonresidential development projects shown on Exhibit "B", attached hereto and incorporated by reference herein.

4. All affordable housing impact fees collected shall be deposited into the City's Affordable Housing Fund to be used to increase and preserve the supply of housing affordable to households of extremely low, very low, low, and moderate incomes (including necessary administrative costs).

5. Adoption of this Resolution is exempt from the California Environmental Quality Act because the adoption of this resolution is not a project, in that it is a government funding mechanism which does not involve any commitment to any specific project (CEQA Guidelines Section 15378(b)(4)), and because it can be seen with certainty that there is no possibility that the fees may have a significant effect on the environment, in that this resolution contains no provisions modifying the physical design, development, or construction of residences or nonresidential structures (CEQA Guidelines Section 15061(b)(3)).

6. This Resolution shall take effect on November 25, 2019, 60 days after its adoption.

THE FOREGOING RESOLUTION WAS DULY AND REGULARLY ADOPTED by the City Council of the City of San Bruno, at a regular meeting of said City Council held on September 24, 2019, by the following vote:

AYES:

NOES:

ABSENT:

APPROVED AS TO FORM:

ATTEST:

Marc Zafferano
City Attorney

Melissa Thurman, CMC
City Clerk

EXHIBIT A

Housing Impact Fees for Residential Development Projects

Type of Residential Unit	Adjusted Fee per Square Foot of Net New Residential Floor Area
Single-Family Detached Home	\$28.30
Apartments and Condominiums	\$26.21

*Residential impact fees for residential projects shall be calculated using the net new square footage of Residential Floor Area for the dwelling unit to which the housing impact fee relates.

As used in the Nexus Study, "Residential Floor Area" for Single-Family Detached Homes, Townhomes, Duplexes, and Triplexes includes all horizontal areas of the several floors of such buildings measured from the exterior faces of exterior walls or from the center line of party walls separating two (2) buildings, minus the horizontal areas of such buildings used exclusively for parking.

As used in the Nexus Study, "Residential Floor Area" for Apartments and Condominiums includes all horizontal areas of the several floors of such buildings measured from the exterior faces of exterior walls or from the center line of party walls separating two (2) buildings, minus the horizontal areas of such buildings used exclusively for parking, elevators, stairwells or stairs between floors, hallways, and between-unit circulation.

Fees shall be paid that are in effect at the time of issuance of the building permit for the residential development, based on the effective rate at the time of building permit issuance.

EXHIBIT "B"

Commercial Linkage Fees for Nonresidential Development Projects

Nonresidential Use	Adjusted Fee per Square Foot of Net New Gross Floor Area
Hotel	\$13.10
Retail, Restaurants and Services	\$6.55
Office, Medical Office and Research and Development Uses	\$13.10

*All commercial linkage fees for nonresidential projects, including new construction and conversion of a residential use to a nonresidential use shall be calculated using the gross floor area of net new nonresidential space, excluding structured parking.



City Council Agenda Item Staff Report

CITY OF SAN BRUNO

DATE: September 24, 2019

TO: Honorable Mayor and Members of the City Council

FROM: Jovan D. Grogan, City Manager

PREPARED BY: Joanne Magrini, Community Services Director

SUBJECT: Authorize Modifications to the Florida Avenue Park Master Plan

EXECUTIVE SUMMARY:

In September 2018, City staff posted notices for removal on two heritage trees located at the Florida Avenue Park site. Within the 10-day notice period, three appeals of the removal were received. This report details the results of the appeal process and the settlement terms that were reached by the City of San Bruno and the appellants. Through this report, staff requests that the City Council authorize modifications to the Florida Avenue Park Master Plan, with the plan that staff will return to Council with final design drawings and cost estimates, which are projected to take two to three months.

BACKGROUND:

On October 28, 2014, the City Council adopted Resolution 2014-108 authorizing the City Manager and City Attorney to complete the purchase of 324 Florida Avenue for future use as a neighborhood park using \$600,000 from the City's Park In-Lieu Fund. The property consists of eight parcels (approximately ½ acre in total land area) located in a relatively dense residential neighborhood. At the time of acquisition, the property included four buildings: the main residence, a duplex, a garage and a workshop. A large adjacent yard was primarily undeveloped with various types of trees and other vegetation including two heritage trees (exceeding 30" in diameter). The condition of the property had fallen into disrepair due to lack of upkeep and vandalism, and had become a public nuisance.

On May 24, 2016, the City Council authorized a contract for remediation of the site, which included the demolition of the buildings on site, proper removal and disposal of contaminated soil and asbestos associated with the property, grading, temporary security of the site with fencing, and salvage of a hand-carved solid mahogany wooden tableau found in the workshop.

Concurrently with the demolition and remediation of the property, an extensive community engagement process was conducted with the residents surrounding the future park site. The neighborhood was invited to attend meetings to provide input on their preferred design and amenities for their future park. At the first meeting in June 2016, residents were asked to articulate their vision for the park and share concerns regarding neighborhood impacts, safety, and maintenance. Residents were also asked their preferences regarding placement of various park elements such as play areas and features for young children, teens and adults, social and

gathering areas, spaces for solitude and quiet contemplation, and open space and natural elements. Following the first neighborhood meeting, a single concept plan was developed for the proposed park.

The second neighborhood meeting was held in August 2016, at which time the concept plan was introduced to the public. Meeting participants/area residents were very pleased with the design and expressed a strong interest in seeing the park constructed in a manner consistent with the presented design concept. On October 25, 2016, City Council approved the Florida Avenue Draft Master Plan, park design concept, and the architectural design services contract for the design phase. The design phase included civil and architectural design including grading plans, planting plans and plant palette, landscape layout, materials plans, irrigation area plans, lighting and electrical plans, and equipment selection such as play equipment, benches, trash receptacles, and picnic tables.

On August 22, 2017, the City Council approved a construction contract with Star Construction, Inc. for the Florida Avenue Park Project. During the project submittal phase, the Contractor identified and requested changes and clarifications related to the design and construction documents. The City did not issue the notice to proceed for the construction work and decided to terminate the contract before work commenced in November 2017. Since that time, City staff has worked with the landscape architecture firm to identify various revisions to construction documents and work through various concerns that were raised about the design and the site.

One of the more significant concerns involved the potential removal of two remaining/heritage trees on the site to protect public health and safety and avoid newfound/increased maintenance costs. This decision is linked to the preferred layout of the park. City staff brought this issue to the attention of the Parks and Recreation Commission at their regular Commission meeting held on September 19, 2018 and received a recommendation on the preferred layout in order to move forward with preparation of final design plans/specifications as well as preparation of the site and documents to rebid the project for construction. Specifically, the Commission recommended to proceed with the recommendation of staff to remove both trees.

Following the Park and Recreation Commission meeting, staff posted the trees for removal in accordance with Municipal Code §8.25.020, which requires a 10 day noticing period. Within the 10-day period, three appeals were received by City staff: one by the City Clerk's office; one by the City Manager; and one by the Community Services Director. All three appeals included the same information and were submitted by the same group of 18 citizens.

The appeal hearing was initially scheduled for October 23, 2018, and was rescheduled to November 13, 2018 at the request of one of the listed appellants. At that meeting, City staff gave a presentation recommending the following five listed design changes to the Florida Park Master Plan that were recommended by City staff and the City's landscape architecture firm:

1. Revise design of walkway area along San Anselmo Ave (i.e., maintain existing walkway alignment and replace mulch at the tree planting strip with decomposed granite).
2. Simplify planting inside the play area fence (i.e., eliminate proposed vines on fence, delete two irrigation valves and associated equipment, limit plants and shrubs to varieties that are not easily trampled).
3. Revise fence plan between the park and adjacent residence (i.e., change proposed property line fence from decorative metal to solid six-foot wood fence).

4. Revise irrigation and electrical plans (i.e., amend plans related to service meter dimensions and change irrigation design to revise quick coupler union PVC grade).
5. Remove the existing pine and cedar trees.

Staff's presentation focused on the removal of the two heritage trees and the written reports prepared by two independent, certified arborists that the City hired to evaluate the trees within the context of the approved Florida Park Master Plan : Tree Management Experts and Kielty Arborist Services. The arborists concluded that construction and use of the park would impact the trees, which would result in diminished health over the next several years. Both arborists determined that although the form of the cedar tree was poor and permanently disfigured, it did not present a hazard. Although it was likely that the health of the cedar would be impacted due to construction of the park, the root damage is estimated to be moderate at 10-15%.

Upon evaluation of the pine tree both arborists indicated that there would be significant safety risks as well as maintenance costs making it unsuitable for retention. The reports indicated that the pine produces large 2-3 pound seed pods, which could fall without warning from 50 to 80 feet, potentially causing serious harm or death. In addition to the safety risk the pine tree poses, there would be increased maintenance issues and costs related to the play equipment due to the honeydew excretions and falling pine needles. Lastly, there is potential impact to the root structure during construction, as root loss will be between 25-30% for the Norfolk Island Pine.

Based on this information, staff recommended that the City Council authorize the five changes to the current design, including removal of the two existing heritage trees. The estimated time line for these changes was: three to five weeks to revise the design and specifications, a three to six month process to rebid and select a contractor, construction start estimated to begin in the first quarter of 2019, and anticipated completion by the third quarter (July-Sept. 2019).

The Appellants made a presentation recommending that the trees remain in the park, noting that the trees were healthy, that the original park design had been altered, and that any public safety risk was overstated or could be mitigated. Oral testimony was given by 13 individuals supporting the Appellants' recommendation.

The City Council then deliberated, and several Council members noted that they had received phone calls and emails from residents in support of removing the trees. The Council also noted that this is not an environmental impact issue, but really just a neighborhood dispute and that people supporting the removal of the trees did not want to come in person to exacerbate the situation, so they provided feedback via email and phone calls to the Council members instead. A motion was made and seconded to deny the appeal and uphold the staff's recommendation, which passed on a 4-0 vote. Staff indicated that a formal resolution with findings would be presented to the City Council at a later date.

Following the City Council's denial of the appeal, the attorney representing the appellants ("Friends of Florida Park") stated that they would sue the City based on alleged violations of the California Environmental Quality Act ("CEQA") if the trees were removed. To avoid a potentially lengthy delay caused by litigation, the City agreed to defer removing the trees, and met with the appellants and their attorney to discuss whether the matter could be resolved without significantly reducing the useable area of the park, unwarranted expense to relocate or redesign its key features, or unsustainable maintenance requirements. Meetings were scheduled for

December 19, 2018 and January 11, 2019. On March 30, 2019, the City hosted a community event at the park location to obtain input from residents regarding the alternatives. During the gathering, a member of the public asked if the pine were removed, could the City plant a large tree in its place. Staff did some research and obtained information from nurseries about whether a mature native tree such as a coast live oak could be purchased, and for what cost. On May 2, 2019, staff again met with the appellants to explore this idea, which was favorably received.

Following several months of discussions, the City and appellants reached a settlement in which the City would remove the pine and retain the cedar. The City would agree to plant a mature coast live oak in the park in place of the pine. The oak would be approximately 20-25' high and about 18' in breadth. The appellants requested that the City pay their attorney fees to resolve the threat of litigation, and because CEQA allows recovery of attorney's fees in court, the City also agreed to pay the appellants' attorney \$15,000. In exchange, the appellants would withdraw their appeal and release the City from all related claims. Per the settlement, the City would consider a revised plan for the park that includes the oak in place of the pine, or in another suitable location within the park.

As a result of the appeal, and to prepare for potential litigation, the City commissioned a historical resources impact analysis by Page & Turnbull. Their report determined that the proposed project was found not to have any impacts on historic resources, and the finding applies whether the park includes or does not include the heritage trees. A biological study was also performed by WRA to address site conditions at the Florida Avenue Park Project site. Their report documented the habitat conditions and the very limited potential for special-status species to be present at the site. The reports are attached to this document as Attachments 1 and 2. Despite these findings, and while the City was confident in its legal position, if the City were sued, litigation could consume several years and cost substantial five- or six-figure sums, delaying the project and making it more costly to build due to construction cost escalation.

DISCUSSION:

The proposed action at this meeting, which conforms to the settlement between the City and the appellants, is for the City Council to review the revised conceptual Florida Park Master Plan prepared by the City's consultant, MIG, and approve it by motion (Attachment 3). The revised design retains all of the park's previously-approved features, makes the modifications previously suggested by staff at the November 2018 meeting, and retains the cedar and removes the pine, which would be replaced with a mature coast live oak.

Approval of the modifications to the Plan is exempt under CEQA's "common sense" exemption as well as the exemption for minor alterations of land, and no exceptions to the exemptions apply, as documented in the reports prepared by the City's arborists, historical resource consultant, and biological resource consultant. The planned removal and replacement of the pine with a mature coast live oak is consistent with the City's Heritage Tree ordinance, and is not intended to be a specific measure to mitigate any impacts from the project.

If the City Council approves the revised conceptual design, MIG will prepare detailed drawings consistent with the approved design suitable for bidding, which will be presented to the City Council for approval by resolution at a later date. It is anticipated that the drawings could be completed by the first quarter of 2020, with a formal bid process to occur thereafter. Park construction could begin in mid-2020.

FISCAL IMPACT:

The FY2018-19 Capital Improvement Program Budget includes a total available appropriation for Florida Avenue Park site demolition and clean-up, planning, design, and construction. Costs expended by the City to mitigate potential City liability and resolve the threat of litigation are estimated to total \$76,000 and include approximately \$5,000 for the Page and Turnbull historical assessment, \$11,000 for the WRA biological assessment, arborist costs of \$3,978, City outside counsel fees of \$21,000, fees paid to the appellants' attorney of \$15,000, and approximately \$20,000 for the coast live oak that will replace the Pine tree. Due to the delay in construction of the park, there will likely be additional expenses associated with cost escalation for construction of the park. City staff are exploring additional funding sources to support the increased costs. At this time, staff requests that the City Council authorize modifications to the Florida Avenue Park Master Plan, and staff will return to Council with final design drawings and cost estimates, which are projected to take two to three months.

ALTERNATIVES:

1. Recommend changes in the conceptual design and direct staff to return at a later meeting.

RECOMMENDATION:

Authorize Modifications to the Florida Avenue Park Master Plan. Staff will return to the City Council with final design drawings and cost estimates, which are projected to take two to three months.

DISTRIBUTION:

1. None

ATTACHMENTS:

1. Page & Turnbull Historical Resources Impact Analysis
2. WRA Biological Technical Memorandum
3. Conceptual Master Plan

DATE PREPARED:

September 16, 2019

324 FLORIDA AVENUE PARK SITE
HISTORIC RESOURCES IMPACT ANALYSIS

SAN BRUNO, CALIFORNIA
[18407]

PREPARED FOR:
CITY OF SAN BRUNO



PAGE & TURNBULL

imagining change in historic environments through design, research, and technology

FEBRUARY 22, 2019

FINAL

ATTACHMENT 1

ARCHITECTURE
PLANNING & RESEARCH
PRESERVATION TECHNOLOGY

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I. INTRODUCTION

This Historic Resources Impact Analysis has been prepared at the request of the City of San Bruno for the proposed park site at 324 Florida Avenue (APN 020-366-050, 020-366-060, 020-366-070, 020-366-080, and 020-366-090) (**Figure 1 and Figure 2**). The irregularly-shaped subject parcel is comprised of five unimproved parcels located a few blocks southeast of San Mateo Avenue (San Bruno’s commercial corridor), and adjacent to the identified Cupid’s Row Historic District, which was previously determined eligible for listing in the California Register of Historical Resources. The subject block is bounded by Taylor Avenue and Florida Avenue to the south, Martin Place to the west, and San Anselmo Avenue North to the east. The subject property previously accommodated four buildings including a single-family residence, a residential duplex, an auto garage, and a workshop; all of these buildings were recently demolished.

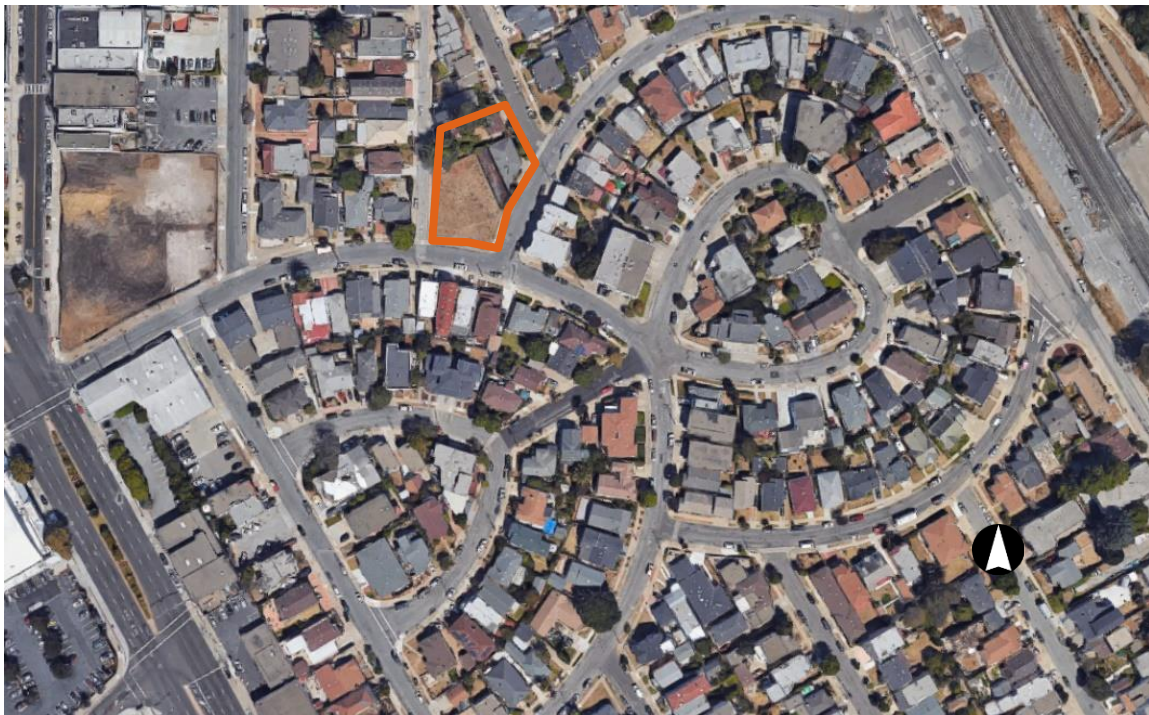


Figure 1: Aerial image of subject block and adjacent double heart-shaped street plan with property outlined in orange. Note that the aerial image depicts the subject parcel developed with buildings that have since been demolished. Source: Google Maps, 2019. Edited by Page & Turnbull.



Figure 2: Assessor's map of the subject block and adjacent double heart-shaped street plan. The subject parcel is highlighted in orange. Source: County of San Mateo Assessor. Edited by Page & Turnbull. <https://www.smcacre.org/assessor-maps>

METHODOLOGY

This report provides a summary of the current historic status of the subject property, a records request overview from the Northwest Information Center of the California Historical Resources Information System, and an existing conditions description of the proposed park site at 324 Florida Avenue. The report also includes a description of the proposed Florida Avenue Park Master Plan and a project impacts analysis. The project impacts analysis identifies potential impacts on identified historic resources pursuant to the California Environmental Quality Act (CEQA), inclusive of project-specific impacts and cumulative impacts analysis.

Page & Turnbull reviewed 90 percent construction documents for the proposed Florida Avenue Park by Moore Iacofano Goltsman, Inc. and BFK Engineers, Surveyors, Planners, dated December 12, 2016; arborist reports completed by Tree Management Experts and Kielty Arborist Services in 2018; and a previous Historic Resource Evaluation report prepared by Page & Turnbull for the City of San Bruno in September 2015. All photographs in this report were taken by Page & Turnbull in February 2019, unless otherwise noted.

SUMMARY OF FINDINGS

In 2015, the four buildings at 324 Florida Avenue were determined ineligible for individual listing in the California Register of Historical Resources (California Register), but the property remained a potential contributor to the identified California Register-eligible Cupids Row Historic District. Due to the recent demolition of all buildings on the site, 324 Florida Avenue would no longer be considered a contributor to the Cupids Row Historic District. This Historic Resources Impact Analysis additionally finds that the proposed Florida Avenue Park Master Plan does not appear to impact any nearby identified historic resources under CEQA, and that the proposed park design is compatible with the Cupid's Row Historic District. These findings apply whether the final park design includes or does not include two heritage trees currently at the site.

II. EXISTING HISTORIC STATUS

324 Florida Avenue is not currently listed in the National Register or California Register and is not listed in the California Historical Resources Information System (CHRIS) San Mateo County database with a status code.

2001 SAN BRUNO REDEVELOPMENT AREA HISTORIC RESOURCE INVENTORY

The City of San Bruno created a San Bruno Redevelopment Area Historic Resource Inventory in 2001, but does not maintain a city-wide local inventory or local preservation ordinance. The San Bruno Redevelopment Plan was adopted in 1999 to improve commercial residential neighborhoods and to stimulate private investment in the oldest part of the City. Like all redevelopment agencies in California, the San Bruno Redevelopment Agency was dissolved in 2012.

The 2001 Historic Resource Inventory, produced by architectural historian Kent Seavey, surveyed the redevelopment area and identified individual properties as well as a potential historic district meeting criteria for listing in the California Register of Historical Resources. The so-called Cupid's Row Historic District, notable for its intertwined double heart-shaped street plan, is located generally between El Camino Real and Huntington Avenue just west of the Southern Pacific Railroad tracks. Platted in 1905 by Hensley Green Company as part of the San Bruno Park Third Addition, the unusual and distinctive roadways define a district with a large concentration of residential housing units constructed between 1909 and 1951. The residences were designed in a mix of architectural styles and the neighborhood is generally associated with the development of railroad/streetcar and automobile suburbs. Approximately 110 properties comprise the potential historic district, with 82 properties identified as potential contributors in 2001.¹

2015 HISTORIC RESOURCE EVALUATION

Page & Turnbull authored a Historic Resource Evaluation for 324 Florida Avenue in 2015. The summary of determination is excerpted below.

Research conducted for this report did not find supporting documentation about Henry Perroset, or his woodcarver father Francois Perroset, to demonstrate their significance as artists, craftsmen, or persons important to local, state, or national history. Despite a number of well-crafted elements at the property and continued ownership by the Perroset family since the early twentieth century, the buildings at 324 Florida Avenue do not appear to represent the work of a master or possess high artistic values. Based on the evaluation in this report, the property does not appear individually eligible for listing in the California Register.

324 Florida Avenue remains a contributor to a potential historic district identified in 2001 as eligible for listing in the California Register. Re-survey of the district is outside the scope of this report, but a cursory examination indicates that the district's 87 contributors remain substantially in place among the approximately 110 properties within the proposed boundaries of the potential district. With the high percentage of contributors remaining, the demolition of 324 Florida Avenue appears to have a less-than-significant impact on the potential district's ability to continue to be eligible for the California Register.²

¹ "Cupid's Row Historic District" in *Historical Inventory (San Bruno Redevelopment Area) 2001*, p. 1.

² Page & Turnbull, *324 Florida Avenue Historic Resource Evaluation* (September 2015).

Because the buildings at 324 Florida Avenue have been demolished, the property no longer contributes to the identified Cupid's Row Historic District.

2019 NORTHWEST INFORMATION CENTER RECORDS REQUEST

At the request of the City of San Bruno, Page & Turnbull submitted a records request to the Northwest Information Center (NWIC) for historic records (previous reports, maps, and other database information) within a 0.3-mile radius of the property at 324 Florida Avenue. The search area radius was bounded by El Camino Real, San Felipe Avenue, Huntington Avenue, Sylvan Avenue, San Mateo Avenue, and Jenevein Avenue (**Figure 3**). The results of the cultural resources record search (inclusive of both archeological resources and historical buildings and/or structures) were provided to Page & Turnbull January 25, 2019.³



Figure 3: Aerial image of records search area bound by El Camino Real (west), San Felipe Ave (south), Huntington Ave (east), Sylvan Ave (north), San Mateo Ave (north), and Jenevein Ave (north). The search area is outlined in orange and the subject property is indicated with an orange star. Source: Google Maps, 2019. Edited by Page & Turnbull.

The record search identified five cultural resources studies covering approximately 10 percent of the search area:

³ Northwest Information Center record search results for 324 Florida Avenue, File No. 18-1362 (January 25, 2019).

Report	Year	Title	Author(s)
S-017192 (OHP PRN - UMTA900828A)	1994	BART-San Francisco Airport Extension Project, Draft Environmental Impact Report/Supplemental Environmental Impact Statement, Historic Architectural Survey Technical Report	Laurence H. Shoup, Mark Brack, Nancy Fee, and Bruno Giberti; Cherilyn Widdell; Ward Hill
S-025174	2002	Cultural Resources Report for San Bruno to Mountain View Internodal Level 3 Fiber Optics Project in San Mateo and Santa Clara Counties, California	John Holson, Cordelia Sutch, and Stephanie Pau
S-026045	2000	Cultural Resources Reconnaissance Survey and Inventory Report for the Metromedia Fiberoptic Cable Project, San Francisco Bay Area and Los Angeles Basin Networks	Richard Carrico, Theodore Cooley, and William Eckhardt
S-027930	2003	Cultural Resource Assessment of Alternative Routes for PG&E's Jefferson-Martin Transmission Line, San Mateo County, California	Kyle Brown, Adam Marlow, James Allan, and William Self
S-032250	2003	Historic Property Survey Report, Mission Bells Project, State Route 82/Interstate 101, San Mateo and Santa Clara Counties, California	Philippe Lapin

None of the above listed reports include 324 Florida Avenue; study was concentrated along Huntington Avenue and El Camino Real. The search area contains no previously identified archeological resources.⁴ NWIC base maps and/or the State Office of Historic Preservation Historic Property Directory list the following non-archeological resources within the search area:

⁴ The NWIC record search results report does indicate there is a moderate potential for unrecorded Native American resources in the proposed Florida Avenue Park project area and there is a high potential for unrecorded historic-period archeological resources in the proposed Florida Avenue Park project area. Further analysis of archaeological resources is outside the scope of this report, which is focused on above-ground built historic resources.

Primary No.	Prop. #	Resource Name	Other IDs	Status Code
P-41-001496	091148	Site of Start of California Highway System	Hist. Res. SPHI-SMA-006	7L
P-41-001526	094974	389 Taylor Ave	Proj. Revw. HUD941220A	6Y
P-41-001581	098875	105 Sylvan Ave	Resource Name - 105 Sylvan Ave; OHP Property Number - 098875; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0025-0000; Other - 105-119 Sylvan Avenue	6Y
P-41-001582	098876	493 Huntington Ave	Resource Name - 493 Huntington Ave; OHP Property Number - 098876; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-026-0000; Other - 2224	6Y
P-41-001583	098877	421, 429, 437, 445, 453, 461, 469, 473, 477, 481, 485 Huntington Ave	Resource Name - 421, 429, 437, 445, 453, 461, 469, 473, 477, 481, 485 Huntington Ave; OHP Property Number - 098877; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0027-0000	6Y
P-41-001584	098878	102 Florida Ave	Resource Name - 102 Florida Ave; OHP Property Number - 098878; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0028-0000	6Y
P-41-001585	098879	104 Florida Ave	Resource Name - 104 Florida Ave; OHP Property Number - 098879; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0029-0000	6Y
P-41-001586	098880	105 Florida Ave	Resource Name - 105 Florida Ave; OHP Property Number - 098880; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0030-0000	6Y
P-41-001587	098881	381 Huntington Ave	Resource Name - 381 Huntington Ave; OHP Property Number - 098881; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0031-0000	6Y
P-41-001588	098882	365 Huntington Ave	Resource Name - 365 Huntington Ave; OHP Property Number - 098882; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0032-0000	6Y
P-41-001665	101774	285 Huntington Ave	Resource Name - 285 Huntington Ave; OHP Property Number - 101774; OHP PRN - UMTA900828A; OHP PRN - DOE-41-95-0076-0000	6Y
P-41-001876	117037	San Bruno Park, Third Addition	OHP PRN - DOE-41-96-0167-0000; OHP Property Number - 117037; Resource Name - San Bruno Park, Third Addition; OHP PRN - UMTA900828A	6Y

The above listed resource P-41-001496 (“Site of Start of California Highway System”) is located at the convergence of El Camino Real and San Mateo Avenue. The status code “7L” indicates State Historical Landmarks No. 1 through No. 769 and Points of Historical Interest designated prior to January 1998 which need to be reevaluated using current standards. All other above listed resources with the status code “6Y” have been determined ineligible for the National Register by consensus through Section 106 process (not evaluated for the California Register or Local Listing).⁵

⁵ Section 106 of the National Historic Preservation Act requires projects with a federal nexus (federal agency funding and/or ownership) to undergo a project review process to identify adverse effects on potential historic resources.

SAN BRUNO HERITAGE TREE PROGRAM

The City of San Bruno defines a Heritage Tree as: any native Bay (*Umbellularia californica*), Buckeye (*Aesculus* species), Oak (*Quercus* species), Redwood (*Sequoia sempervirens*), or Pine (*Pinus radiata*) tree that has a diameter of six (6) inches or more measured at fifty-four (54) inches above natural grade; any tree or stand of trees designated by resolution of the city council to be of special historical value or of significant community benefit; a stand of trees, the nature of which makes each dependent on the others for survival; or any other tree with a trunk diameter of ten (10) inches or more, measured at fifty-four (54) inches above natural grade. The City's Heritage Tree Ordinance declares such trees, whether located on City or private property, to be an asset to the community at large and provides penalties for removing or improperly pruning these trees.⁶ Additional information is outlined in Chapter 8.25 of the San Bruno Municipal Code: Heritage Trees.⁷

There are two trees on the subject site that the City of San Bruno determined to be Heritage Trees: a Deodar cedar tree and a Norfolk Island pine tree. Historic resource evaluations do not typically evaluate plants such as trees for historic significance unless they are part of an identified 'cultural landscape' of built and natural features that is eligible for listing in the California Register of Historical Resources. The National Park Service defines a cultural landscape as a geographic area, including both cultural and natural resources and the wildlife or domestic animals therein, associated with a historic event, activity, or person, or exhibiting other cultural or aesthetic values.⁸ There are four non-mutually exclusive types of cultural landscapes: vernacular, designed, historic site, or ethnographic. As described by The Cultural Landscape Foundation, vernacular landscapes have evolved through use by the people whose activities or occupancy shaped those landscapes. Designed landscapes were consciously designed or laid out by a landscape architect, master gardener, architect, or horticulturist according to design principles, or by an amateur gardener working in a recognized style or tradition. Historic sites are cultural landscapes significant for their association with a historic event, activity, or person. Ethnographic landscapes contain a variety of natural and cultural resources that the associated people define as heritage resources.⁹

Furthermore, the California Register's eligibility criteria require significance associated with the following criteria:

Criterion 1 (Events): Associated with events that have made a significant contribution to the broad patterns of local or regional history or the cultural heritage of California or the United States.

Criterion 2 (Persons): Associated with the lives of persons important to local, California or national history (Criterion 2).

Criterion 3 (Architecture/Design): Embodies the distinctive characteristics of a type, period, region or method of construction or represents the work of a master or possesses high artistic values.

⁶ "Trees on Private Property," City of San Bruno. Accessed February 2019,

https://www.sanbruno.ca.gov/gov/city_departments/community_services/trees/heritage_trees.htm

⁷ "San Bruno Municipal Code Chapter 8.25: Heritage Trees," City of San Bruno. Accessed February 2019, http://qcode.us/codes/sanbruno/view.php?topic=8-8_25&frames=on

⁸ National Park Service, "Management Policies 2006: Glossary" (2006) 157. Accessed February 2019, https://www.nps.gov/policy/MP_2006.pdf#page=167

⁹ "About Cultural Landscapes." The Cultural Landscape Foundation. Accessed February 2019, <https://tclf.org/places/about-cultural-landscapes>

Criterion 4 (Information Potential): Has yielded, or has the potential to yield, information important to the prehistory or history of the local area, California or the nation.

The two trees at 324 Florida Avenue were not identified as contributing to the significance of the identified Cupid's Row Historic District in Page & Turnbull's 2015 Historic Resource Evaluation. In the site's current unimproved state, the presence of two Heritage Trees at 324 Florida Avenue does not merit categorization as a cultural landscape under any of the types of cultural landscapes, nor does it meet any of the eligibility criteria of the California Register of Historical Resources. Therefore, the two Heritage Trees are not considered historic resources for the purposes of CEQA.

III. PROPERTY DESCRIPTION

SITE

The subject property consists of five unimproved parcels roughly forming a pentagonal site with street frontage at San Anselmo Avenue North to the east, Florida Avenue to the southeast, Taylor Avenue to the southwest, and Martin Place to the west. The north boundary is adjacent to a separate property. The irregularly-shaped site is generally flat and contains one Deodar cedar tree and one Norfolk Island pine tree (**Figure 4 and Figure 5**). The identification of tree species at the subject property is primarily based on the *Arborist Report* prepared by Tree Management Experts and the *Tree Assessment* prepared by Kielty Arborist Services, both in September 2018.



Figure 4: Deodar cedar tree, located along the site's Martin Place frontage. Source: Kielty Arborist Services, *Tree Assessment* (September 2018).



Figure 5: Norfolk Island pine tree, located at the north end of the property. Source: Kielty Arborist Services, *Tree Assessment* (September 2018).

SURROUNDING NEIGHBORHOOD

The property at 324 Florida Avenue is at the north edge of an unusual subdivision platted in the shape of two hearts. According to a 2001 survey of the California Register-eligible Cupid's Row Historic District,

The district, defined by its unusual and distinctive roadways, forming a pair of intertwined hearts, contains a large concentration of residential housing units constructed between 1909 and 1951. Their architectural styles range from wood shingled hipped cottages to contractor modern, indicative of architectural forms found in railroad/streetcar and automobile suburbs, of which this is an early and interesting example. Building lots are generally narrow (twenty-five feet), and deep (eighty feet) with varying, but uniform setbacks behind grassed lawns, or more modern hardscapes, with a few trees, but mostly low shrubbery for landscaping. The majority of the residences have detached garages to the rear, while some of the post-WWII [World War II] housing incorporates the garages into the building envelop.¹⁰

¹⁰ *Historical Inventory (San Bruno Redevelopment Area) 2001*, p.1.

The surrounding neighborhood is residential and includes one- and two-story single-family residences, as well as some two- and three-story multi-family apartment buildings (**Figure 6 to Figure 21**).¹¹ El Camino Real (State Route 82) is the main thoroughfare to the west of the identified historic district and the Southern Pacific Railroad tracks that now run the Caltrain commuter rail are to the east. About two miles to the southeast, east of the railroad tracks and Highway 101, is San Francisco International Airport. Despite numerous vinyl and aluminum replacement windows, and some demolished and/or renovated buildings, the Cupid's Row Historic District was evaluated as eligible for the California Register in 2001 with integrity of location, design, setting, feeling, and association.



Figure 6: 400 San Anselmo Avenue North. The property looks southwest to 324 Florida Avenue.



Figure 7: 409 San Anselmo Avenue North. The property sits directly north of 324 Florida Avenue.



Figure 8: 333 Florida Avenue. The property is a contributor to the identified Cupid's Row Historic District and looks northwest to 324 Florida Avenue.



Figure 9: 293 and 305 Florida Avenue. These properties look northwest to 324 Florida Avenue.

¹¹ Figure captions specify if the property looks onto 324 Florida Avenue. Note this applies to the property boundary, and not necessarily the orientation of the building. Figure captions also specify if the property is a contributor to the identified Cupid's Row Historic District.



Figure 10: 333 Taylor Avenue. The property is a contributor to the identified Cupid's Row Historic District and looks north to 324 Florida Avenue.



Figure 11: 349, 363, 373, and 381 Taylor Avenue. These properties look north to 324 Florida Avenue. All are contributors to the identified Cupid's Row Historic District.



Figure 12: 388 Taylor Avenue. The property is a contributor to the identified Cupid's Row Historic District and looks east to 324 Florida Avenue.



Figure 13: 25 Martin Place. The property looks east to 324 Florida Avenue.



Figure 14: 415 and 413 San Anselmo Avenue North.



Figure 15: 217 Carlton Avenue.



Figure 16: 203 and 211 Carlton Avenue, both contributors to the identified Cupid's Row Historic District.



Figure 17: 202 Carlton Ave and 148 Cupid Row, both contributors to the identified Cupid's Row Historic District.



Figure 18: 163 and 191 Florida Avenue.



Figure 19: 201 Florida Avenue.



Figure 20: 252 Florida Avenue. The property is a contributor to the identified Cupid's Row Historic District.



Figure 21: 283 and 289 Florida Avenue.

IV. PROPOSED PROJECT

Project Background

In October 2014, the San Bruno City Council adopted a resolution authorizing the City Manager and City Attorney to purchase 324 Florida Avenue for future use as a neighborhood park.¹² At the time of acquisition, the property included four buildings: a single-family residence, a residential duplex, an auto garage, and a workshop. In May 2016 the City Council authorized a contract for site remediation which included the demolition of the buildings on site.¹³ Concurrently with the demolition phase, an extensive community engagement process was conducted with residents of the surrounding area. The first community meeting was held June 2016, and residents were asked to articulate their vision for the park. A concept plan for the park was introduced at the second community meeting held in August 2016. In October 2016 the City Council approved the Florida Avenue Draft Master Plan, park design concept, and the architectural design services contract for the design phase.¹⁴

Concerns were raised regarding the potential removal of two Heritage Trees on the site (the previously described Deodar cedar and the Norfolk Island pine). Two independent arborist reports were submitted in early September 2018. The Tree Management Experts recommended that the Deodar cedar be pruned and that a Tree Protection Report be created and implemented; they also recommended that the Norfolk Island pine be removed due to risks posed from falling cones.¹⁵ Kiely Arborist Services recommended that both trees be removed.¹⁶ City staff brought the issue to the attention of the Parks and Recreation Committee at their regular Commission meeting held on September 19, 2018 and received a recommendation to move forward with the tree removal. City staff posted the trees for removal (in accordance with Municipal Code Chapter §8.25.020) and received three appeals (containing the same information and all submitted by the group “Friends of Florida Park”).

City staff developed two conceptual alternatives for the Parks and Recreation Commission to reconsider, if there was interest in retaining the Heritage Trees. Alternate 1 relocates the children’s play area away from the trees and fences off the area around the trees.¹⁷ Alternate 2 eliminates all play equipment in the park and fences off the area around the trees.¹⁸ Due to the arborist reports, site considerations, and desire to provide an appealing new park, City staff again recommended that both Heritage Trees be removed and the City Council denied the Friends of Florida Park appeal.¹⁹

Proposed Park and Playground Description

Page & Turnbull reviewed 90 percent construction documents for the proposed Florida Avenue Park by landscape architect Moore Iacofano Goltsman, Inc., and BFK Engineers, Surveyors, Planners, dated December 12, 2016. The current proposed park and playground plan is based upon initial community feedback and has been modified from the original concept due to contractor requested changes, arborist recommendations, and City staff review. Most notably, the Heritage Trees were

¹² “Florida Avenue Park: December 2018 Project Update,” City of San Bruno. Accessed February 2019, https://www.sanbruno.ca.gov/gov/city_departments/community_services/new_parks_planning_and_construction/florida_avenue_park.htm

¹³ Jovan D. Grogan, San Bruno City Manager, “City Council Agenda Item Staff Report: Appeal of the Removal of Two Heritage Trees from the Florida Avenue Park Development Site” (November 13, 2018), 1.

¹⁴ Ibid.

¹⁵ Tree Management Experts, *Arborist Report* (September 6, 2018).

¹⁶ Kiely Arborist Services, *Tree Assessment* (September 7, 2018).

¹⁷ Alternate 1 will add 2-3 months to the project timeline and may increase the project cost by \$50,000-75,000.

¹⁸ Alternate 2 will add 1-2 months to the project timeline and will be less expensive than the current/recommended design.

¹⁹ Grogan, 2.

initially going to be retained and now are proposed to be removed. Based on information conveyed by the December 2016 drawings, a written description presented on the City's website, and Jovan D. Grogan's November 2018 letter, "City Council Agenda Item Staff Report: Appeal of the Removal of Two Heritage Trees from the Florida Avenue Park Development Site," the proposed project includes the following features:

- Park entrance with signage, seating and scored stone paving;
- Fourteen new trees and new shrub landscaping;
 - Tree species to include big leaf maple, frontier elm, and swan hill olive
 - Shrub landscaping to include dwarf olive and Julia phelps ceanothus
 - Trees and shrubs dispersed throughout park and clustered in community grove and urban woodlands sections
- Children's play area at north section of park with play equipment, rubberized play surfacing, and seating;
- Adult exercise features including a cardio course and multi-generational play equipment
- Neighborhood square at central section of park with scored stone paving, varied seating, and picnic tables;
- Pedestrian paths with bench seating and score stone paving;
 - Walkway area along San Anselmo Avenue North with decomposed granite at the tree planting strip
- Great Lawn area at Florida and Taylor avenues with turf or drought tolerant grass; and
- Solid six-foot wood fence between the park and the adjacent residences

The Florida Park Master Plan documents are available at the Florida Avenue Park page on the City of San Bruno's website:

https://www.sanbruno.ca.gov/gov/city_departments/community_services/new_parks_planning_and_construction/florida_avenue_park.htm

V. CEQA FRAMEWORK FOR EVALUATION OF POTENTIAL IMPACTS

The following information has been included to understand how the provisions of the California Environmental Quality Act would relate to alterations of the subject property at 324 Florida Avenue.

CALIFORNIA ENVIRONMENT QUALITY ACT (CEQA)

The California Environment Quality Act (CEQA) is state legislation (Pub. Res. Code §21000 et seq.), which provides for the development and maintenance of a high quality environment for the present-day and future through the identification of significant environmental effects.²⁰ CEQA applies to “projects” proposed to be undertaken or requiring approval from state or local government agencies. “Projects” are defined as “...activities which have the potential to have a physical impact on the environment and may include the enactment of zoning ordinances, the issuance of conditional use permits and the approval of tentative subdivision maps.”²¹ Historic and cultural resources are considered to be part of the environment. In general, the lead agency must complete the environmental review process as required by CEQA. In the case of the proposed project at Mission Dolores Park, the City of San Francisco will act as the lead agency.

According to CEQA, a “project with an effect that may cause a substantial adverse change in the significance of an historic resource is a project that may have a significant effect on the environment.”²² Substantial adverse change is defined as: “physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historic resource would be materially impaired.”²³ The significance of an historical resource is materially impaired when a project “demolishes or materially alters in an adverse manner those physical characteristics of an historical resource that convey its historical significance” and that justify or account for its inclusion in, or eligibility for inclusion in, the California Register.²⁴ Thus, a project may cause a substantial change in a historic resource but still not have a significant adverse effect on the environment as defined by CEQA as long as the impact of the change on the historic resource is determined to be less-than-significant, negligible, neutral or even beneficial.

A property may qualify as a historic resource if it falls within at least one of four categories listed in CEQA Guidelines Section 15064.5(a), which are defined as:

1. A resource listed in, or determined to be eligible by the State Historical Resources Commission, for listing in the California Register of Historical Resources (Pub. Res. Code SS5024.1, Title 14 CCR, Section 4850 et seq.).
2. A resource included in a local register of historical resources, as defined in Section 5020.1(k) of the Public Resources Code or identified as significant in an historical resource survey meeting the requirements of section 5024.1 (g) of the Public Resources Code, shall be presumed to be historically or culturally significant. Public agencies must treat any such resource as significant unless the preponderance of evidence demonstrates that it is not historically or culturally significant.
3. Any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural,

²⁰ State of California, California Environmental Quality Act, http://ceres.ca.gov/topic/env_law/ceqa/summary.html, accessed 31 August 2007.

²¹ Ibid.

²² CEQA Guidelines subsection 15064.5(b).

²³ CEQA Guidelines subsection 15064.5(b)(1).

²⁴ CEQA Guidelines subsection 15064.5(b)(2).

- engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California may be considered to be an historical resource, provided the lead agency's determination is supported by substantial evidence in light of the whole record. Generally, a resource shall be considered by the lead agency to be "historically significant" if the resource meets the criteria for listing on the California Register of Historical Resources (Pub. Res. Code SS5024.1, Title 14 CCR, Section 4852).
4. The fact that a resource is not listed in, or determined to be eligible for listing in the California Register of Historical Resources, not included in a local register of historical resources (pursuant to section 5020.1(k) of the Pub. Resources Code), or identified in an historical resources survey (meeting the criteria in section 5024.1(g) of the Pub. Resources Code) does not preclude a lead agency from determining that the resource may be an historical resource as defined in Pub. Resources Code sections 5020.1(j) or 5024.1.²⁵

In 2001, 324 Florida Avenue was identified as a potential contributor to the California Register-eligible Cupid's Row Historic District. In 2015, 324 Florida Avenue was determined ineligible for individual listing in the California Register but was confirmed as a contributor to the identified historic district. Since 2015, all buildings on the subject property have been demolished and the site no longer retains integrity as a district contributor. Thus, the site itself is not considered a historic resource for the purposes of this CEQA review.

Furthermore, as discussed in the previous Existing Historic Status section, the two Heritage Trees that remain on the site do not qualify as historic resources for the purposes of CEQA review.²⁶

As the lead agency in CEQA determinations, the City of San Bruno has requested additional review of whether the proposed project will be compatible with any surrounding historic resources. As the NWIC records search confirmed that there are no other identified historic resources in the area, the only known historic resource is the California Register-eligible Cupid's Row Historic District, which was identified in the 2001 San Bruno Redevelopment Area Historic Resource Inventory.

The following discussion analyzes whether the proposed park development project will have any potential impacts on identified historic resources pursuant to CEQA, inclusive of project-specific impacts and cumulative impacts analysis.

PROPOSED PROJECT ANALYSIS

Project-Specific Impacts Under CEQA

The Florida Avenue Park Master Plan project at 324 Florida Avenue does not cause any adverse impacts on the site itself since the previous demolition of the former buildings on the site caused it to no longer be considered a historic resource.

The NWIC records search conducted in January 2019 identified the "Site of Start of California Highway System" as the only identified historic resource within a .3-mile radius of 324 Florida Avenue. The "Site of Start of California Highway System" was assigned a Status Code of "7L," indicating a reevaluation is required. As the "Site of Start of California Highway System" is not a confirmed historic resource, nor is not associated with or within view of the subject site, the

²⁵ Pub. Res. Code SS5024.1, Title 14 CCR, Section 4850 et seq.

²⁶ According to CEQA Guidelines Section 15064.5(a), Category 3: "Generally, a resource shall be considered by the lead agency to be 'historically significant' if the resource meets the criteria for listing on the California Register of Historical Resources."

proposed construction of a park and playground at 324 Florida Avenue does not cause an adverse impact on this resource.

324 Florida Avenue is located directly north of a double-heart shaped street plan, which was identified in 2001 as the California Register-eligible Cupid's Row Historic District. The unusual and distinctive roadways define a potential district with a large concentration of residential housing units constructed between 1909 and 1951 in a mix of architectural styles. Approximately 110 properties comprise the potential historic district, with 82 properties identified as potential contributors in 2001. Alterations have and continue to occur to the individual contributors, and there appears to be a limited number of demolished or renovated buildings. However, the unusual and distinctive roadways remain unaltered and windshield surveys in September 2015 and February 2019 indicate that many of the buildings listed as contributors in 2001 appear to remain. The installation of a park at 324 Florida Avenue will result in a minor change to the setting of the district; however, the minor change would not significantly impact the integrity of the district.

324 Florida Avenue is directly visible from approximately six identified potential contributors, and minimally visible from the property lines of another approximately six potential contributors, located some distance away from the subject property.²⁷ 324 Florida Avenue features four street frontages at Florida Avenue, Taylor Avenue, Martin Place, and San Anselmo Avenue North (**Figure 22 to Figure 25**). Identified contributors are located on Florida and Taylor avenues and are not located on Martin Place or San Anselmo North. There is no specific threshold established to assess when a proposed project compromises the integrity of a historic district, and consequently would represent an adverse impact to the resource. A generally accepted rule of thumb is that retaining two-thirds or more of the district contributors would not compromise the integrity of a district. As the subject property is currently not considered a contributing resource, the proposed construction of a park and playground at 324 Florida Avenue would not alter the number of contributors to the potential Cupid's Row Historic District or jeopardize the district's continued eligibility for listing in the California Register.

The proposed construction of a park and playground at 324 Florida involves new paving, tree plantings, play equipment, pathways, lawn, and other proposed park features. The Cupid's Row Historic District documentation notes that residential landscaping consists of "grassed lawns, or modern hardscapes, with a few trees, but mostly low shrubbery for landscaping."²⁸ This description generally matches the proposed park landscaping. Overall, the proposed design (with or without the Heritage Trees) is compatible with the identified district and would not impact the district's integrity of location, design, setting, feeling or association. Therefore, the park development project will not cause an impact to the identified Cupid's Row Historic District.

²⁷ Cupid's Row Historic District." *Historical Inventory (San Bruno Redevelopment Area)* 2001. Properties include: 221, 240, 268, and 333 Florida Avenue; 333, 349, 363, 381, 388, 389, 431, and 449 Taylor Avenue. Note this is an approximation of contributing properties within the line of sight of 324 Florida Avenue.

²⁸ Ibid.



Figure 22: View from San Anselmo Avenue North, looking southwest.



Figure 23: View from Florida Avenue, looking northwest.



Figure 24: View from Martin Place, looking southeast.



Figure 25: View from Taylor Avenue, looking northeast.

Cumulative Impacts Under CEQA

“Cumulative impacts” refers to two or more individual effects which, when considered together, are considerable or which compound or increase other environmental impacts. The individual effects may be changes resulting from a single project or a number of separate projects. The cumulative impact from several projects is the change in the environment which results from the incremental impact of the project when added to other closely related past, present, and reasonably foreseeable probably future projects. Cumulative impacts can result from individually minor but collectively significant projects taking place over a period of time.

The California Register-eligible Cupid’s Row Historic District is comprised of approximately 110 properties, with 82 properties identified as potential contributors. Many properties appear to feature vinyl and aluminum replacement windows. Observations indicate that the district has not experienced extensive demolition of potential contributors. Therefore, the proposed Florida Avenue Park Master Plan project at 324 Florida Avenue does not appear to contribute to any cumulative impacts.

VI. CONCLUSION

In 2015, 324 Florida Avenue was determined ineligible for individual listing in the California Register, but remained a potential contributor to the identified California Register-eligible Cupids Row Historic District. This Historic Resources Impact Analysis finds that the proposed Florida Avenue Park Master Plan is compatible with the California Register-eligible Cupid's Row Historic District and does not cause any project-specific or cumulative impacts to historic resources under CEQA. These findings apply whether the final park design includes or does not include two Heritage Trees currently at the site.

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February 25, 2019

Ms. Joanne Magrini
Director, San Bruno Community Services Department.
851 Traeger Ave # 360
San Bruno, CA 94066

**Subject: Biological Technical Memorandum for the Florida Avenue Park Project,
324 Florida Avenue, San Bruno, California**

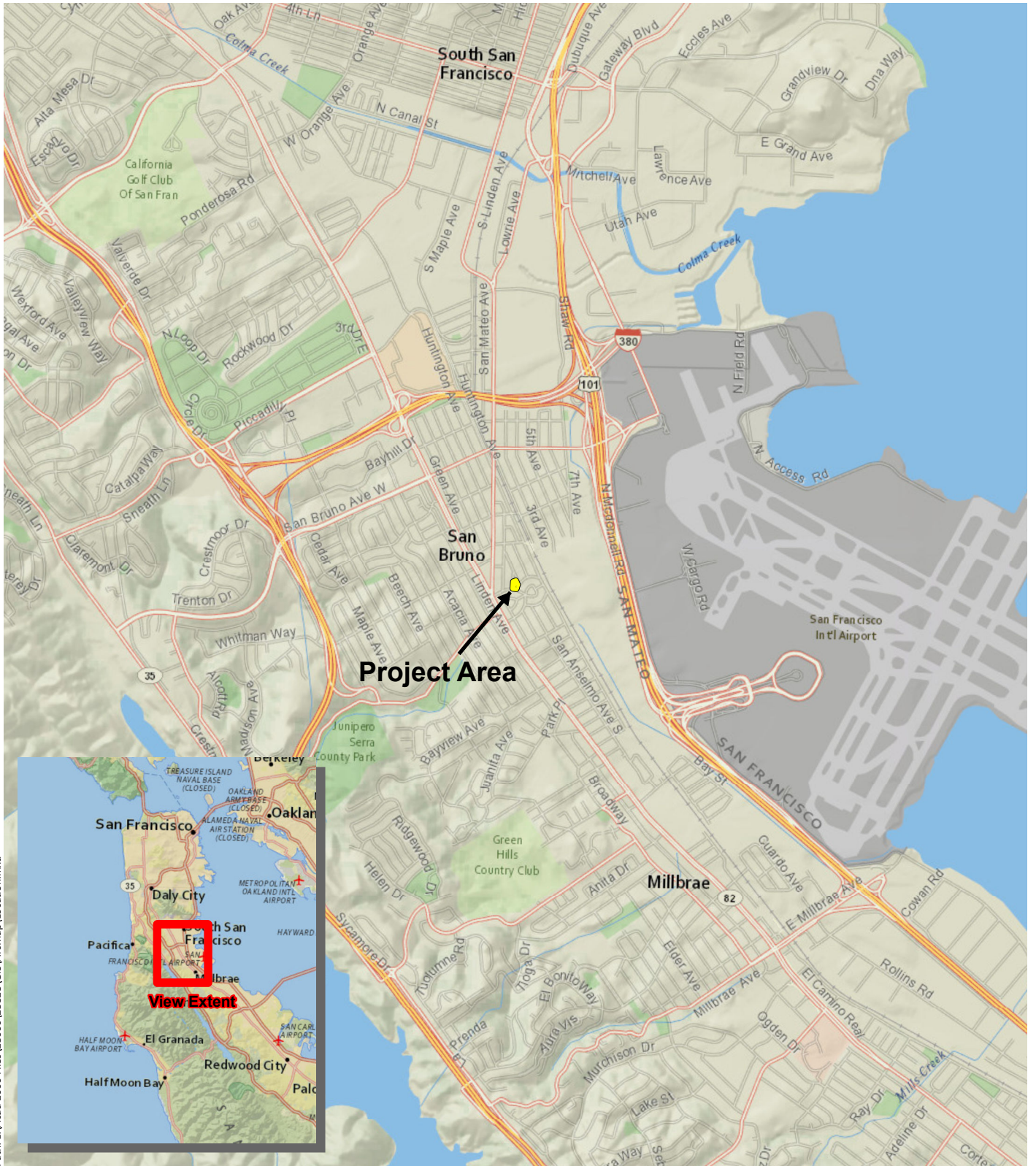
Dear Ms. Magrini:

This technical memorandum provides the results of the recent biological survey that WRA, Inc. (WRA) conducted for the Florida Avenue Park Project in San Bruno, San Mateo County, California. The Project Area, (Figure 1) is located approximately half a mile west of San Francisco International Airport and consists of a ruderal/disturbed, formerly residential parcel that has undergone remediation.

This report describes the results of the Biological evaluation and site visit to the Project Area to address: (1) presence of special-status plant and wildlife species; (2) potential to support special-status plant and wildlife species; and (3) the presence of other sensitive biological resources protected by local, state, and federal laws and regulations including the identification of any wetlands and non-wetland waters. This memorandum also contains an evaluation of potential impacts to special-status species and sensitive biological resources that may or may not occur as a result of the Proposed Project.

PROJECT DESCRIPTION

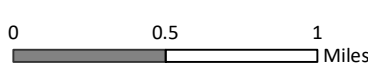
The Proposed Project includes the construction of a pocket park in San Bruno's Florida Avenue neighborhood. The park is anticipated to include benches, picnic tables and play equipment, in addition to tree and vegetation plantings. The Proposed Project will potentially involve the removal of two heritage trees, for which two arborist reports have been prepared (Kielty 2018 and Liggett 2018). Tree removal is predicated on the overall age and condition of the trees as well as concerns that large, dense pinecones could become a hazard if they were to fall on park users below. Replacement vegetation will include trees, shrubs, groundcover and grasses, stormwater planter perennials, and vines. The park is designed to meet the needs of the neighborhood and reflect the desires of the community.



Sources: National Geographic, WRA | Prepared By: smortensen, 2/8/2019

Figure 1. Project Area Regional Location Map

Florida Avenue Park
 San Bruno, San Mateo County, California



BIOLOGICAL SITE ASSESSMENT

The evaluation of biological resources presented in this report is based on a site visit conducted by a WRA biologist on February 12, 2019, review of background literature, and professional scientific judgment of WRA biologists with expertise in the characteristics of the Project Area as well as regional vegetation, plant, and wildlife species. Background literature sources utilized for the review included, but were not limited to:

- California Natural Diversity Database records (CNDDDB; CDFW 2019a)
- U.S. Fish and Wildlife Service (USFWS) Information for Planning and Conservation (IPaC) Species List (USFWS 2019a)
- The Western Bat Working Group (WBWG) online species accounts (WBWG 2019)
- CDFG publication “California Bird Species of Special Concern” (Shuford and Gardali 2008)
- CDFW publication “California Amphibians and Reptile Species of Special Concern” (Thomson et al. 2016)
- Breeding Bird Atlas of San Mateo County, California (Metropulos et al. 2001)

Plant and wildlife species observed within the Project Area were documented (Table 1) and all biological communities were documented.

ASSESSMENT RESULTS

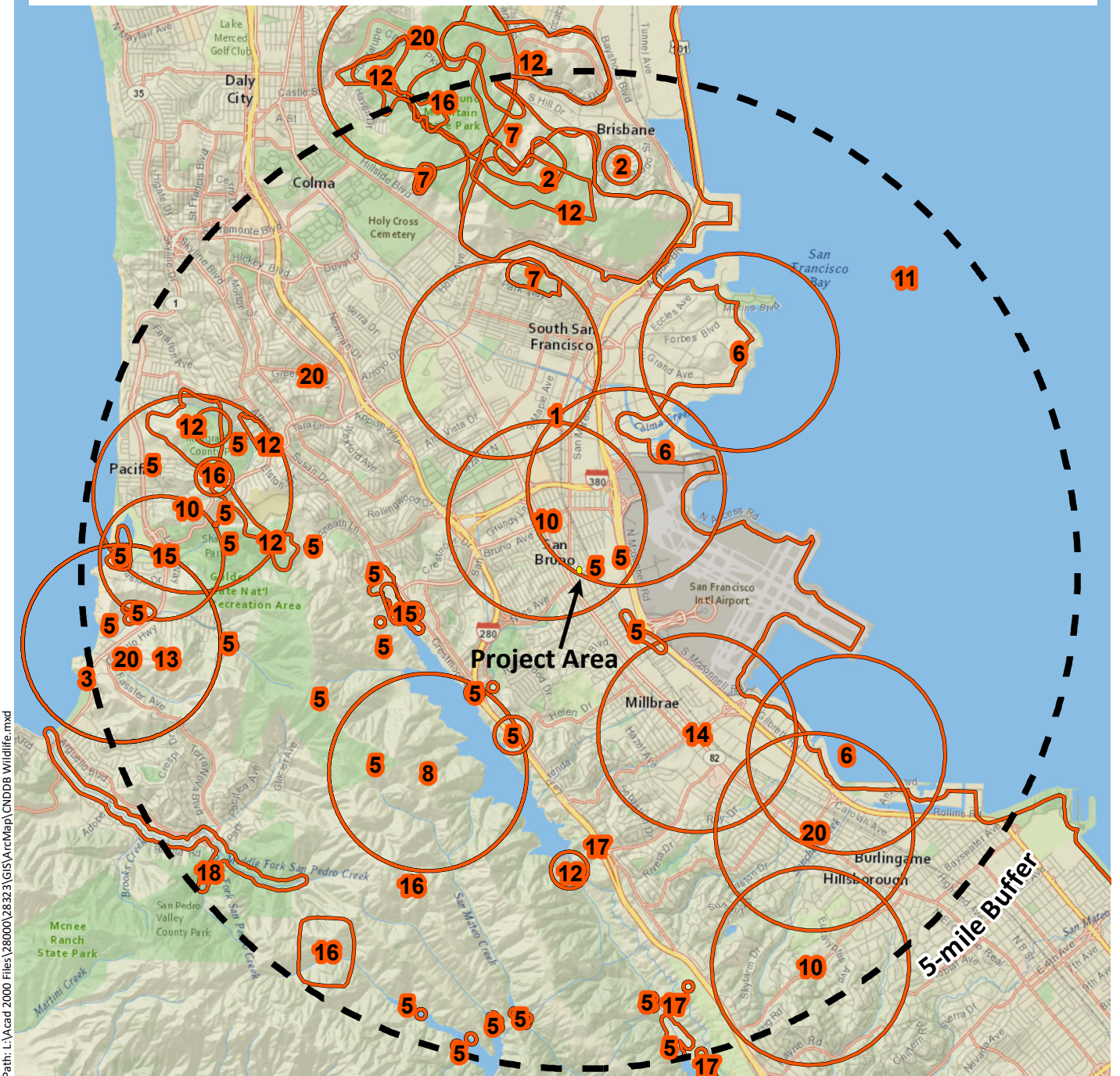
Biological Communities

The Project Area is approximately 0.4 acres of previously developed and ruderal/disturbed areas (Attachment A - Site Photographs). No native or naturalized vegetation alliances are present and common non-native and invasive plants species adapted to disturbance occupy these areas. There are two trees within the Project Area. The closest water feature to the Project Area is a flood control channel to the east, separated from the Project Area by dense residential development, and two sets of train tracks. The triangular site is bordered by minimally landscaped sidewalks to the east, west, and south, and by a residence directly to the north. The substrate of the area is disturbed in ruderal/disturbed areas with engineered fill. Ruderal/disturbed areas are dominated by non-native, invasive plants. Ruderal/disturbed areas are not considered environmentally sensitive, and have a very low potential to support special-status plant species, particularly in the landscape context present on site, completely surrounded by roads and dense residential development. The two trees in the Project Area are non-native species and have low potential to serve as habitat for special status wildlife species in this area. Non-special status birds protected under the MBTA or CDFW codes could potentially utilize the trees for nesting.

Special-status Plant and Wildlife Species

A total of 34 special-status wildlife species have been documented from the San Francisco South, Hunter’s Point, Montara Mountain, and San Mateo 7.5 minute quadrangles (Attachment B), and 21 of those special-status species are found within 5 miles of the Project Area (Figure2). Given the site conditions, however, the Project Site has no potential to support any special-status plant species. The entirety of the Project Area is comprised of disturbed ruderal, non-native habitat;

- | | | | | |
|--------------------------------|----------------------------------|----------------------------|-----------------------------------|--|
| 1. Alameda song sparrow | 5. California red-legged frog | 9. fringed myotis | 13. Myrtle's silverspot butterfly | 17. San Francisco dusky-footed woodrat |
| 2. Bay checkerspot butterfly | 6. California Ridgway's rail | 10. hoary bat | 14. pallid bat | 18. steelhead - central California coast DPS |
| 3. big free-tailed bat | 7. callippe silverspot butterfly | 11. longfin smelt | 15. saltmarsh common yellowthroat | 19. Townsend's big-eared bat |
| 4. California giant salamander | 8. foothill yellow-legged frog | 12. Mission blue butterfly | 16. San Bruno elfin butterfly | 20. western bumble bee |
| | | | | 21. western pond turtle |

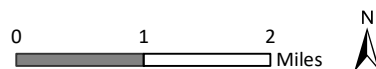


Path: L:\Acad 2000 Files\28000\28323\GIS\ArcMap\CNDDDB Wildlife.mxd

Sources: National Geographic, CNDDDB February 2019, WRA | Prepared By: smortensen, 2/8/2019

Figure 2. Special-Status Wildlife Species Documented within 5-miles of the Project Area

Florida Avenue Park
San Bruno, San Mateo County, California



none of the hydrologic, soil, topographic, or pH conditions; aquatic, wetland or vernal pool habitats; upland grassland habitats; or specific larval host plants that are necessary to support special-status plants and/or wildlife exist within or immediately adjacent to the Project Area. The land has been developed for many decades, and this land use history has degraded the local habitat and virtually eliminated any potential for special-status species to inhabit or move through the Project Area.

There is an approximately 45-foot deodar cedar (*Cedrus deodara*) present within the Project Area. The tree is generally in poor condition, with possible crown rot due to a past topping, with numerous dead limbs recently removed (Kielty 2018). Additionally, a 90-foot Norfolk Island pine (*Araucaria heterophylla*) is also present within the Project Area, and is in fair condition (Kielty 2018). The tree is located approximately 4 feet from a residence, with the upper branches situated directly overhead the residence. The tree has lost its top, and thus side limb development is likely to be faster, and those limbs will be proportionally longer, increasing the risk of limb failure (Leggitt 2018).

All plant and wildlife species observed during the February 12, 2019 site visit were common, urban-adapted native species, ornamental plants used in landscaping, or non-native invasive plants and wildlife. Wildlife and plant species observed during the site visit are listed in Table 1 below:

Table 1: Wildlife and dominant plant species observed on February 12, 2019 site visit

Common Name	Scientific Name
Wildlife	
House sparrow	<i>Carpodacus mexicanus</i>
Rock pigeon	<i>Columba livia</i>
American crow	<i>Corvus brachyrhynchos</i>
European starling	<i>Sturnus vulgaris</i>
House finch	<i>Passer domesticus</i>
Mourning dove	<i>Zenaida macroura</i>
Domestic cat	<i>Felis domesticus</i>
Western gray squirrel	<i>Sciurus griseus</i>
Plants	
Norfolk Island pine	<i>Araucaria heterophylla</i>
Deodar cedar	<i>Cedrus deodara</i>
Fat-hen	<i>Atriplex prostrata</i>
Wildoats	<i>Avena fatua</i>
Deadly nightshade	<i>Atropa belladonna</i>
Black mustard	<i>Brassica nigra</i>

Although the Project Area does not constitute habitat for any special-status species, the two trees on the site may be used as nesting habitat by bird species protected by the Migratory Bird Treaty Act (MBTA) and CDFW codes. These laws apply to a wide variety of native birds, including species that are non-migratory and/or commonly found in San Mateo County. Disturbance to breeding birds within and adjacent to the Project Area from construction noise and tree trimming or removal could result in nest abandonment or failure, which is considered a violation of the MBTA or CDFW codes.

Assessing the site according to the California Environmental Protection Act (CEQA) Environmental Checklist (Appendix G), as summarized in Table 2, we find the project does not have the potential to cause adverse impacts according based on the established thresholds of significance.

SUMMARY & RECOMMENDATIONS

Based on the site visit and review of information pertinent to the Project Area, the construction of the Project will not result in impacts to special-status plant and wildlife species or to any sensitive habitats. The habitats required by the special-status species known to occur in the Project Area vicinity are not present due to the developed nature of the Project Area, persistence of non-native species, and overall lack of natural habitat. The Project Area is subject to regular human disturbance from traffic and other activities.

The Project would have no impact to nesting birds protected by the MBTA and CDFW codes as long as construction occurs from September 1 to January 31, outside of the breeding bird season. If ground disturbance or vegetation removal must be conducted between February 1 and August 31, a qualified biologist will conduct preconstruction nesting bird surveys. These surveys will occur no less than 14 days prior to the start of ground disturbance or vegetation removal in any given area of construction to locate any nesting birds within the Project Area and adjacent areas that may be impacted by the Project. If nesting birds are found, they will be avoided by Project activities until a qualified biologist determines that the young have fledged. If construction activities do not begin within 14 days from the time of the survey, another survey will be completed before work begins.

Because the Project is in a highly developed area, nesting birds may be accustomed to regular disturbance and may nest over or adjacent to the Project Area. If a bird nests within the Project Area after the start of construction, construction activities can continue as long as the nest is physically avoided and construction noise levels do not significantly increase.

**Table 2: California Environmental Quality Act (CEQA) Environmental Checklist:
Appendix G.**

Would the Project:	Analysis
<i>A. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?</i>	The Project Area has been developed for many decades, and this land use history has degraded the local habitat and virtually eliminated any potential for special-status species that will inhabit or move through the Project Area. The Project will not have a substantial adverse effect on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service.
<i>B. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?</i>	The Project Area does not contain any riparian habitat or other sensitive natural communities, thus will not have a substantial adverse effect on those communities.
<i>C. Have a substantial adverse effect on state or federally protected wetlands as defined (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?</i>	The Project Area does not contain any state or federally protected wetlands, thus will not have a substantial adverse effect on those features.
<i>D. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?</i>	The Project Area is situated within dense residential development, and does not serve as a migration corridor, nor does it contain native wildlife nursery sites. It is not within any essential connectivity areas (CNDDDB 2019).
<i>E. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?</i>	The Project does not conflict with any local policies or ordinances protecting biological resources. The city of San Bruno has obtained a Tree Removal Permit for the removal of two heritage trees within the Project Area.
<i>F. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?</i>	The Project does not conflict with the provisions of any local, regional, or state conservation plans.

If you have questions or require additional information, please do not hesitate to contact me or Paul Curfman, at 415-524-7544.

Sincerely,

A handwritten signature in blue ink, appearing to read "Stewart DesMeules".

Stewart DesMeules
WRA Biologist

Enclosures:

Attachment A: Representative Photographs of the Project Area

Attachment B: CNDDDB and USFWS Results for a 5-mile radius around the Project Area

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Attachment A

Representative Photographs of the Project Area



The Project Area, facing north towards the deodar cedar and Norfolk Island pine.



View of the deodar cedar and the pruned Norfolk Island pine.



The Project Area, facing south.



Residential development and road adjacent to the Project Area.



Scar at the base of the Norfolk Island pine.



Removed bark at the base of the deodar cedar within the Project Area.

Attachment B

Figure 2. Special-status Wildlife Species Documented Within 5-miles of the Project Area

Appendix B. Potential for Special Status Wildlife Species to Occur in the Project Area. List compiled from the California Department of Fish and Wildlife (CDFW) Natural Diversity Database (2019), U.S. Fish and Wildlife Service (USFWS) Information for Planning and Conservation Database (2019), searches of the San Francisco South, Hunter's Point, Montara Mountain, and San Mateo USGS 7.5' quadrangles, a review of historical and current satellite imagery via Google Earth (2019) and a review of other CDFW lists and publications.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Mammals				
American badger <i>Taxidea taxus</i>	SSC	Most abundant in drier open stages of most shrub, forest, and herbaceous habitats, with friable soils. Requires friable soils and open, uncultivated ground. Preys on burrowing rodents.	No Potential. No suitable open shrub, forest or herbaceous habitat exists within the Project Area.	No further actions are recommended for this species.
southern sea otter <i>Enhydra lutris nereis</i>	FT, CFP, MMC SSC	Nearshore marine environments from about Año Nuevo, San Mateo County. To Point Sal, Santa Barbara County. Needs canopies of giant kelp and bull kelp for rafting and feeding. Prefers rocky substrates with abundant invertebrates.	No Potential. No aquatic habitat exists within the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
fringed Myotis <i>Myotis thysanodes</i>	WBWG High	Associated with a wide variety of habitats including dry woodlands, desert scrub, mesic coniferous forest, grassland, and sage-grass steppes. Buildings, mines and large trees and snags are important day and night roosts.	No Potential. No rocky outcrops, cliffs or caves are present within the Project Area. Trees within the Project Area don't contain suitable structural features to support roosting by this species.	No further actions are recommended for this species.
pallid bat <i>Antrozous pallidus</i>	SSC, WBWG	Found in deserts, grasslands, shrublands, woodlands, and forests. Most common in open, forages along river channels. Roost sites include crevices in rocky outcrops and cliffs, caves, mines, trees and various human structures such as bridges, barns, and buildings (including occupied buildings). Roosts must protect bats from high temperatures. Very sensitive to disturbance of roosting sites.	No Potential. No rocky outcrops, cliffs or caves are present within the Project Area. The existing structures on the property don't support thermally stable conditions required by this species. Trees within the Project Area are small and don't contain suitable structural features to support roosting by this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Townsend's big-eared bat <i>Corynorhinus townsendii</i>	SC, SSC, WBWG	This species is associated with a wide variety of habitats from deserts to mid-elevation mixed coniferous-deciduous forest. Females form maternity colonies in buildings, caves and mines and males roost singly or in small groups. Foraging occurs in open forest habitats where they glean moths from vegetation.	No Potential. The Project Area does not contain the mixed coniferous forest typically associated with this species, and does not contain caves or other similar structures to support roosting.	No further actions are recommended for this species.
silver-haired bat <i>Lasionycteris noctivagans.</i>	WBWG	Primarily a forest dweller, feeding over streams, ponds, and open brushy areas. Summer habitats include a variety of forest and woodland types, both coastal and montane. Roosts in hollow trees, snags, buildings, rock crevices, caves, and under bark.	No Potential. No water sources exist within the Project Area. Trees within the Project Area don't contain suitable structural features to support roosting by this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
hoary bat <i>Lasiurus cinereus</i>	WBWG	Prefers open forested habitats or habitat mosaics, with access to trees for cover and open areas or habitat edges for feeding. Roosts in dense foliage of medium to large trees. Feeds primarily on moths. Requires water.	No Potential. The Project Area does not contain suitable trees required for roosting by this species.	No further actions are recommended for this species.
big free-tailed bat <i>Nyctinomops macrotis</i>	SSC, WBWG	Occurs rarely in low-lying arid areas. Requires high cliffs or rocky outcrops for roosting sites.	No Potential. No high cliffs or rocky outcrops are present within the Project Area. This Project Area is also outside of the typical range for this species.	No further actions are recommended for this species.
San Francisco dusky-footed woodrat <i>Neotoma fuscipes annectens</i>	SSC	Forest habitats of moderate canopy and moderate to dense understory. Also in chaparral habitats. Constructs nests of shredded grass, leaves, and other material. May be limited by availability of nest-building materials.	No Potential. No suitable forest or chaparral habitats exist within the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
salt-marsh harvest mouse <i>Reithrodontomys raviventris</i>	FE, SE, CFP	Endemic to emergent salt and brackish wetlands of the San Francisco Bay Estuary. Pickleweed marshes are primary habitat; also occurs in various other wetland communities with dense vegetation. Does not burrow, builds loosely organized nests. Requires higher areas for flood escape.	No Potential. The Project Area does not contain emergent salt and brackish wetlands.	No further actions are recommended for this species.
Birds				
tricolored blackbird <i>Agelaius tricolor</i>	ST, BCC, SSC	Nearly endemic to California, where it is most numerous in the Central Valley and vicinity. Highly colonial, nesting in dense aggregations over or near freshwater in emergent growth or riparian thickets. Also uses flooded agricultural fields. Abundant insect prey near breeding areas essential.	No Potential. The Project Area does not contain wetlands with emergent vegetation to support breeding in this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
marbled murrelet <i>Brachyramphus marmoratus</i>	FT, SE	Predominantly coastal marine. Nests in old-growth coniferous forests up to 30 miles inland along the Pacific coast, from Eureka to Oregon border, and in Santa Cruz/San Mateo Counties. Nests are highly cryptic, and typically located on platform-like branches of mature redwoods and Douglas firs. Forages on marine invertebrates and small fishes.	No Potential. The Project Area does not contain suitable forest habitat to support breeding in this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
bank swallow <i>Riparia riparia</i>	ST	Summer resident in riparian and other lowland habitats near rivers, lakes and the ocean in northern California. Nests colonially in excavated burrows on vertical cliffs and bank cuts (natural and manmade) with fine-textured soils. Historical nesting range in southern and central areas of California has been eliminated by habitat loss. Currently known to breed in Siskiyou, Shasta, and Lassen Cos., portions of the north coast, and along Sacramento River from Shasta Co. south to Yolo Co.	No Potential. No vertical cliffs, bank cuts, or other suitable nesting habitat exists for this species in the Project Area.	No further actions are recommended for this species.
grasshopper sparrow <i>Ammodramus savannarum</i>	SSC	Summer resident. Breeds in open grasslands, generally with low- to moderate-height grasses and scattered shrubs. Well-hidden nests are placed on the ground.	No Potential. No grasslands this species would utilize for nesting are present within the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
<p>golden eagle <i>Aquila chrysaetos</i></p>	<p>BCC, CFP</p>	<p>Occurs year-round in rolling foothills, mountain areas, sage-juniper flats, and deserts. Cliff-walled canyons provide nesting habitat in most parts of range; also nests in large trees, usually within otherwise open areas.</p>	<p>No Potential. There are no cliffs, rocky outcroppings or suitable trees that would support nesting by this species.</p>	<p>No further actions are recommended for this species.</p>
<p>great egret <i>Ardea alba</i></p>	<p>none (breeding sites protected by CDFW); CDF sensitive</p>	<p>Year-round resident. Nests colonially or semi-colonially, usually in trees, occasionally on the ground or elevated platforms. Breeding sites usually in close proximity to foraging areas: marshes, lake margins, tidal flats, and rivers. Forages primarily on fishes and other aquatic prey, also smaller terrestrial vertebrates.</p>	<p>No Potential. The Project Area does not contain any potential roost trees close enough to water to be utilized by this species. Species may occasionally fly over the Project Area.</p>	<p>No further actions are recommended for this species.</p>

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
short-eared owl <i>Asio flammeus</i>	SSC	Occurs year-round, but primarily as a winter visitor; breeding very restricted in most of California. Found in open, treeless areas (e.g., marshes, grasslands) with elevated sites for foraging perches and dense herbaceous vegetation for roosting and nesting. Preys mostly on small mammals, particularly voles.	No Potential. The Project Area does not contain marsh habitat, and is outside the known breeding range for this species.	No further actions are recommended for this species.
long-eared owl <i>Asio otus</i>	SSC	Occurs year-round in California. Nests in trees in a variety of woodland habitats, including oak and riparian, as well as tree groves. Requires adjacent open land with rodents for foraging, and the presence of old nests of larger birds (hawks, crows, magpies) for breeding.	No Potential. The Project Area does not contain the riparian habitat used by this species for nesting.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
burrowing owl <i>Athene cunicularia</i>	SSC, BCC	Year-round resident and winter visitor. Occurs in open, dry grasslands and scrub habitats with low-growing vegetation, perches and abundant mammal burrows. Preys upon insects and small vertebrates. Nests and roosts in old mammal burrows, most commonly those of ground squirrels.	No Potential. No open grassland exists within the Project Area. Additionally, no suitable burrows, or burrow surrogates were observed within the Project Area during the site visit.	No further actions are recommended for this species.
oak titmouse <i>Baeolophus inornatus</i>	BCC	Occurs year-round in woodland and savannah habitats where oaks are present, as well as riparian areas. Nests in tree cavities.	No Potential. The two trees within the Project Area contain no suitable nesting cavities for this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Swainson's hawk <i>Buteo swainsoni</i>	ST, BCC	Summer resident in California's Central Valley. Nests in tree groves and isolated trees in riparian and agricultural areas, including near buildings. Forages in grasslands and scrub habitats as well as agricultural fields, especially alfalfa. Preys on arthropods year-round as well as smaller vertebrates during the breeding season.	No Potential. The Project Area and surroundings do not contain the flat, open habitat this species requires for nesting and foraging.	No further actions are recommended for this species.
western snowy plover <i>Charadrius nivosus</i> <i>(alexandrines) nivosus</i>	FT, SSC, BCC, RP	Federal listing applies only to the Pacific coastal population. Year-round resident and winter visitor. Occurs on sandy beaches, salt pond levees, and the shores of large alkali lakes. Nests on the ground, requiring sandy, gravelly or friable soils.	No Potential. The Project Area does not contain any beaches, salt pond levees or alkali lakes.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
northern harrier <i>Circus cyaneus</i>	SSC	Year-round resident and winter visitor. Found in open habitats including grasslands, prairies, marshes and agricultural areas. Nests on the ground in dense vegetation, typically near water or otherwise moist areas. Preys on small vertebrates.	No Potential. Due to the highly disturbed nature of the Project Area and close proximity to roads, and residences, and the Project Area contains no suitable grassland, prairie or marsh habitat to support nesting in this species.	No further actions are recommended for this species.
olive-sided flycatcher <i>Contopus cooperi</i>	SSC, BCC	Summer resident. Typical breeding habitat is montane coniferous forests. At lower elevations, also occurs in wooded canyons and mixed forests and woodlands. Often associated with forest edges. Arboreal nest sites located well off the ground.	No Potential. The Project Area does not contain the woodland, forest, or riparian habitats required by this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
yellow rail <i>Coturnicops noveboracensis</i>	BCC, SSC	Summer resident in eastern Sierra Nevada in Mono County, breeding in shallow freshwater marshes and wet meadows with dense vegetation. Also a rare winter visitor along the coast and other portions of the state. Extremely cryptic.	No Potential. The Project Area does not contain the freshwater marsh habitat this species requires for breeding.	No further actions are recommended for this species.
white-tailed kite <i>Elanus leucurus</i>	CFP	Year-round resident in coastal and valley lowlands with scattered trees and large shrubs, including grasslands, marshes and agricultural areas. Nests in trees, of which the type and setting are highly variable. Preys on small mammals and other vertebrates.	No Potential. The Project Area provides no foraging habitat for this species and the trees within the Project Area have no potential to support nesting by this species. The species may occasionally fly over the area.	No further actions are recommended for this species.
snowy egret <i>Egretta thula</i>	none (breeding sites protected by CDFW)	(Rookery) colonial nester, with nest sites situated in protected beds of dense tules. Rookery sites situated close to foraging areas: marshes, tidal-flats, streams, wet meadows, and borders of lakes.	No Potential. The Project Area does not contain any potential roost trees close enough to water to be utilized by this species. Species may occasionally fly over the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
American peregrine falcon <i>Falco peregrinus anatum</i>	FD, SD, CFP, BCC	Year-round resident and winter visitor. Occurs in a wide variety of habitats, though often associated with coasts, bays, marshes and other bodies of water. Nests on protected cliffs and also on man-made structures including buildings and bridges. Preys on birds, especially waterbirds. Forages widely.	No Potential. The Project Area does not contain suitable elevated nesting structures such as cliffs, tall buildings, or transmission towers. This species may occasionally pass through the Project Area.	No further actions are recommended for this species.
Saltmarsh common yellowthroat <i>Geothlypis trichas sinuosa</i>	BCC, SSC	Resident of the San Francisco Bay region, in fresh and salt water marshes. Requires thick, continuous cover down to water surface for foraging; tall grasses, tule patches, willows for nesting.	No Potential. The Project Area does not contain marsh habitat, tall shrubs or other suitable vegetation required for nesting.	No further actions are recommended for this species.
black oystercatcher <i>Haematopus bachmani</i>	BCC	Year-round resident of rocky coast habitats along the Pacific coast. Also occurs on coastal and lower estuarine mud-flats. Forages primarily on intertidal invertebrates.	No Potential. The Project Area does not contain coastal cliffs or estuarine habitats required for nesting and foraging by the species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
caspian tern <i>Hydroprogne caspia</i>	BCC	Summer resident in northern California. Nests colonially on sparsely-vegetated islands (including man-made islands), sandbars and beaches near expanses of open water. Forages on fishes.	No Potential. The Project Area does not contain and is not near flat sand or gravel flats which are required to support nesting by this species.	No further actions are recommended for this species.
least bittern <i>Ixobrychus exilis</i>	SSC, BCC	Summer resident in portions of the Central Valley and southern California. Typically breeds in deeper freshwater marshes with dense emergent and woody vegetation.	No Potential. The Project Area does not contain any marsh habitat needed by this species for nesting.	No further actions are recommended for this species.
loggerhead shrike <i>Lanius ludovicianus</i>	BCC, SSC	Year-round resident in open woodland, grassland, savannah and scrub. Prefers areas with sparse shrubs, trees, posts, and other suitable perches for foraging. Preys upon large insects and small vertebrates. Nests are well-concealed in densely-foliaged shrubs or trees.	No Potential. The Project Area does not contain suitable open habitats for foraging by this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
California black rail <i>Laterallus jamaicensis coturniculus</i>	ST, CFP	Year-round resident in marshes (saline to freshwater) with dense vegetation within four inches of the ground. Prefers larger, undisturbed marshes that have an extensive upper zone and are close to a major water source. Extremely secretive and cryptic.	No Potential. The Project Area does not contain the marsh habitat this species requires for breeding.	No further actions are recommended for this species.
Lewis's woodpecker <i>Melanerpes lewis</i>	BCC	Uncommon resident in California occurring on open oak savannahs, broken deciduous and coniferous habitats. Breeds primarily in ponderosa pine forests, riparian woodlands and disturbed pine forests but is also known to nest in orchards and oak woodlands. Rare nester in the San Francisco Bay Area.	No Potential. The Project Area does not contain any pine forests or riparian woodlands required by this species for nesting.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Alameda song sparrow <i>Melospiza melodia pusillula</i>	BCC, SSC	Year-round resident of salt marshes bordering the south arm of San Francisco Bay. Inhabits primarily pickleweed marshes; nests placed in marsh vegetation, typically shrubs such as gumplant.	No Potential. The Project Area does not contain emergent brackish water vegetation that this species typically inhabits.	No further actions are recommended for this species.
Samuels (San Pablo) song sparrow <i>Melospiza melodia samuelis</i>	BCC, SSC	Year-round resident of tidal marshes along the north side of San Francisco and San Pablo Bays. Typical habitat is dominated by pickleweed, with gumplant and other shrubs present in the upper zone for nesting. May forage in areas adjacent to marshes.	No Potential. The Project Area does not contain emergent brackish water vegetation that this species typically inhabits.	No further actions are recommended for this species.
black-crowned night heron <i>Nycticorax nycticorax</i>	none (breeding sites protected by CDFW)	(Rookery) colonial nester, usually in trees, occasionally in tule patches. Rookery sites located adjacent to foraging areas: lake margins, mud-bordered bays, marshy spots.	No Potential. The Project Area does not contain any potential roost trees close enough to water to be utilized by this species. Species may occasionally fly over the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
double-crested cormorant <i>Phalacrocorax auritus</i>	DFG:WL	(Rookery site) colonial nester on coastal cliffs, offshore islands, and along lake margins in the interior of the state. Nests along coast on sequestered islets, usually on ground with sloping surface, or in tall trees along lake margins.	No Potential. The Project Area does not contain any potential roost trees close enough to water to be utilized by this species. Species may occasionally fly over the Project Area.	No further actions are recommended for this species.
Nuttall's woodpecker <i>Picoides nuttallii</i>	BCC	Year-round resident in lowland woodlands throughout much of California west of the Sierra Nevada. Typical habitat is dominated by oaks; also occurs in riparian woodland. Nests in tree cavities.	Unlikely. The trees within the Project Area do not contain suitable cavities for nesting. The species may occasionally fly over the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
California Ridgway's (clapper) rail <i>Rallus obsoletus obsoletus</i>	FE, SE, CFP	Year-round resident in tidal marshes of the San Francisco Bay estuary. Requires tidal sloughs and intertidal mud flats for foraging, and dense marsh vegetation for nesting and cover. Typical habitat features abundant growth of cordgrass and pickleweed. Feeds primarily on molluscs and crustaceans.	No Potential. The Project Area does not contain the saltwater marsh habitat this species requires for breeding.	No further actions are recommended for this species.
short-tailed albatross <i>Phoebastria albatrus</i>	FE, SSC	Highly pelagic; comes to land only when breeding. Nests on remote Pacific islands. A rare non-breeding visitor to the eastern Pacific.	No Potential. The Project Area contains no breeding habitat for this species.	No further actions are recommended for this species.
black skimmer <i>Rynchops niger</i>	BCC, SSC	Found primarily in southern California; South San Francisco Bay has a small resident population. Nests colonially on gravel bars, low islets, and sandy beaches	No Potential. The Project Area does not contain and is not near flat sand or gravel bar habitats required to support nesting by this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Allen's hummingbird <i>Selasphorus sasin</i>	BCC	Summer resident along the California coast, breeding in a variety of woodland and forest habitats, including parks and gardens with abundant nectar sources. Nest in shrubs and trees with dense vegetation.	Unlikely. The Project Area does not contain open woodlands chaparral or scrub habitats that are known to support nesting by this species.	No further actions are recommended for this species.
Lawrence's goldfinch <i>Spinus (= Carduelis) lawrencei</i>	BCC	Summer resident; generally uncommon and local. Typically found in arid open woodlands, including oak savannah. Breeding distribution is erratic from year to year.	No Potential. The Project Area does not contain the open woodland or savannah habitats this species prefers.	No further actions are recommended for this species.
black-chinned sparrow <i>Spizella atrogularis</i>	BCC	Prefers sloping ground in mixed chaparral, chamise-redshank chaparral, sagebrush, and similar brushy habitats. Often on arid, south-facing slopes with ceanothus, manzanita, sagebrush, and chamise.	No Potential. The Project Area does not contain any chaparral or similar brushy habitats.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
California least tern <i>Sternula antillarum browni</i>	FE, SE, CFP	Summer resident along the coast from San Francisco Bay south to northern Baja California; inland breeding also very rarely occurs. Nests colonially on barren or sparsely vegetated areas with sandy or gravelly substrates near water, including beaches, islands, and gravel bars. In San Francisco Bay, has also nested on salt pond margins.	No Potential. The Project Area does not contain and is not near flat sand or gravel flats which are required to support nesting by this species.	No further actions are recommended for this species.
Suisun song sparrow <i>Melospiza melodia maxillaris</i>	BCC, SSC	Year-round resident of brackish-water marshes along Suisun Bay. Inhabits cattails, tules, bulrushes and other emergent vegetation, including pickleweed. Nests typically placed in shrubs.	No Potential. The Project Area does not contain emergent brackish water vegetation that this species typically inhabits.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
yellow-billed magpie <i>Pica nuttalli</i>	BCC	Endemic to the Central Valley and central Coast Ranges. Favors open park-like areas with expanses of open ground, including oak savannah, orchards, and along stream courses. Large, dome-shaped stick nests are placed in trees.	No Potential. This species is not known to nest within the vicinity of the Project Area.	No further actions are recommended for this species.
yellow warbler <i>Setophaga (Dendroica) petechia brewsteri</i>	SSC, BCC	Summer resident throughout much of California. Breeds in riparian vegetation close to water, including streams and wet meadows. Microhabitat used for nesting variable, but dense willow growth is typical. Occurs widely on migration.	No Potential. The Project Area does not contain riparian habitat with thick continuous cover required to support this species.	No further actions are recommended for this species.
yellow-headed blackbird <i>Xanthocephalus xanthocephalus</i>	SSC	Summer resident. Breeds colonially in freshwater emergent wetlands with dense vegetation and deep water, often along borders of lakes or ponds. Requires abundant large insects such as dragonflies; nesting is timed for maximum emergence of insect prey.	No Potential. The Project Area does not contain any water bodies with suitable surrounding marsh.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Reptiles and Amphibians				
Pacific (western) pond turtle <i>Actinemys marmorata</i>	SSC	A thoroughly aquatic turtle of ponds, marshes, rivers, streams and irrigation ditches with aquatic vegetation. Require basking sites such as partially submerged logs, vegetation mats, or open mud banks, and suitable upland habitat (sandy banks or grassy open fields) for egg-laying.	No Potential. The Project Area does not contain aquatic habitat to support this species.	No further actions are recommended for this species.
foothill yellow-legged frog <i>Rana boylei</i>	SSC	Found in or near rocky streams in a variety of habitats. Prefers partly-shaded, shallow streams and riffles with a rocky substrate; requires at least some cobble-sized substrate for egg-laying. Needs at least 15 weeks to attain metamorphosis. Feeds on both aquatic and terrestrial invertebrates.	No Potential. While this species is known to the region, the Project Area does not contain any aquatic habitat to support the species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
San Francisco garter snake <i>Thamnophis sirtalis tetrataenia</i>	FE, SE, CFP, RP	Vicinity of freshwater marshes, ponds and slow moving streams in San Mateo County and extreme northern Santa Cruz County. Prefers dense cover and water depths of at least one foot. Upland areas near water are also very important.	No Potential. The Project Area is not within the vicinity of any suitable water bodies that this species would occur near.	No further actions are recommended for this species.
California giant salamander <i>Dicamptodon ensatus</i>	SSC	Occurs in the north-central Coast Ranges. Moist coniferous and mixed forests are typical habitat; also uses woodland and chaparral. Adults are terrestrial and fossorial, breeding in cold, permanent or semi-permanent streams. Larvae usually remain aquatic for over a year.	No Potential. The Project Area does not contain any forested habitat or aquatic features that this species would utilize.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
California red-legged frog <i>Rana draytonii</i>	FT, SSC, RP	Lowlands and foothills in or near permanent sources of deep water with dense, shrubby or emergent riparian vegetation. Requires 11 to 20 weeks of permanent water for larval development. Associated with quiet perennial to intermittent ponds, stream pools and wetlands. Prefers shorelines with extensive vegetation. Disperses through upland habitats after rains.	No Potential. This species has been documented within one mile of the Project Area, however that occurrence was associated with a canal separated from the Project Area by dense residential development and two sets of railroad tracks (CNDDDB 2019). The Project Area does not contain any water bodies, nor are there water bodies within the typical dispersal distance for the species. Therefore there is no potential that this species would occur within the Project Area at any time.	No further actions are recommended for this species.
Fishes				
tidewater goby <i>Eucyclogobius newberryi</i>	FE, SSC	Brackish water habitats along the California coast from Agua Hedionda Lagoon, San Diego County to the mouth of the Smith River. Found in shallow lagoons and lower stream reaches; requires fairly still but not stagnant water and high oxygen levels.	No Potential. No aquatic habitat is present to support this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
<p>hardhead <i>Mylopharodon conocephalus</i></p>	<p>SSC, FS sensitive</p>	<p>Found in low to mid-elevation streams in the Sacramento-San Joaquin drainage; also occurs in the Russian River and tributaries. Favors clear, deep pools with sand-gravel-boulder bottoms and slow water velocity. Not found where exotic Centrarchids predominate.</p>	<p>No Potential. No aquatic habitat is present to support this species.</p>	<p>No further actions are recommended for this species.</p>
<p>steelhead - central CA coast DPS <i>Oncorhynchus mykiss irideus</i></p>	<p>FT</p>	<p>Occurs from the Russian River south to Soquel Creek and Pajaro River. Also in San Francisco and San Pablo Bay Basins. Adults migrate upstream to spawn in cool, clear, well-oxygenated streams. Juveniles remain in fresh water for 1 or more years before migrating downstream to the ocean.</p>	<p>No Potential. No aquatic habitat is present to support this species.</p>	<p>No further actions are recommended for this species.</p>

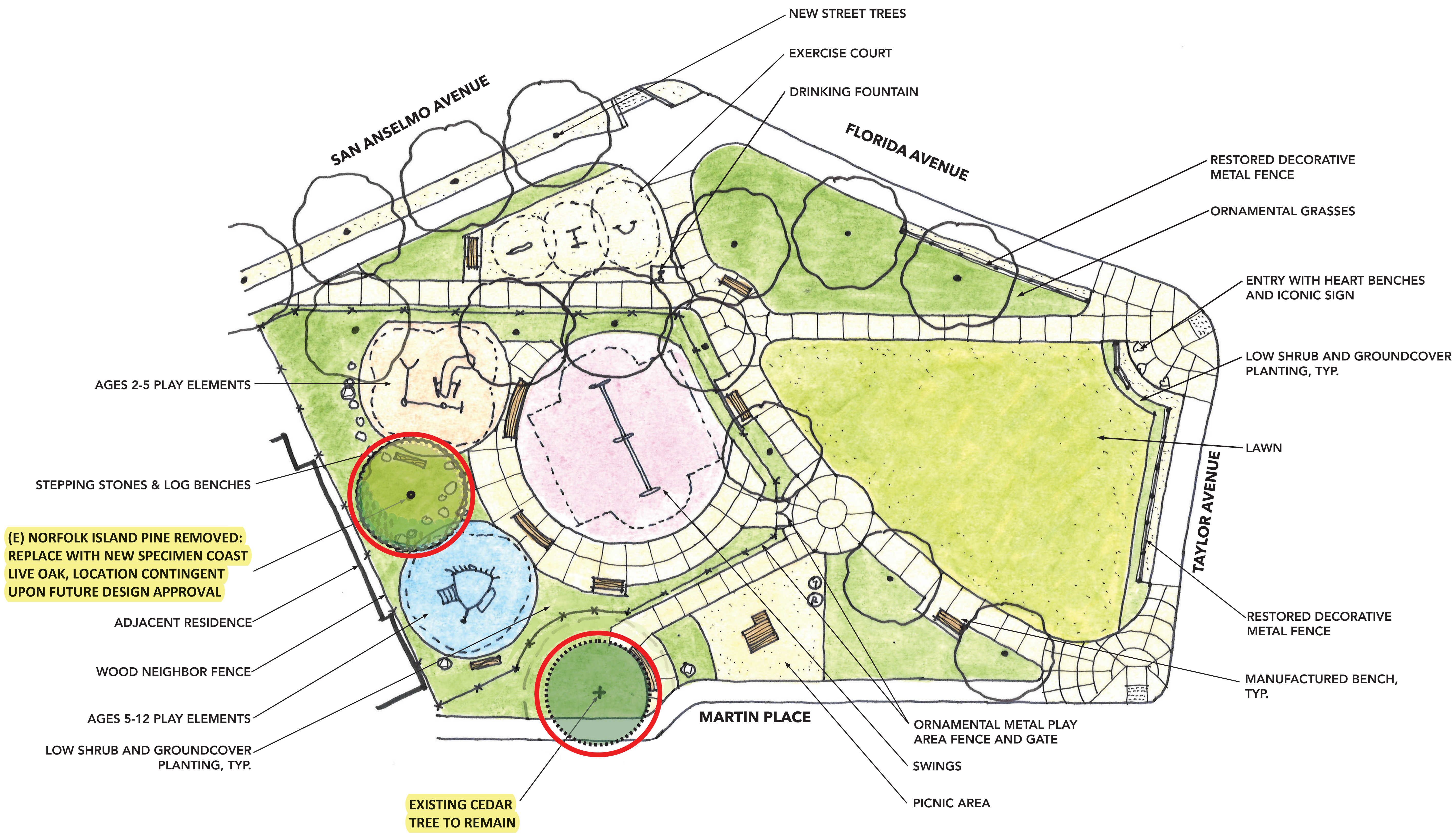
SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
longfin smelt <i>Spirinchus thaleichthys</i>	FC, ST, SSC	Euryhaline, nektonic and anadromous. Found in open waters of estuaries, mostly in middle or bottom of water column. Prefer salinities of 15 to 30 ppt, but can be found in completely freshwater to almost pure seawater.	No Potential. No aquatic habitat is present to support this species.	No further actions are recommended for this species.
Delta smelt <i>Hypomesus transpacificus</i>	FT, SE, RP	Lives in the Sacramento-San Joaquin estuary in areas where salt and freshwater systems meet. Occurs seasonally in Suisun Bay, Carquinez Strait and San Pablo Bay. Seldom found at salinities > 10 ppt; most often at salinities < 2 ppt.	No Potential. No aquatic habitat is present to support this species.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Invertebrates				
western bumble bee <i>Bombus occidentalis</i>	SSI	Formerly common throughout much of western North America; populations from southern British Columbia to central California have nearly disappeared (Xerces 2015). Occurs in a wide variety of habitat types. Nests are constructed annually in pre-existing cavities, usually on the ground (e.g. mammal burrows). Many plant species are visited and pollinated.	Unlikely. No burrows or burrow surrogates were observed within the Project Area during the site visit.	No further actions are recommended for this species.
Mission blue butterfly <i>Icaricia icarioides missionensis</i>	FE, SSI, RP	Inhabits grasslands and coastal chaparral of the San Francisco peninsula and southern Marin County, but mostly found on San Bruno Mountain. Three larval host plants: <i>Lupinus albifrons</i> , <i>L. variicolor</i> , and <i>L. formosus</i> , of which <i>L. albifrons</i> is favored.	No Potential. The Project Area does not contain grasslands or the coastal chaparral habitat that this species requires.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
<p>Myrtle's silverspot butterfly <i>Speyeria zerene myrtleae</i></p>	<p>FE, RP, SSI</p>	<p>Restricted to the fog belt of northern Marin and southernmost Sonoma County, including the Point Reyes peninsula; extirpated from coastal San Mateo County. Occurs in coastal prairie, dunes, and grassland. Larval foodplant is typically <i>Viola adunca</i>. Adult flight season may range from late June to early September.</p>	<p>No Potential. The Project Area is outside the range of this species.</p>	<p>No further actions are recommended for this species.</p>
<p>Bay checkerspot butterfly <i>Euphydryas editha bayensis</i></p>	<p>FT, SSI, RP</p>	<p>Restricted to native grasslands on outcrops of serpentine soil in the vicinity of San Francisco Bay. <i>Plantago erecta</i> is the primary host plant; <i>Orthocarpus densiflorus</i> and <i>O. purpurscens</i> are the secondary host plants.</p>	<p>No Potential. The Project Area does not contain any host plants for this species, nor does it contain native grasslands.</p>	<p>No further actions are recommended for this species.</p>

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
San Bruno elfin butterfly <i>Callophrys mossii bayensis</i>	FE, SSI	Limited to the vicinity of San Bruno Mountain, San Mateo County. Colonies are located on rocky outcrops and cliffs in coastal scrub habitat on steep, north-facing slopes within the fog belt. Species range is tied to the distribution of the larval host plant, <i>Sedum spathulifolium</i> .	No Potential. The Project Area is outside the range of this species.	No further actions are recommended for this species.
monarch butterfly <i>Danaus plexippus</i>	SSI	Winter roost sites extend along the coast from northern Mendocino to Baja California, Mexico. Roosts located in wind-protected tree groves (eucalyptus, Monterey pine, Monterey cypress), with nectar and water sources nearby.	Unlikely. No suitable roost trees are present within the Project Area.	No further actions are recommended for this species.

SPECIES	STATUS*	HABITAT REQUIREMENTS	POTENTIAL TO OCCUR IN THE PROJECT AREA	DISCUSSION
Callippe silverspot butterfly <i>Speyeria callippe callippe</i>	FE, SSI	Two populations in San Bruno mountain and the Cordelia Hills are recognized. Hostplant is Viola pedunculata, which is found on serpentine soils. Most adults found on east-facing slopes; males congregate on hilltops in search of females.	No Potential. The Project Area is not within the area of the two known populations for this species.	No further actions are recommended for this species.



NORFOLK ISLAND PINE REMOVED



City Council Agenda Item Staff Report

CITY OF SAN BRUNO

DATE: September 24, 2019

TO: Honorable Mayor and Members of the City Council

FROM: Jovan D. Grogan, City Manager

PREPARED BY: Darcy Smith, Community and Economic Development Director

SUBJECT: Receive Report on the San Mateo Avenue Conceptual Streetscape Plan, and Provide Direction to City Staff

BACKGROUND:

Preparation of the San Mateo Avenue Conceptual Streetscape Plan (Plan) has been underway since City Council authorized the service contract with Wallace, Robert, & Todd (WRT) in January 2019. The draft Plan is included within the report (see Attachment 1). After seven months of preparation, the draft Plan was completed in July 2019 and it includes a conceptual design guideline for the San Mateo Avenue. At its regularly scheduled meeting on August 20, 2019, the Planning Commission received public testimonies, accepted the report and provided comments regarding various elements of the Plan. The Planning Commission also voted unanimously to forward the draft Plan to the City Council for final adoption. The comments and input from the Planning Commission meeting are included in Attachment 2 and will be incorporated after Council's comments and feedback are provided.

The Plan serves as a design guideline to create an inviting and cohesive downtown atmosphere for pedestrians, bicyclists, and motorists. The Plan also provides design recommendation and elements that beautify and enhance the San Mateo Avenue public realm.

The intent of the September 24, 2019 City Council Study Session is to introduce the draft Plan, to answer any questions and to receive comments from City Council before formal adoption of the San Mateo Avenue Conceptual Streetscape Plan in October 2019.

The 2009 General Plan envisions and promotes the San Mateo Avenue downtown area as "*the symbolic heart of the city.*" Specific design and implementation policies were set in place to "*provide the residents with a pleasant and economically vital commercial and entertaining destination.*" Such policies include improving the visibility of the downtown, expanding the streetscape amenities and upgrading and enhancing the downtown appearance.

Further, building upon the implementation policies of the *General Plan*, the 2013 Transit Corridors Plan (TCP) focuses on articulating the community's vision for revitalized commercial corridors in proximity to the San Bruno Avenue Caltrain Station. The vision includes fostering dynamic architecture and welcoming gateways, convenient transportation connections, pedestrian-oriented "green" streets, and additional housing, jobs, retail, and restaurants, while maintaining a sense of the City's history. The TCP identifies San Mateo Avenue - the Central Business District/Downtown, as one of the five character areas for public realm improvements. The other

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character areas are Station Area, El Camino Real, Huntington Avenue and Civic Center as designated in the Transit Corridors Plan. The TCP also identifies San Mateo Avenue Streetscape improvements as a short-term implementation project to enhance San Mateo Avenue - the Central Business District.

In recognition of these policies set in the General Plan and TCP, at its January 8, 2019 meeting, the City Council adopted Resolution 2019-09 authorizing the City Manager to execute a consultant services agreement with Wallace, Robert, & Todd (WRT) in the amount of \$125,000 from the City's General Fund balance to prepare the San Mateo Avenue Conceptual Streetscape Plan. The draft Plan preparation process which included various community outreach sessions, took approximately seven months, from January through August 2019, to complete.

Community Input

In the early stage of Plan preparation, staff and consultant underwent a thorough community engagement effort that intended to solicit ideas for the Conceptual Streetscape Plan. Collectively, community input was gathered that mapped downtown's assets and challenges, prioritized design guidance and principles, and outlined desired activities for downtown's future. Through the process, it was evident that the community envisions a unique and elegant design character for the downtown area. The community has a high interest in further activating San Mateo Avenue through linking existing paseos, Posy Park and Centennial Plaza. Additional vegetation and landscaping that can create a welcoming atmosphere for San Mateo Avenue was emphasized, too. In addition, the community voiced to ensure pedestrian safety, to better promote the downtown by installing wayfinding signage and gateways. These inputs have been carefully incorporated into the draft Plan.

Planning Commission Input

The Plan was first introduced to the Planning Commission at its May 7, 2019 meeting as a study session discussion. At the May 7, 2019 meeting, staff provided updates of the project, shared results from the community engagement effort and received input from the Commission on the conceptual designs. The Commission was not provided the draft Plan for review at the time.

After the initial introduction in May, staff presented the actual draft Plan for review and received feedback from the Planning Commission in August 20, 2019. The Planning Commission accepted the report, received public testimonies, provided comments and inputs to staff and forwarded the Plan for consideration by the City Council. Planning Commission's comments and feedback are included in Attachment 2 and will be incorporated into the Final Plan along with City Council's input.

DISCUSSION:

Project Boundary

The project location and limits are within the public right-of-way of San Mateo Avenue between Huntington Avenue at the Caltrain Station to the north and the intersection with El Camino Real to the south. The area encompasses approximately 0.65 miles section of San Mateo Avenue in the downtown area. The planning area comprises all publicly-owned property and does not include any private property.

Existing Conditions

Although San Mateo Avenue is measured a 60-foot right-of-way, with two travel lanes, bulb-outs at intersections and mid-block crossing and parallel parking and an average of ten to twelve-foot wide side walk on both sides, the street seems much narrower than its actual scale. Existing businesses along San Mateo Avenue include restaurants, retail shops, service-oriented stores such as hair or nail salon as well as educational centers and banks. Building architectural style ranges from Art-Deco to Modernist. Although the eclecticism of the buildings is encouraged by cultural diversity represented in the community, the overall character of San Mateo Avenue lacks a sense of architectural and streetscape design consistency and unity.

Further, parking in the downtown is of a great value. The majority of downtown businesses rely on offsite street parking and existing surface parking areas behind San Mateo Avenue. However, as the Downtown Parking Management Plan finds, the downtown area lacks clear wayfinding and directional signage that can direct drivers and pedestrians to these parking lots. Paseos that connect San Mateo Avenue to the parking lots are not well lit and underutilized.

The San Mateo Avenue Conceptual Streetscape Plan Objective and Goals

The proposed streetscape design will maintain the existing layout of the street and most of the existing curb-line. The proposed improvements will be limited to the public right of way and are focused on the sidewalk and planting areas from the back-of-curb to the property line. The scope of this Plan does not include improvements to building facades or private properties. The Plan will further the vision and goals as set in the implementing policies of the General Plan and TCP. The plan aims to improve the existing downtown aesthetic appearance and to create a more inviting environment for the community and visitors alike. The objective is to provide design guidance for the public right-of-way and to support the City's goal of beautifying the public realm, supporting local business, spurring investment within the downtown core, enhancing the downtown's character, and increasing the attractiveness of the downtown as a destination.

Six distinct goals and their purposes are identified in the Plan:

- Activation:** To institute non-physical improvement to the San Mateo Avenue corridor through social programs such as farmer's market, street fair or music concerts in addition to linking existing paseos (alleys), Posy Park and Centennial Plaza area.
- Greening:** To beautify San Mateo Avenue with the assistance of installing additional vegetation and landscaping that create a welcoming atmosphere and are native to San Bruno.
- Beautification:** To create a unified, and updated image through a unique and elegant design element for the downtown.
- Safety:** To enhance and promote pedestrian safety along San Mateo Avenue corridor through the implementation of additional lighting.
- Way finding:** To better direct traffic to the downtown area and to the nearby existing parking lots by installing wayfinding and gateway signs.
- Identity:** To achieve a cohesive design character for the downtown

Activation

In addition to physical improvements, the Plan proposes a variety of non-infrastructure program to activate the corridor. These programmatic ideas and activity needs expressed by the community through the community engagement process includes farmer’s market, music concerts, games, art events and competitions, street fairs, seasonal events and special dining events in these “special places”. See representative images below.



Representative Images only

Ideas for the Centennial Plaza

Greening

One of the key recommendations includes the installation of regularly spaced street trees in at-grade tree wells. The Plan recommends tree planting at approximately 40-foot on-center. Trees are proposed in areas such as the existing midblock and at corner bulb-outs to avoid line-of-sight conflict. Currently, there are 45 trees in pots with 11 trees planted in-grade. The Plan recommends to increase the number of street trees to a total of 118 trees and to add approximately 8,000 square feet of additional landscaping planting area along San Mateo Avenue. Proposed tree types include *Ulmus parvifolia* (Chinese elm) and *Koelreuteria bipinnata* (Chinese flame tree). Chinese Elm has been planted in front of the recently completed Aperture building at 406 San Mateo Avenue as a prototype street tree; it has been observed to date to be growing successfully. Examples below illustrate at-grade suggested landscaping options for the intersection of San Mateo Avenue and Jenevein Avenue.



Proposed at grade plantation at the corner of San Mateo Avenue and Jenevein Avenue

Beautification

The proposed design character that was developed through the community-engagement process emphasizes a “*simple, elegant and unique*” design. Design elements for the light fixtures, street furnishings and hardscape include the use of stainless-steel or silver-colored metal (e.g., aluminum or powder coated steel), simple cast-in-place concrete, permeable unit pavers in the parking strip with a consistent gray color, rectangular or square in shape with crisp finish that are aligned with the tree-planting zone as the primary material.

Safety

A broad consensus emerged through the community-engagement process that improved lighting should be a priority of the design. A preliminary photometric analysis confirmed that the existing streetlights do not provide adequate light levels or uniformity in the downtown. Improved lighting is recommended to help activate the street at night which would help utilize the under used areas of the street due to safety concerns, especially during night time. The proposed lighting design includes installation of pedestrian-level, pole-mounted light fixtures in the paseos to provide lighting for safety and night time activation. Secondly, the Plan recommends to install accent lighting in enhancing visual interest at night. Accent lighting can also create a sense of unique character and attractiveness and create street ambience. Other lighting recommendations such as seasonal lighting and gateway lighting are illustrated in the Plan as well. Representative pictures below include examples of pedestrian-level lighting, seasonal lighting and street lights.



Representative Images only

Way Finding and Gateway

Another identified goal in the Plan is to increase the visibility of downtown by installing way finding and gateway signage. The design intent is to prioritize simplicity and legibility and to promote existing public parking lots off of San Mateo Avenue. A signage and gateway program is recommended for San Mateo Avenue that includes parking way finding signage, vehicular corridor directional signage, paseo gateway monuments and gateway monuments. The gateway monuments are proposed to mark the boundaries of the downtown and to create a sense of place and belongingness.

Identity

The Plan proposes a design character for the downtown that helps create a cohesive identity for San Mateo Avenue. In addition to a cohesive design palate, the Plan encourages the activation of special places such as paseos, Posy Park, and Centennial Plaza.

Consistency with Adopted Planning Documents

General Plan

The Plan is consistent with *General Plan* policies related to improving the image of downtown as a welcoming business district to shop, dine and entertain.

Transit Corridors Plan (TCP)

The TCP identifies San Mateo Avenue Streetscape improvements as a short-term implementation project to enhance the Central Business District and as one of the five character areas for public realm improvements. The Plan proposes several infrastructure improvements that are consistent with the public realm design guidelines policies in the TCP.

San Bruno Walk'n Bike Plan

The *Walk'n Bike Plan* identifies San Mateo Avenue as needing additional streetscape improvements to improve the pedestrian network, encourage safer sidewalks with better lights, tree planting at grade that improves pedestrian comfort, more signage, better pavements and more seating areas.

Downtown Parking Management Plan (PMP)

The *Downtown Parking Management Plan* recommends efficient parking strategies in the downtown. The Plan would help implement the PMP by providing way finding and directional sign.

Green Infrastructure Plan (GI Plan)

The City's GI Plan was recently adopted by City Council on August 27, 2019 and the draft Plan has incorporated design features that will improve the water quality of San Francisco Bay, augment local water supplies, reduce flooding and increase green space. Specifically, the Plan will incorporate green and sustainable design elements, plant street trees in tree wells, install naturally drained landscape and encourage the use of native and drought-tolerate vegetation.

Environmental Review

The Plan provides a design guideline that includes minor improvement to area within the public right-of-way such as crosswalk stripping, tree planting, landscape, wayfinding signs and light poles installation. Therefore, the project qualifies for a Categorical Exemption under CEQA guidelines Section 15301 (c) for minor alteration to existing streets and sidewalks.

Other Consideration –Angled Parking Conversion on San Mateo Avenue

Through the Plan preparation process, the community expressed desire in maintaining all existing on-street parking spaces and the current two-way traffic pattern. Consequently, staff and WRT engaged the traffic consultant, Parisi Transportation Consulting, to explore the possibility of converting existing parallel parking spaces to either 60-degree or 45-degree angled parking configuration. In conclusion, as stated in the memo that is included in the draft Plan, the consultant recommended against the angled parking conversion since the existing roadway is not wide enough to accommodate angled parking and two-way travel lanes.

The memo explains that if 60-degree parking is proposed on one side of San Mateo Avenue and the existing two-way travel lanes are to remain, a minimum of 75.8-ft would be required (19.8-ft stall depth + 28-ft one travel lane x 2, see illustration below). This would be 15.8-ft short of existing roadway width since San Mateo Avenue is a 60-ft right-of-way. If one-side 60-degree parking is proposed and current traffic direction is limited to one-way only (19.8-ft + 28-ft = 47.8-ft), the result may yield five additional spaces.

FIGURE 1
 PARKING DIMENSION TABLE

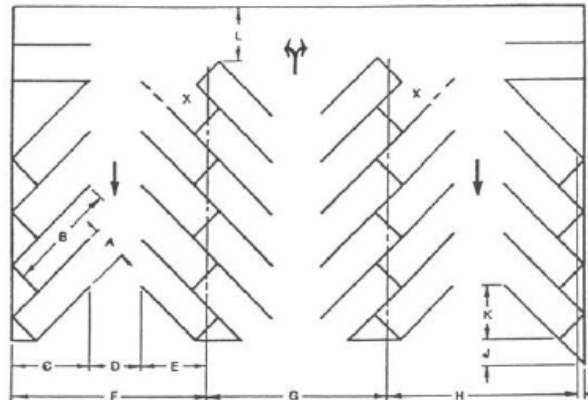


Table 1: San Mateo's Standards for Aisle Width for On-Street and Off-Street Parking Stalls

Dimension	Diagram	45 Degree		60 Degree	
		Off-Street	On-Street	Off-Street	On-Street
Stall Depth	C	18.70	18.70	19.80	19.80
Aisle Width Between Stall Lines	D	12.00	28.00	16.00	28.00
For on-street parking use the above dimensions, adding 16 feet to dimension "D" for 45-degree parking and 12 feet for 60-degree parking					

Further, if 45-degree parking is proposed on one side of San Mateo Avenue with maintaining the existing two travel lanes, a minimum of 64.7-ft would be required (18.7-ft stall depth + 28-ft one travel lane x 2). This would be 4.7-feet short of existing roadway width. If one-side 45-degree parking is proposed with only one-way traffic, then a total of 31 parking spaces would be eliminated.

Because the conversion of current two-way traffic into a one-way street was not part of the Streetscape Plan scope and would require additional traffic analysis and further community / downtown business merchant engagement, staff does not recommend the one-way traffic conversion on San Mateo Avenue even though it may potentially generate five additional parking spaces.

In summary, since the community's desire is to maintain all existing on street parking and the current two-way traffic direction, the consultant concluded and recommends against the angular parking conversion proposal to be included in the Plan.

Conclusion

As a conceptual design plan, the draft San Mateo Avenue Conceptual Streetscape Plan will guide the next steps of the design process, while leaving flexibility for future implementation. The final implementation will depend on project budget once funding is identified and on potential technical constraints that were beyond the scope of this study.

NEXT STEPS

With input and direction received from City Council, staff will finalize the San Mateo Avenue Conceptual Streetscape Plan and present it for City Council's adoption in October 2019.

FISCAL IMPACT:

The cost of the Plan preparation has been fully funded by the City's General Fund, as authorized by the City Council. At this time, the City has not identified a funding source to implement the Conceptual Streetscape Plan at an estimated cost of \$9.333 million for the streetscape improvements and an additional \$3.478 million for the Centennial Plaza improvements and \$6.291 million for the Posy Park improvements . The total cost is estimated at \$19.102 million. A detailed cost estimate with priority ranking from one to four is included in Appendix E of the Plan. This project was identified as potential project in the City's Development Impact Fee Nexus Study, and future sources of funding could include Development Impact Fees, as well as grants or community benefit payments from development projects. Further, a ranking and rough-order-of-magnitude cost estimates of the proposed improvements were calculated and included to guide budgeting decisions for future consideration.

The Plan guides the next steps of the design process, while leaving flexibility for future implementation. The final implementation will depend on project budget once funding is identified and on potential technical constraints that were beyond the scope of this study.

RECOMMENDATION:

Receive report and provide comments to staff on the San Mateo Avenue Conceptual Streetscape Plan (Plan).

DISTRIBUTION:

None

ATTACHMENTS:

1. Draft Plan
2. Planning Commission comments and feedbacks

DATE PREPARED:

September 11, 2019

SAN MATEO AVENUE

CONCEPTUAL STREETScape PLAN



ATTACHMENT 1



City of San Bruno, CA | September 2019

CITY COUNCIL REVIEW DRAFT

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CITY COUNCIL REVIEW DRAFT

CHAPTER 1: INTRODUCTION

- A. CONTEXT
- B. PROJECT BACKGROUND
- C. PROJECT OVERVIEW
- D. SCOPE AND PURPOSE OF THE SAN MATEO AVENUE STREETScape PLAN
- E. PLANNING CONTEXT
- F. CEQA COMPLIANCE
- G. PROCESS
- H. GOALS

A. CONTEXT

The City of San Bruno, an ethnically and culturally diverse city, maintains a small-town atmosphere within a large metropolitan area. It is uniquely located on the San Francisco Peninsula in San Mateo County, about twelve miles south of San Francisco. Located along Highway 101 and Interstate 280, accessible by Bay Area Rapid Transit (BART), Caltrain and the San Mateo Transit (SamTrans) bus system, San Bruno is a transit hub for the bay area. Adjacent to the San Francisco International Airport, the City has easy accessibility to the vast cultural, educational, and recreational opportunities within the San Francisco Bay Area.

San Mateo Avenue, from Huntington Avenue to El Camino Real, serves as San Bruno's downtown central business district. A two-lane, low-speed roadway lined with shops, restaurants and services, it functions as a pedestrian-oriented shopping district, where visitors may park once and visit multiple destinations.

One of the most striking and valuable assets of San Mateo Avenue is the diverse community that is served by businesses along the corridor. In a region where cultural diversity is not uncommon, San Mateo Avenue stands out because of the varied mix of restaurants, shops and services that line the street. There are very few vacancies, the existing business seem to be thriving. Many businesses have been in their locations for some time, while several new businesses are finding success. Not only are these shops

and restaurants ethnically diverse, but a range of ages and stages of life are served, from toddlers at daycares to children learning to swim, newlyweds at the wedding chapel and wedding photo shop, to new parents. The range of businesses and services is striking, including restaurants; fitness, yoga, martial arts and dance classes; the Volunteer Firemen's Hall; grocery stores; hair salons; dry cleaners; laundromats; a leather supply store; a notary; an auto parts store; banks; bakeries; and more. A surprisingly large number of these businesses are sole-proprietor, locally owned businesses.

B. PROJECT BACKGROUND

In 2013, the City adopted the Transit Corridors Plan which articulates the community's vision for revitalized commercial corridors in proximity to the San Bruno Caltrain Station. The vision includes fostering dynamic architecture and welcoming gateways, convenient transportation connections, pedestrian-oriented "green" streets, and additional housing, jobs, retail, and restaurants, while maintaining a sense of the City's history. The Transit Corridors Plan identifies five character areas for public realm improvements: Station Area, El Camino real, Central Business District, Huntington Avenue and Civic Center. San Mateo Avenue is the main corridor of the Central Business District area. The San Mateo Avenue Streetscape Plan represents the further development of recommendations from the Transit Corridors Plan.

C. PROJECT OVERVIEW

The project location and limits are within the public right-of-way on San Mateo Avenue between Huntington Avenue at the Caltrain Station to the north and the intersection with El Camino Real to the south. San Mateo Avenue is downtown San Bruno's main street. It has a 60-foot right-of-way, with sidewalks approximately eight to ten feet wide, eight foot wide bulb outs at intersections, parallel parking on both sides of the street, and 12-foot vehicle travel lanes. Extending from Huntington Avenue to the north to El Camino Real to the south, San Mateo Avenue stretches approximately 0.65 miles. At the northern entrance to downtown is the Caltrain Station and adjacent, newly renovated Posy Park. Both of these entrances to Downtown San Bruno offer opportunities to be designed as prominent and inviting gateways.

The objective of this plan is to provide design guidance for the public right-of-way to support the City's goals of beautifying the public realm, supporting local businesses, spurring investment within the downtown core, enhancing the downtown's character, and increasing the attractiveness of the downtown as a destination.

D. SCOPE AND PURPOSE OF THE SAN MATEO AVENUE STREETSCAPE PLAN

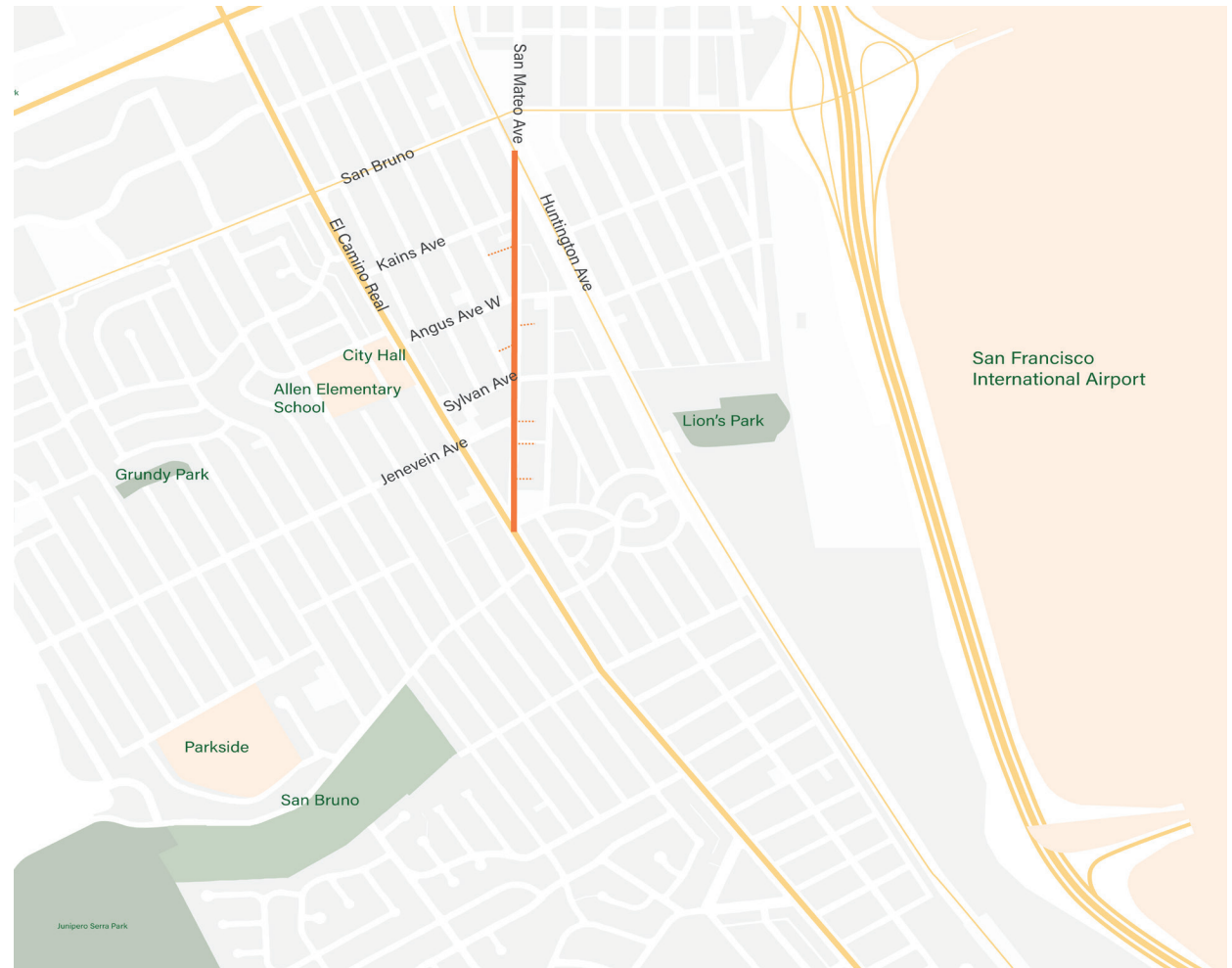
The San Mateo Avenue Streetscape Plan describes the concept-level design intent that was developed through a community-engagement process. It describes the following:

- Project Vision and Goals: purpose and intent
- Design Character: aesthetic theme, comparable precedents, materials
- Project Scope: area of the project, elements of the design, quantities of the elements, locations of the elements
- Prioritization of Elements: provides a guide for decision-making once a project budget is identified, as the entire scope described may not be funded

As a concept plan, this document will guide the next steps of the design process, while leaving flexibility for future implementation. The ultimate design will depend on the project budget once funding is identified, and on potential technical constraints that were beyond the scope of this study to identify. For example, constraints may be found after a full topographic analysis, dependent on a topographic survey, is complete. The design process subsequent to adoption of this plan and determination of the project funding would typically include the following steps:

- Schematic Design: Define the scope of the project including limit-of-work, included elements, quantities, and basic layout

PROJECT AREA CONTEXT MAP



- Design Development: Selection of materials based on the project budget, and refined layout based on technical factors
- Construction Documentation: Development of contract documents that precisely describe the design to a contractor and are the basis for bidding and procurement

E. PLANNING CONTEXT

This plan is informed by two prior planning studies, the Transit Corridors Plan and the San Bruno Downtown Parking Study. Key recommendations from these plans are described below.

Transit Corridors Plan

Adopted in February 2013, the Transit Corridors Plan defines the San Mateo Avenue study area from the Caltrain overpass to El Camino Real as the city's "Central Business District." It articulates a vision for the corridor as a "pedestrian-oriented and green" street that provides a "pleasant outdoor shopping experience" and a "one-of-a-kind mix of ethnic markets and goods." The future character of the street is described as an "artistic place" with a "whimsical yet handsome combination of old and new characteristics." Beyond serving as a shopping district, the vision for the corridor includes supporting cultural programming, such as a "diverse range of performers and musicians [who] fill up the streets and restaurants." The vision also includes future upkeep of the street, describing it as "well maintained, clean and inviting." And the vision includes the

development of "new urban plazas and pocket parks" beyond the street right-of-way.

"Vision Elements" described in the Transit Corridors Plan that are further developed by the San Mateo Avenue Streetscape Plan include:

Downtown as a Day and Night

Destination

- Make San Mateo Avenue into an exciting destination for visitors, workers and residents alike.
- Invigorate existing businesses and program activities to support an economically vibrant Downtown that is busy with business and community life.

Local Character and Distinctive Identity

- Cultivate a more distinctive identity for San Bruno while building upon its local character in future growth.
- Integrate San Bruno history into public art and streetscape designs while also developing new themes and motifs for a contemporary identity.
- Install exciting and attractive new gateways to direct visitors and patrons to destinations while creating a welcoming atmosphere.

Sustainable, Mixed-Use Development

- Explore opportunities for "sustainable" infrastructure that beautifies the urban environment with ecological technologies such as "green streets" and drought-tolerant plantings.

Safe and Inviting Pedestrian Realm

- Develop circulation and streetscape

improvements that provide highly visible crosswalks, sidewalk plantings, engaging sidewalk design and traffic calming strategies to support a safe and inviting pedestrian realm.

The San Mateo Avenue Streetscape Plan adheres to the Public Realm Design Guidelines described in the Transit Corridors Plan. Specifically, the San Mateo Avenue Streetscape Plan incorporates the following:

- A1-1: Reduce crossing distance at crossing locations by utilizing pedestrian safety features such as bulb-outs.
- A2-3: Explore using special paving materials, colors, and/or patterns for crosswalks to heighten visibility and lend identity to the area while creating an attractive pedestrian environment.
- A3-3: Ensure that planters and tree wells are at least four feet wide.
- A3-8: Promote outdoor dining and display of selected goods on sidewalks.
- A3-9: Ensure at least a 12-foot tree canopy clearance.
- A3-10: Place new street trees in appropriate locations to avoid blocking views and access to building entrances or signage.
- A3-11: Ensure that trees do not obstruct ADA access, or infringe on pedestrian and/or bicycle circulation.
- A4-1: Provide both pedestrian-oriented and

automobile- oriented street lighting within the whole Transit Corridors Plan area, with first priority to the Pedestrian Emphasis Zones designed to meet established lighting standards to provide safe and comfortable pedestrian environment.

- A4-2: Provide pedestrian-friendly streetscape amenities.
- A4-3: Provide bicycle racks.
- A4-4: Explore opportunities for artistic design of street furnishings.
- A4-5: Install public art pieces.
- A4-7 to A4-9:

Provide shelters at bus stops where possible; provide a minimum six-foot sidewalk clearance; design visually iconic bus shelters.

- A5-1: Develop consistent thematically branded wayfinding and signage.
- A5-3: Design and install gateway amenities.
- A5-7: Provide visually attractive, easy-to-read and well-located signage to direct vehicles to Downtown parking areas.
- A6-1&2: Install stormwater planters where possible.
- A6-3: Encourage the use of permeable pavers around tree wells.
- A6-5: Explore the use of permeable paving materials along parking lanes.

- B4-4: Retain bulb-outs at street crossings.
- B4-5: Plant street trees in tree wells with grates rather than in pots.
- B4-6: Install raised crosswalks at all key intersections.
- B4-7: Install attractive and creative pavement materials.

Three recommendations from the Transit Corridors Plan were studied and excluded from the scope of the San Mateo Avenue Streetscape Plan:

- Provide, where feasible, angled parking (Guideline B4-3)

Angled parking was studied as part of the San Mateo Avenue Streetscape Plan process and found to be unfeasible due to the limited width of the street (see Appendix).

- Raised Crosswalks at all Key Intersections (Guideline B4-6)

The potential to include raised crosswalks was discussed during stakeholder meetings and was determined not to be a priority for further study. Raised crosswalks could conflict with stormwater drainage and are generally not desired by transit agencies.

- Reconfiguration of the intersection of San Mateo Avenue and El Camino Real (Guideline B4-8)

City staff determined that the reconfiguration of the intersection of San Mateo Avenue and El Camino Real should be considered outside the

scope of the San Mateo Avenue Streetscape Plan because it is likely to be a longer-term implementation project than the rest of the streetscape plan (requiring property acquisition), and because it would trigger CEQA analysis by altering the traffic operations of both streets. The San Mateo Avenue streetscape design allows for the potential future reconfiguration of the intersection by keeping furnishings, trees and the southern gateway monument outside of the realignment footprint described in the Transit Corridors Plan.



El Camino Real Reconfiguration from Transit Corridors Plan

Downtown Parking Management Plan:

Adopted in January 2019, the Downtown Parking Management Plan provides a set of phased parking management recommendations to manage the high afternoon and evening parking demand in the study area (including San Mateo Avenue), help users find and use available parking, improve parking availability for residents, and potentially increase the parking supply. Specifically, the Downtown Parking Management Plan recommends that the city:

- Adjust enforcement hours to better manage the heavy-use evening period
- Adjust time restrictions, primarily to convert 5-hour spaces to 10-hour spaces for employee use
- Install improved signage to help drivers locate available parking
- Improve parking lot maintenance and security
- Explore temporary use of the Sylvan Avenue Caltrain Station as additional public parking
- Explore converting parallel parking on San Mateo Avenue to diagonal parking to increase capacity
- Install parking meters on San Mateo Avenue to encourage short-term parking and direct long-term parkers into lots
- Formalize overnight parking arrangements in public lots to increase supply available to residents
- Begin process of planning and securing funds for a parking garage

The San Mateo Avenue Streetscape Plan supports these recommendations by including:

- Wayfinding signage to off-street parking
- Improved security and wayfinding through the alleyways (or “paseos”) that lead from the street to the off-street parking
- Locations and quantity of parking meter kiosks

The San Mateo Avenue Streetscape Plan planning process included the analysis of the potential to install diagonal parking on San Mateo Avenue. Parallel parking was determined not to be feasible due to the limited width of the street. (See Appendix.)

F. CEQA COMPLIANCE

The Transit Corridors Plan completed a program-level EIR that illustrates relevant environmental resources along San Mateo Avenue that would apply to the recommendations described in the San Mateo Avenue Streetscape Plan. The proposed improvements should fall within the classes of projects as defined in the Public Resources Code to have been determined not to have a significant effect on the environment and are thus exempt from CEQA. As this project will make modifications to existing infrastructure but make negligible or no expansion of the existing use, it should qualify for a Categorical Exemption under Section 15301 of the CEQA Guidelines, which applies to minor alterations of existing infrastructure within the public’s right-of-way. The City of San Bruno can further support this finding by imposing standard conditions

of approval on the project. (See Appendix for further explanation of CEQA compliance.)

G. PROCESS

This plan is the outcome of a public-engagement process that included the following meetings and workshops:

- March 12, 2019 - ‘Walk’shop – a walking tour, Community Workshop #1
- March 28, 2019 - Stakeholder Meeting #1
- March 30, 2019 - Drop-in Community Workshop #2
- March 11 - April 10, 2019 – On-line Survey
- May 7, 2019 - Planning Commission Meeting #1
- May 8, 2019 - Stakeholder Meeting #2
- May 15, 2019 - Parks and Recreation Meeting
- May 16, 2019- Arts Commission Meeting
- May 22, 2019 - Community Meeting #3
- August 20, 2019 - Planning Commission Meeting #2

PUBLIC ENGAGEMENT PROCESS



March 12th 'Walk'shop



March 30th Community Meeting



H. GOALS

Goals for the streetscape improvements were developed during the community engagement process by categorizing the community and stakeholders' comments collected during the 'Walk'shop, first stakeholder meeting and from the on-line survey. Draft goals were presented for review and comment at the subsequent community and stakeholders workshops, and Planning Commission, Parks and Recreation Commission, and Arts Commission meetings.

PROJECT GOALS

ACTIVATION

Activate the corridor, alleys (or "paseos"), Posy Park and Centennial Plaza day and night

GREENING

Plant trees, increase vegetation

BEAUTIFICATION

Provide an updated and unified corridor aesthetic, including additional seating opportunities and new paving

SAFETY

Ensure pedestrian safety

WAYFINDING

Provide wayfinding signage to San Mateo Avenue and parking lots and design gateways

IDENTITY

San Mateo Avenue's character should be simple, elegant and unique.

CITY COUNCIL REVIEW DRAFT

CHAPTER 2: EXISTING CONDITIONS

- A. LAND USE
- B. CHARACTER
- C. STREETScape CONDITION
- D. PARKING

A. LAND USE

The land use along the corridor is primarily commercial with a large diversity of business types. Retail stores and restaurants are the primary business types. Service-oriented businesses, such as hair and beauty salons, a laundromat, two banks, an auto-repair shop, a gas station and a yoga studio, also line the street. Several institutional, educational and community-serving businesses also occupy buildings along the corridor, including the Bay Area Entrepreneur Center, the Teamsters Union, and La Petite Baleen Swim School.

At the southern end of the corridor, The Aperture, a new multi-unit mixed-use building, was recently completed. Toward the north end, Artichoke Joe's Casino is a regional draw, and further north is access to the Caltrain Station. Just north of the study area, on Huntington Ave., a new multi-unit residential development is planned.

Open spaces along the corridor are Posy Park on the north end of the study area, the plaza to the north of Chase Bank and Centennial Park across from the Jenevein Ave. intersection.












Behind the buildings, many of the blocks contain surface parking lots, which are connected to the San Mateo Avenue sidewalks via alleyways, referred to as "paseos" throughout this document.

B. CHARACTER

San Mateo Avenue is a small-scale corridor, with one travel lane in each direction, parallel parking on both sides of the street, and 10-foot to 12-foot-wide sidewalks. Bulb-outs at the corner and mid-block crossings serve to further reduce the perceived scale of the street.

The buildings are primarily one and two stories in height, with an eclectic mix of architectural styles ranging from various historical styles to Art-Deco and Modernist. The eclecticism of the buildings is matched by the diversity of uses and ethnicities and cultures represented by the restaurants, shops and services along the street. This lends the street a fascinating and rich character, and not a sense of consistency or unity.

EXISTING ZONING MAP

-  Markets
-  Restaurants
-  Goods/Services
-  Community
-  Open Space
-  Trees
-  Paseos
-  Bulb Outs
-  Architecture of Interest
-  Exist. Active Sidewalk
-  Artwork/Distinctive



Rotary Centennial Clock



Exist. Low use Open Space



460 San Mateo Ave
Art Moderne, 1940



Fountain



Artwork Viewing



Open space



495 San Mateo



Active sidewalk seating



601-605 San Mateo Ave
Mediterranean Revival, 1930



609-617 San Mateo Ave
Mission Revival, 1909



757 San Mateo Ave
Art Moderne, 1935



Artwork: Jenevein Plaza, Jenevein Ave



Active sidewalk seating



588 San Mateo Ave
Mission Revival/Art Moderne 1925



598 San Mateo Ave
Art Moderne, 1932



Active sidewalk seating



Artwork: Posy Park Mosaics

C. STREETScape CONDITION

Despite its assets, the San Mateo Avenue streetscape is considered widely by the San Bruno community to be “run down.”

Pedestrian Realm

The existing street design includes many pedestrian-friendly features, including bulb-outs with code-compliant curb-ramps (in most, but not all cases), high-visibility crosswalk striping, mid-street pedestrian-crossing signs and pedestrian-activated flashing beacons.

Street furnishings include precast-concrete trash receptacles that are unsightly and do not provide for recycling, as well as several mismatched trash receptacles, newspaper racks that are in disrepair, a few (quantity?) raised planters with overgrown shrubs that prohibit sitting on the integrated benches. There are no existing bike racks and no city-owned benches along the sidewalk. Public benches are located at Centennial Park, the Citibank plaza and Posy Park.



Pedestrian friendly crosswalk



Unsightly trash cans with no room for recycling



Newsracks in disrepair



Paving patchwork



Newly replaced asphalt



Existing Light Poles



Uneven Surfaces



Double parked vehicle off-loading



Wide Light Pole Spacing

Sidewalk Conditions

The sidewalk is continuous along both sides of the street, with adequate width for pedestrian access, however the pavement is a patchwork of concrete that has been repaired over time and there are many locations with uneven surfaces that exceed the CBC maximum unevenness for accessibility. There are code-compliance shortcomings at building entrances and sidewalk cross-slopes exceeding 2%.

Vehicular Roadway

The asphalt pavement in the roadway is in excellent condition, having been replaced recently after a water-line and sewer-line replacement project. Because there are no loading or drop-off zones, deliveries and patron drop-offs depend on double parking. While there is enough roadway width to allow vehicles to pass, curbside-management and roadway striping improvements would improve the street's vehicular function.

Lighting

Pole-mounted lights are widely spaced and do not provide adequate light levels.

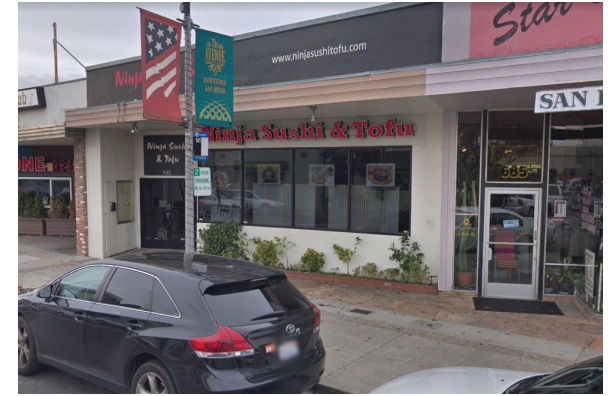


Bus connecting to San Bruno BART

Transit

There are four bus stops along the corridor. These are not constructed per SamTrans standards and they do not provide adequate accessibility for bus riders with disabilities.

San Bruno Station is located at the northern end of the corridor.



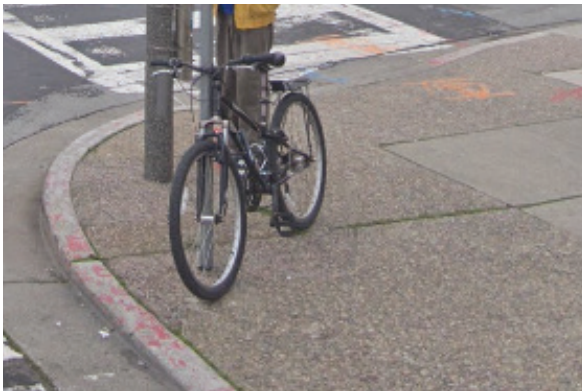
Non-compliant bus stops



San Bruno Station



Bicycle Racks at Posey Park



Bicycle locked up at Sylvan Ave and San Mateo Ave

Bicycle Facilities

There are no bicycle facilities (bike lanes, “sharrows” or signage) in the roadway, and there are no existing bike racks along the street, with the exception of two bike racks and bike lockers at Posy Park.



Trees limited in growth due to pot planting

Planting

Greenery is limited along the street, due primarily to the small size and number of street trees, most of which were planted in raised pots which have limited their growth. Most of the shrubs that are planted at grade or in raised planters are in poor condition or are at the end of their expected life-spans.



Curb Buildout with Planting



Raised planter with overgrown planting

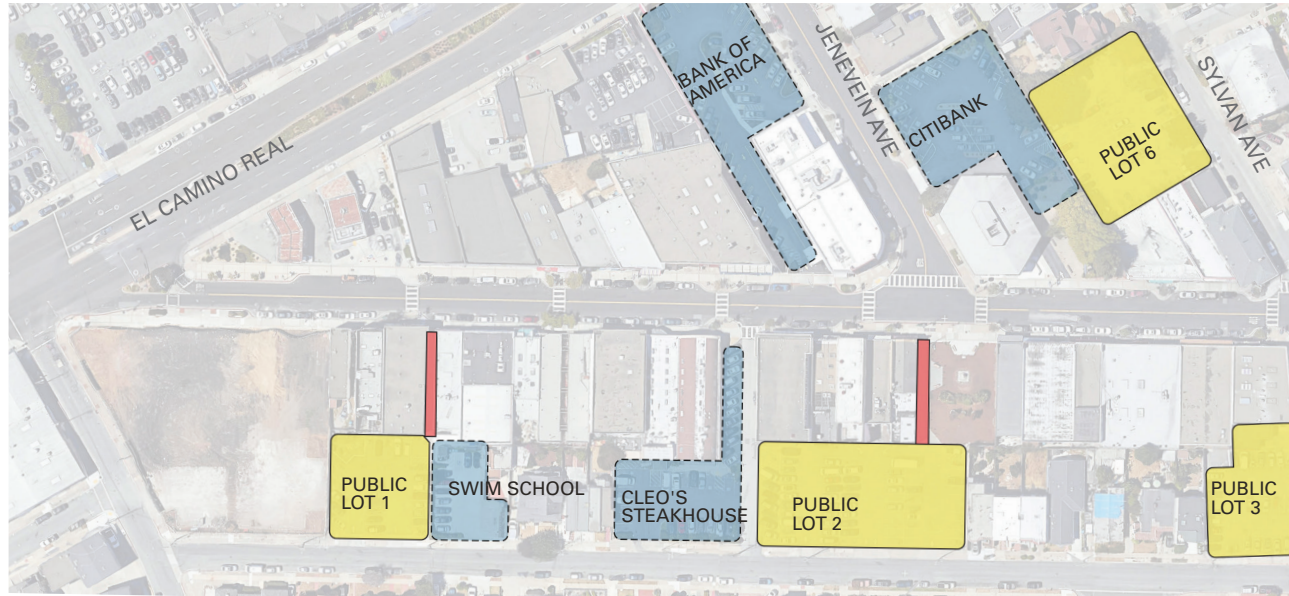
D. PARKING

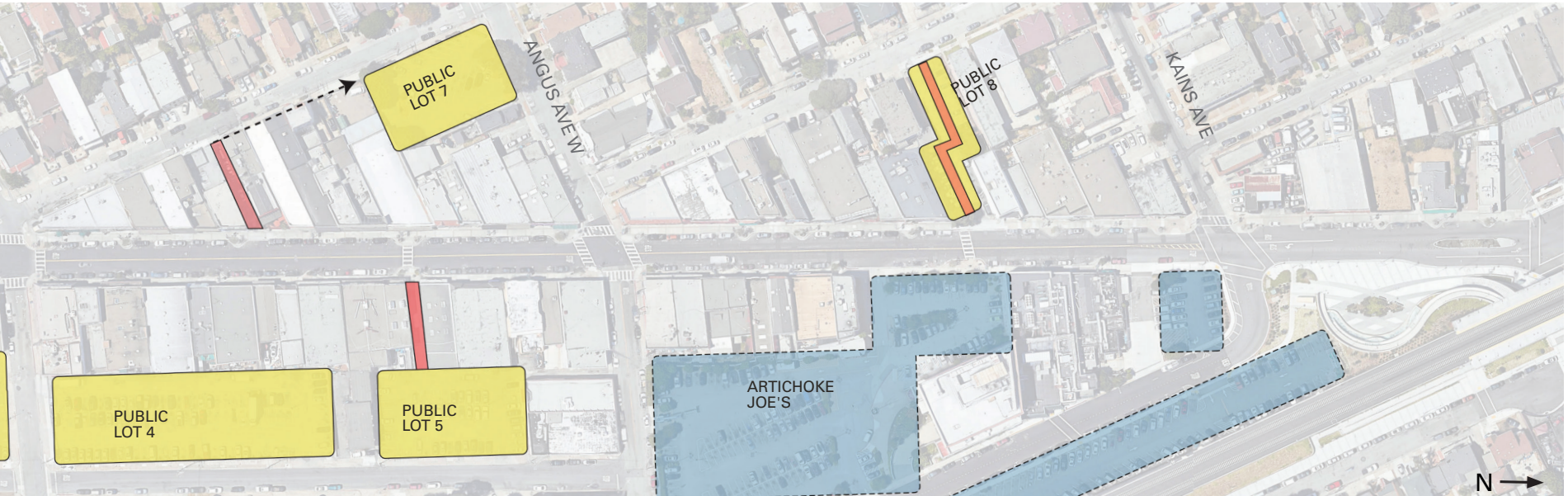
It is clear from the Downtown Parking Management Plan and from community input that parking is of great value along the corridor. Weekend and weekday peak-hour (8:00 p.m. and 6:00 p.m., respectively) on-street parking occupancy on San Mateo Ave. is 91%. Off-street parking occupancy for these times is 81% and 84%, respectively. Partly because of the relative availability of off-street parking, the Downtown Parking Management Plan recommends other means of managing parking demand instead of increasing on-street parking. These recommendations include pricing and enforcement, and increasing the use of off-street parking resources through better signage, connectivity, and lighting improvements.

The San Mateo Avenue Streetscape Plan follows the Downtown Parking Management Plan's recommendations by including:

- Wayfinding signage to direct drivers and pedestrians to and from the on-street parking lots
- Safety and character improvements in the paseos, which provide critical connections to the on-street parking resources
- Locations for parking-meter kiosks

PASEO CONNECTIONS TO EXISTING OFF-SITE PARKING LOTS





- EXISTING PASEOS
- PUBLIC PARKING LOT
- PRIVATE PARKING LOT

CITY COUNCIL REVIEW DRAFT

CHAPTER 3: RECOMMENDATIONS

- A. DESIGN OVERVIEW
- B. BASELINE FURNISHINGS AND HARDSCAPE
- C. OPPORTUNITIES FOR UNIQUE AND ARTISTIC EXPRESSION
- D. GREENING AND STORMWATER MANAGEMENT
- E. MOBILITY AND PARKING
- F. LIGHTING
- G. SPECIAL PLACES
- H. WAYFINDING AND GATEWAYS
- I. ACTIVATION
- J. PRIORITIZATION AND COST ESTIMATE

A. DESIGN OVERVIEW

Project Scope

The proposed streetscape design maintains the existing layout of the street and most of the existing curb-line. The proposed improvements will be limited to the public right of way and are focused on the sidewalk and planting areas from the back-of-curb to the property line. The scope of this study does not include improvements to building facades, private properties, or land-use.

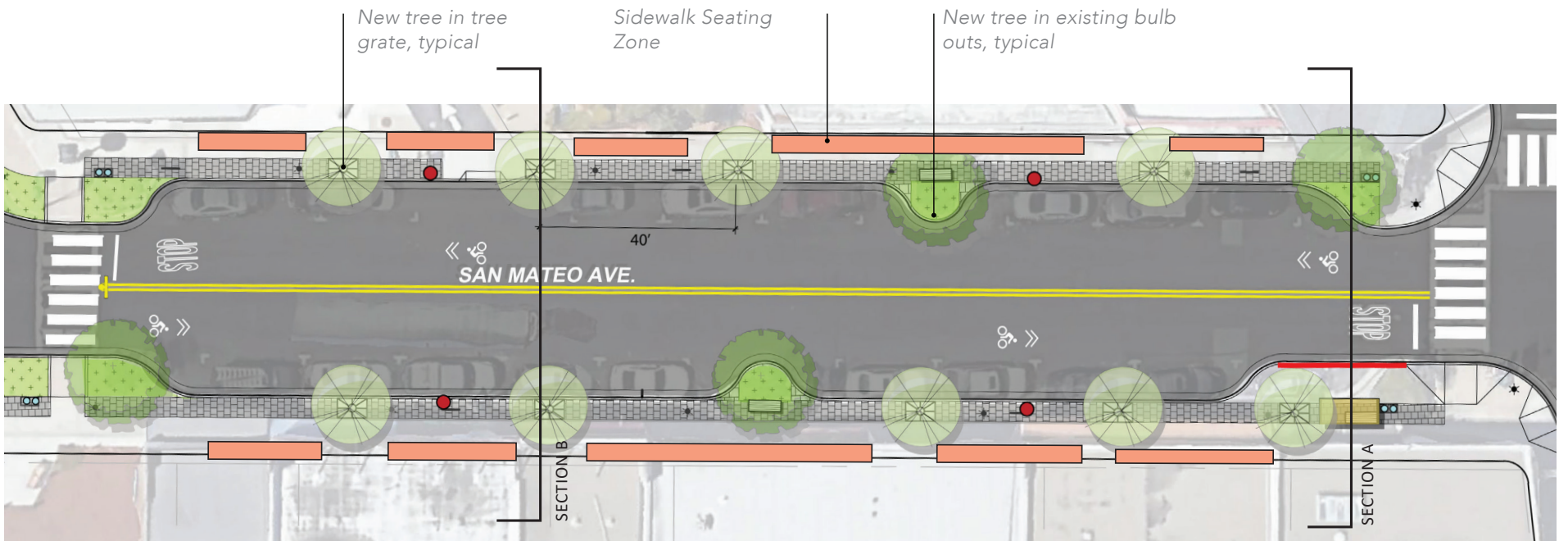
The proposed improvements include:

- Furnishings including benches, bike racks, trash and recycling receptacles
- New sidewalk paving
- Accessibility improvements to bring the public right of way into conformance with current California building code.
- Opportunities for unique and artistic expression, including public art, custom furnishings and special paving
- Improved Planting and Greening
- Stormwater-management features including permeable paving, suspended pavement, and bioretention planting areas
- Mobility and parking improvements including changes to the street and curb striping, improved bus stops, sharrows, and directional signage and lighting to improve access to off-street surface parking
- Lighting improvements, including new streetlights and decorative lighting
- Improvements to special places including Posy Park, Centennial Park, and paseos
- Wayfinding signage and gateways

Design Character

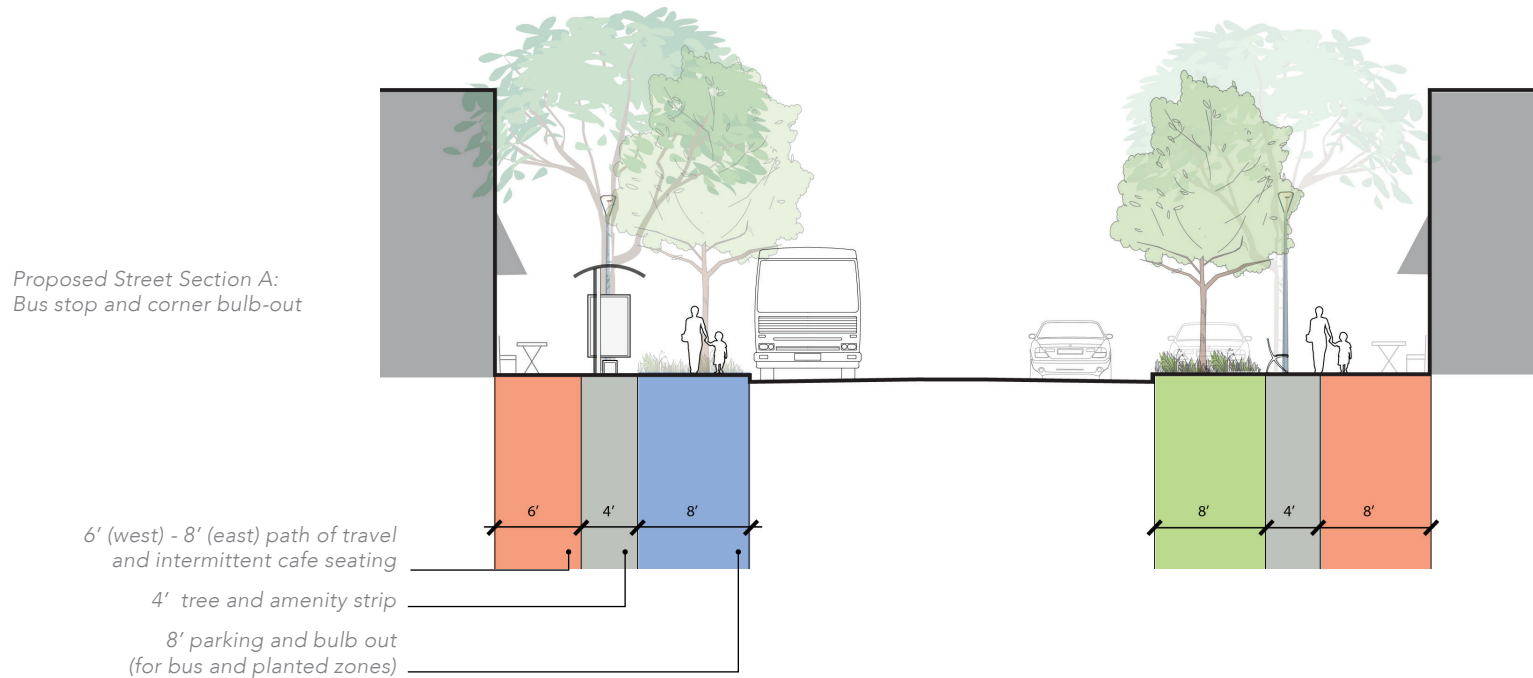
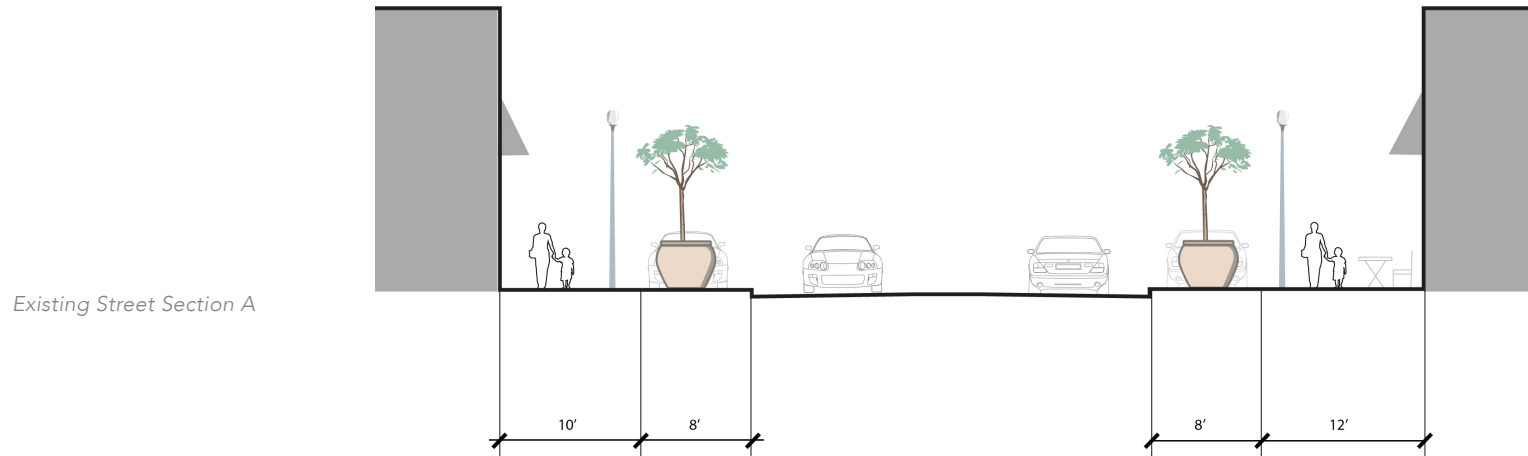
The proposed design character was developed through the community-engagement process. The key words that emerged to describe the preferred character were “simple,” “elegant,” and “unique.” Because the street environment is already diverse and visually complex, the proposed design is intended to be distinguished from its context by being refined, consistent, and simple. At the same time, the design is meant to convey a unique sense of place through the selection of furnishings and materials that are not found on many other streetscapes, and through the implementation of select custom elements and artistic expression in focused locations. The design is meant to be forward-looking and contemporary, while the forms of the furnishings convey a sense of movement related to the city’s connection to transportation.

TYPICAL SIDEWALK USE ZONES

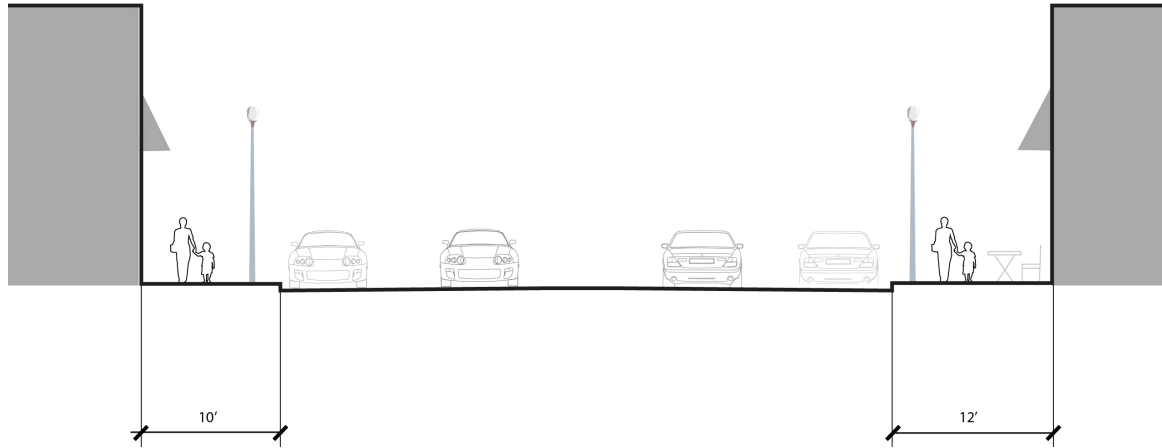


Typical sidewalk use zones plan

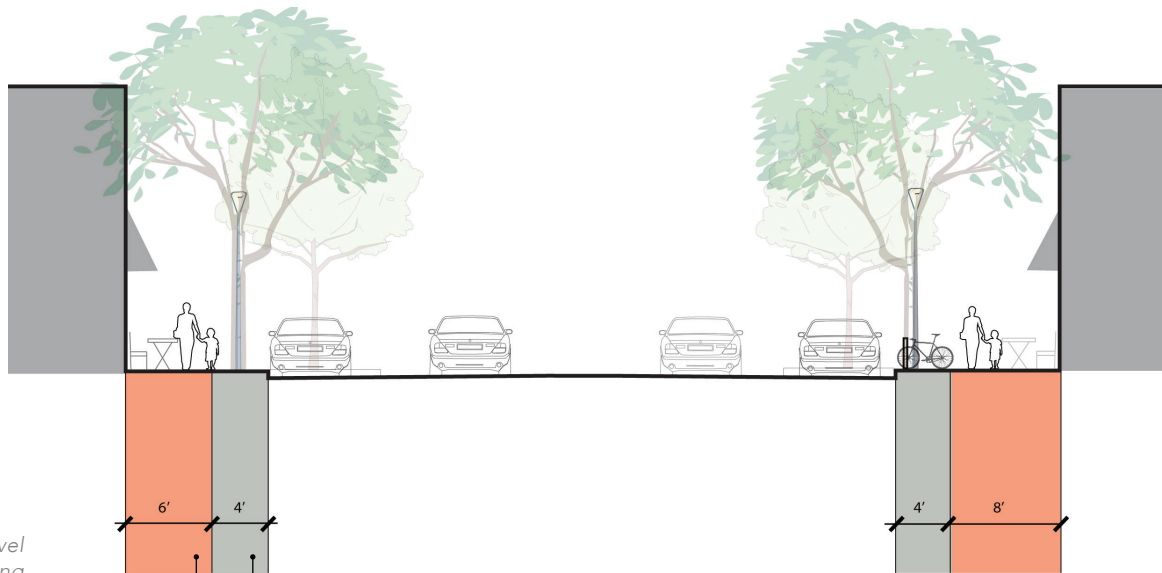
TYPICAL STREET SECTIONS



Existing Street Section B

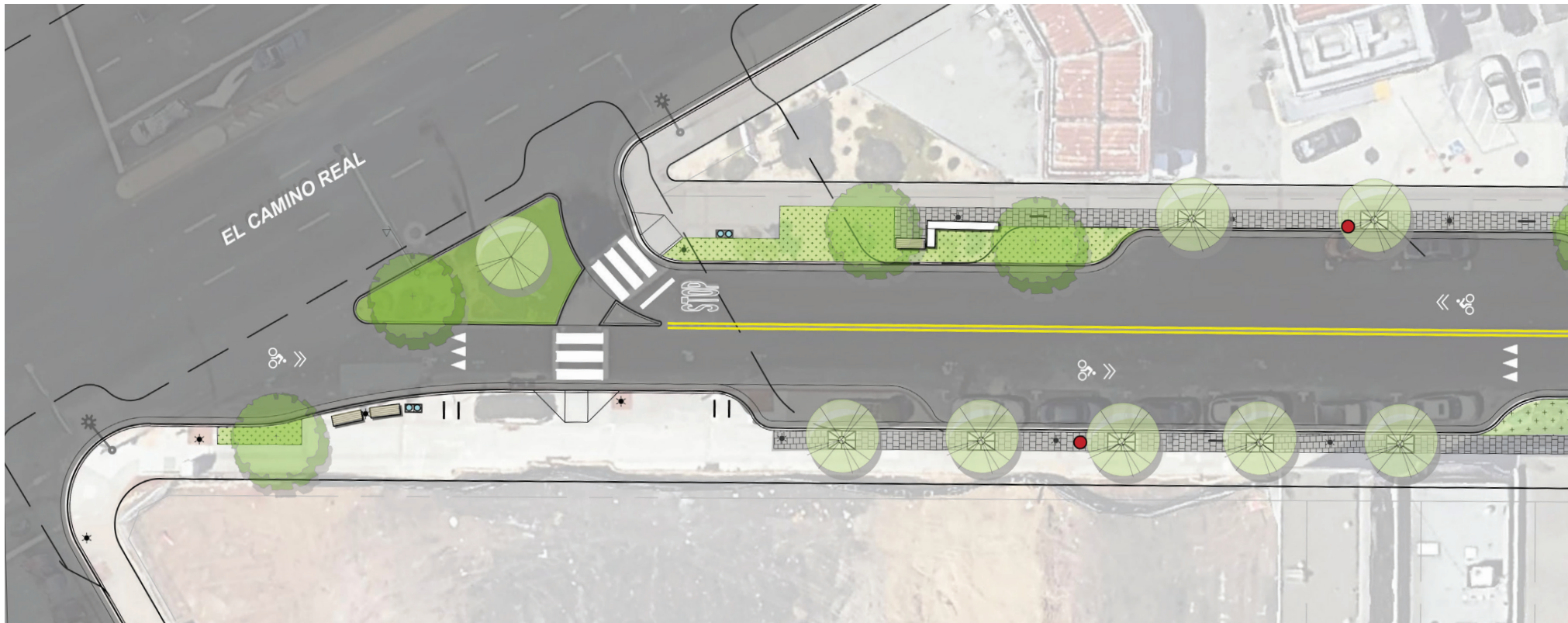


Proposed Street Section B:
Midblock



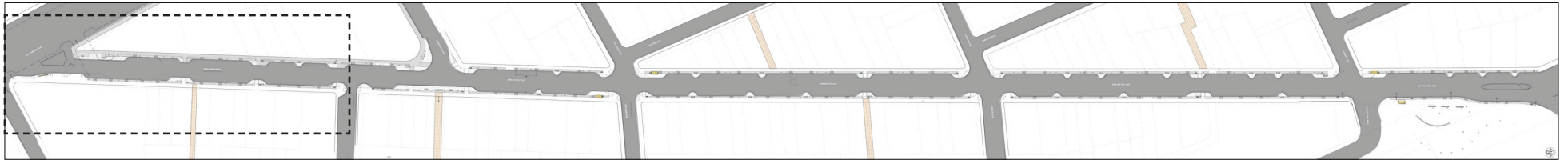
6' (west) - 8' (east) path of travel
and intermittent cafe seating
4' tree and amenity strip

DESIGN OVERVIEW: ILLUSTRATIVE PLAN



LEGEND

- | | | |
|-----------------|---------------------|------------------------|
| * LIGHTPOLE | SEATWALL | ULMUS PARVIFLORA |
| — BIKE RACK | TRASH AND RECYCLING | KOELREUTERIA BIPINNATA |
| ● PARKING METER | BUS SHELTER | PLANTING |
| BENCH | PASEO | |



KEY PLAN



MATCHLINE, SEE PAGES 30-31



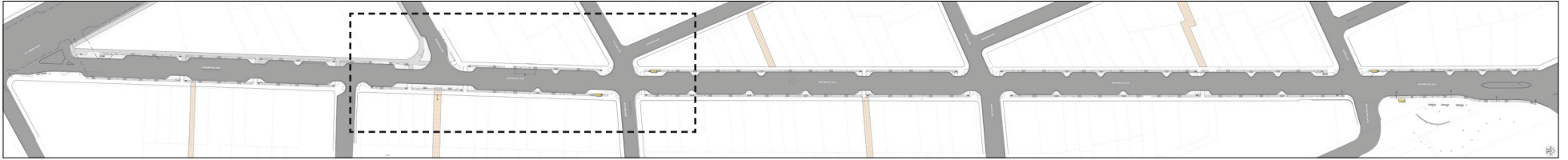
DESIGN OVERVIEW: ILLUSTRATIVE PLAN



MATCHLINE, SEE PAGES 28-29

LEGEND

- | | | |
|---------------|---------------------|------------------------|
| * LIGHTPOLE | SEATWALL | ULMUS PARVIFLORA |
| BIKE RACK | TRASH AND RECYCLING | KOELREUTERIA BIPINNATA |
| PARKING METER | BUS SHELTER | PLANTING |
| BENCH | PASEO | |



KEY PLAN



MATCHLINE, SEE PAGES 32-33



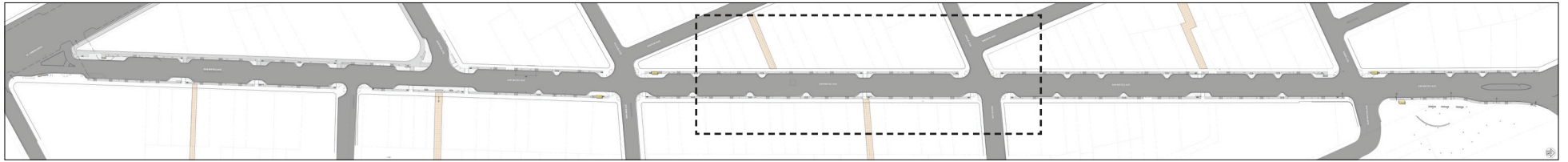
DESIGN OVERVIEW: ILLUSTRATIVE PLAN

MATCHLINE, SEE PAGES 30-31



LEGEND

- | | | |
|-----------------|---------------------|------------------------|
| * LIGHTPOLE | SEATWALL | ULMUS PARVIFLORA |
| — BIKE RACK | TRASH AND RECYCLING | KOELREUTERIA BIPINNATA |
| ● PARKING METER | BUS SHELTER | PLANTING |
| ▭ BENCH | PASEO | |



KEY PLAN



MATCHLINE, SEE PAGES 34-35



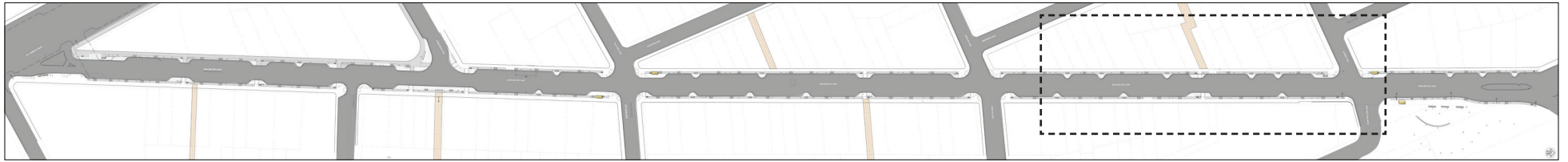
DESIGN OVERVIEW: ILLUSTRATIVE PLAN



MATCHLINE, SEE PAGES 32-33

LEGEND

- | | | |
|-----------------|---------------------|------------------------|
| * LIGHTPOLE | SEATWALL | ULMUS PARVIFLORA |
| — BIKE RACK | TRASH AND RECYCLING | KOELREUTERIA BIPINNATA |
| ● PARKING METER | BUS SHELTER | PLANTING |
| BENCH | PASEO | |



KEY PLAN

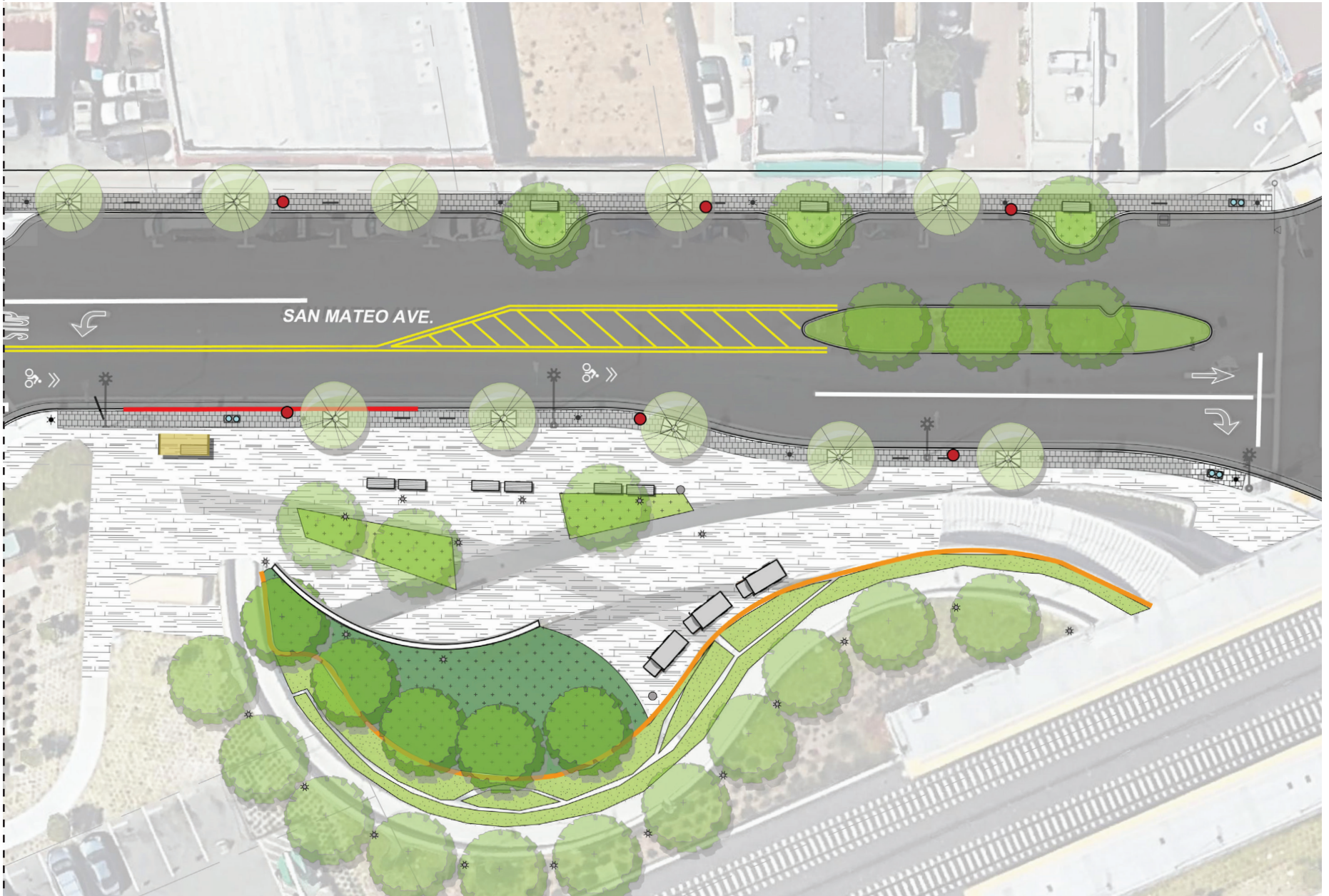


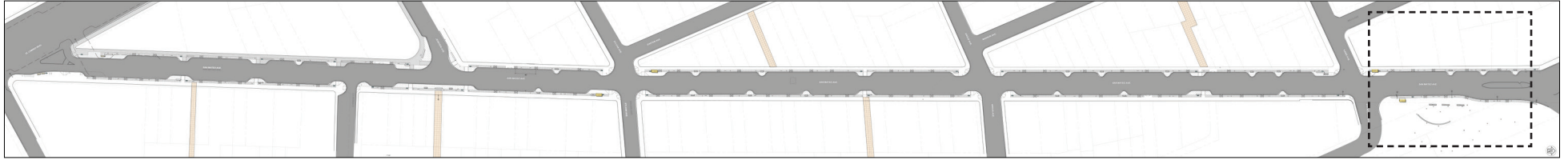
MATCHLINE, SEE PAGES 36-37



DESIGN OVERVIEW: ILLUSTRATIVE PLAN

MATCHLINE, SEE PAGES 34-35





KEY PLAN

LEGEND

- * LIGHTPOLE
- BIKE RACK
- PARKING METER
- ▭ BENCH

- ▭ SEATWALL
- ⊙ TRASH AND RECYCLING
- ▭ BUS SHELTER
- ▭ PASEO

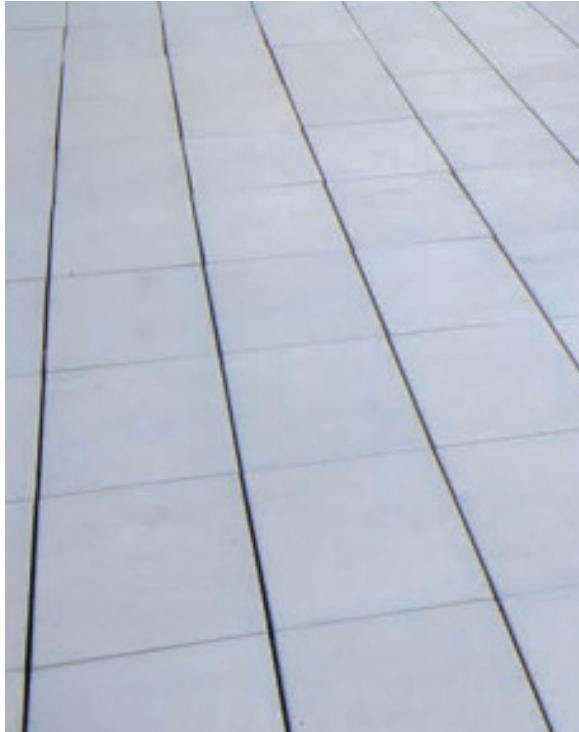
- ⊙ ULMUS PARVIFLORA
- ⊙ KOELREUTERIA BIPINNATA
- ▭ PLANTING

B. BASELINE FURNISHINGS AND HARDSCAPE

Baseline palette

The baseline furnishings and hardscape palette creates the “simple and elegant” backdrop for both the existing character of the street and the proposed opportunities for unique and artistic expression. Key features of the proposed palette are described below.

Because the final selection of materials and furnishings will occur during future design and construction documentation phases and are dependent on the project budget, this plan provides some alternatives that express the design intent. The exception to this is the trash and recycling bins, which have been vetted with City staff and Recology (the waste-management company providing trash and recycling collection services) for their suitability. Trash receptacle replacement may be a near-term project that is completed prior to the overall streetscape implementation.



Cast-in-place Concrete



Unit Paving

Paving

- Simple cast-in-place concrete pavement with a broom finish and a rectangular grid of scorelines in the walkway zone. Though cast-in-place concrete is a common material, the pavement should be finished and scored with attention to craftsmanship and detail. The finish and color should be consistent throughout the site, and the scorelines should be deep, straight and crisp.
- Permeable unit pavers of a consistent gray color aligned with the tree-planting zone, in a running-bond pattern. The unit pavers should be rectangular or square, with a crisp edge.



Lightpole: (top) Lunaria Light, Sternberg Lighting; (bottom) FGP Area Light,
 Bench: (top) Parc Vue bench, Landscape Forms (bottom) Lily bench, Victor Stanley. Bike Rack: (top) Ring Bike Rack, Landscape Forms, (bottom) U Bike Rack, Conceptual Site Furnishings.
 Tree Grate: (top) Rain Tree grate, Iron Age Designs, (bottom) Cascade Tree Grate, Urban Accessories.
 Trash and Recycling: Universal Litter and Recycling Receptacle, Forms + Surfaces.



Lightpole

Bench

Bike Rack

Tree Grate

Trash and Recycling Receptacles

Furnishings and Light Fixtures

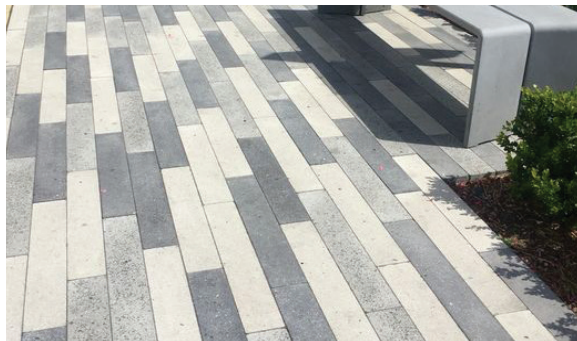
- Modern/contemporary furnishings and light fixtures with a motif of curving forms expressive of movement and flight.
- Stainless-steel or silver-colored metal (e.g., aluminum or powder coated steel) is the primary material for the furnishings and light fixtures.

C. OPPORTUNITIES FOR UNIQUE AND ARTISTIC EXPRESSION

Within the simple and elegant framework, the plan proposes select locations and features for artistic expression. Because the existing streetscape environment is so varied and visually complex, it is important that the locations and quantities of artistic elements don't conflict with their context. An overall theme for these elements should be adhered to, such as one artistic paving pattern applied to all of the paseos and potentially at Posy Park and Centennial Park as well. Similarly, one artistic or custom seatwall design or bench should be implemented throughout the corridor, and one unique crosswalk design. If the crosswalks are constructed of unit pavers, the pattern and material could be the same as in the paseos to further unify these artistic elements throughout the streetscape.

The images included in this section are examples of similar elements that illustrate some of the possibilities; they are not meant to represent the final design concept for San Mateo Avenue. Refinement of the overall design would occur during the Design Development process, during which stage the materials and patterns of all of these elements would be selected.

Through the community-engagement process, the following opportunities for unique and artistic expression were identified:



Paving

Paving at Paseos, Parks and Plazas

Paving at the paseos (or alleyways), parks and plazas. This would ideally be a type of unit paving similar to the "baseline" permeable pavers, but in different colors and/or sizes, arranged in a custom-design pattern. Alternatively, cast in place concrete with special colors, finishes and scoring patterns could be utilized.



Artistic Crosswalks

Artistic Crosswalks

Artistic crosswalk striping or crosswalk materials. Crosswalks can be simply painted in creative ways (given they conform to the functional safety requirements of standard crosswalks), or they can be constructed out of unit paving or cast-in-place concrete.



Custom Seatwall and Lighted Bench

Seatwalls and planter walls

Custom cast-in-place concrete or custom precast concrete seatwalls and planter walls could be located in certain locations throughout the streetscape, as indicated on the overall plan. Alternatively, there is a range of prefabricated precast seatwall products that would fit with the overall design theme.

Custom, artistic benches could be located in the parks, plazas and paseos to emphasize the uniqueness of those spaces. Alternatively,



Seasonal Lighting

budget permitting, a custom bench could be designed and fabricated for use along the entire corridor.

Custom Lighting

Custom designed lighting features or uncommon lighting products could be incorporated throughout the design. Opportunities include:

- Lighting incorporated into benches and seatwalls. (These should be durable LED fixtures, recessed such that they are difficult to tamper with.)



Paseo Lighting

- Lighting at the paseos
- Seasonal lighting
- Gateway elements (see below)
- All custom lighting elements should be designed and engineered to withstand wind-loads. Further study would be required to determine the suitability of cable-suspended lighting fixtures, for example.

D. GREENING AND STORMWATER MANAGEMENT

Greening

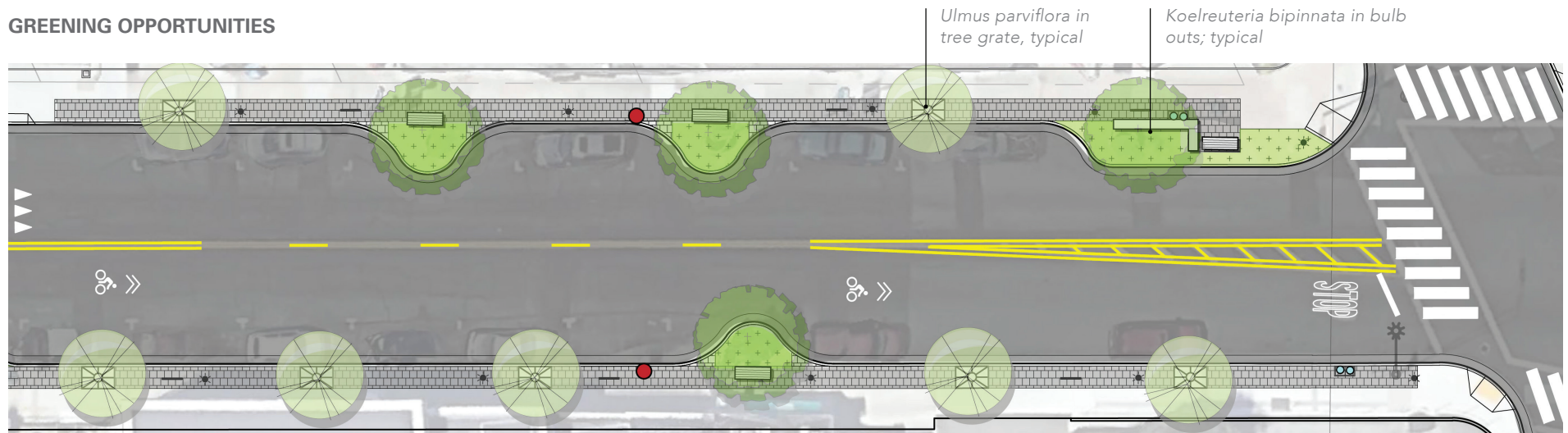
One of the principle recommendations of this plan, as well as the Transit Corridors Plan, is to plant regularly spaced street trees in at-grade tree wells. This plan recommends trees in tree wells with tree grates at approximately 40-foot on-center spacing. (Final tree spacing will need to be coordinated with underground utilities and other potential conflicts.) Trees should also be planted in the existing mid-block and corner bulb-outs (where sight-lines would not be blocked), which provide the opportunity to remove the pavement from the bulb-outs to provide a larger soil volume. Currently, there are 45 trees in pots, and 11 trees planted in-grade. This plan recommends 118 trees planted in-grade, and 7739 square feet of planting area.

Soils and Pavement - Planting Construction Considerations

In order to prevent sidewalk damage as well as promote tree health, it is critical that the tree wells are large. The majority of sidewalk damage is due to the pavement being placed too close to the root-flare of the tree. This plan recommends 4' x 6' tree wells; this should be considered a minimum. This plan recommends additional improvements to the soil under the sidewalks, such as structural soil or suspended pavement, which also help to prevent sidewalk damage and are beneficial to the trees.

Suspended pavement is the best option, and it can be cost-competitive with structural soil if considered on the basis of planting-soil volume (as opposed to total volume).

GREENING OPPORTUNITIES





Ulmus parvifolia



Koelreuteria bipinnata



Mid-block bulb-out opportunity

Trees

Two tree species are recommended. The trees planted in new tree wells along the corridor (i.e., not in the bulb-outs) should be *Ulmus parvifolia* (Chinese elm), while the summer-flowering *Koelreuteria bipinnata* (Chinese flame tree) is indicated in the existing bulb-outs.

Ulmus parvifolia is a large-scale shade tree with a relatively open branching habit that will provide ample dappled shade and will be an appropriate scale for the street. At maturity, the branches will be high enough to provide visibility to shop signs for pedestrians and from vehicles. *Koelreuteria bipinnata* will be a contrasting, smaller ornamental tree which flowers in the

summer. Both species are climate-appropriate for San Bruno; the *Ulmus parvifolia* was recently planted at The Aperture development, at the south end of the project area.

Understory

All planting and irrigation will be required to meet MWELo water-use requirements. Shrubs and groundcovers should be drought-tolerant and low-water-use.



Corner bulb-out opportunity

Stormwater Management

SMCWPPP C.3 stormwater treatment requirements provide an exclusion for projects that are limited to “sidewalk replacement.” If this streetscape project is determined to fall under that category, no stormwater management features would be required.

If additional elements of the project are implemented such that the project is determined not to fall under the “sidewalk replacement category,” then the use of a permeable pavement strip in the sidewalk, sized to act as self-retaining areas, combined with the proposed new trees planted along the corridor, meets the SMCWPPP C.3 stormwater-treatment requirements to manage runoff from the replaced sidewalks within the project area (assuming that the roadway pavement itself is not replaced).

Budget permitting, this plan recommends additional stormwater-management features including:

- Suspended pavement under the parking lane and sidewalks to treat stormwater and provide additional planting soil to support healthy, large and long-lived trees. (Where necessary, underground utilities can be routed through the suspended pavement structure.)
- Permeable pavement in the parking lane
- Creating bioretention planting areas or flow-through planters in existing bulb-outs where existing drain inlets and/or grades allow. There is not enough potential planting area within existing bulb-outs to treat stormwater

for the entire project area watershed to comply with C.3 requirements, however this recommendation is based on the principal of providing as much stormwater treatment as possible. (In order to treat the stormwater from the entire project area to meet C.3 requirements, approximately 3,300 additional square feet of bioretention area would be required – however this would require the removal of on-street parking spaces, which the plan does not recommend.)

- Creating a bioretention planting area at the southwest corner of Jenevein Ave. and San Mateo Ave. (This would treat stormwater flowing from Jenevein Ave., not San Mateo Ave.)

Potential Constraints to greening and stormwater management

Three underground utilities are potential constraints to planting trees and constructing sub-grade stormwater-management features such as bioretention planters and suspended pavement. They are:

- A 44-inch steel storm-drain pipe that runs along the east side of the street from El Camino Real to mid-block between Sylvan and Angus Avenues. The top of this pipe is approximately 3-4 feet below grade. This plan recommends planting trees above this pipe, as the steel material is not likely to leak or need replacement in the foreseeable future. The depth to the top of the pipe is more than adequate to allow for the trees’ roots.
- A 20-foot-wide concrete box culvert that runs from midblock between Syvan and

Angus Avenues to the southeast corner of Angus Ave. with its eastern edge under the sidewalk. The exact depth from finish grade to the top of the box culvert is unknown, but it is likely to be between 1 foot and 3 feet deep. If it is equal to or greater than 2 feet below grade, this plan recommends planting trees on top of the structure. If it is less than 2 feet below grade, it may be possible to provide planting in raised planters.

- An 8-inch water line on the east side of the street from Sylvan Ave. to Angus Ave. (The water line continues north, past Angus, but at this point, it jogs away from proposed amenity and planting areas.) This plan recommends moving this water line to the back-of-sidewalk in order to make more room for tree planting in this area.



Green infrastructure

Jenevein Greening Opportunity

At-grade planting opportunities are shown throughout the plan. These include primarily the existing mid-block and corner bulb-outs. In addition to these, the intersection of Jenevein Ave. and San Mateo Ave. represents a significant opportunity to increase planting by shortening the corner radii as shown. The proposed layout accommodates a 40-ft bus to make a right turn from southbound San Mateo Ave. onto Jenevein Ave. completely within its lane, and a 30-ft box truck to make a right turn from Jenevein Ave. onto San Mateo Ave. while encroaching slightly into the opposite lane. (These 30-foot box truck types use this street only occasionally.) It also accommodates street sweepers. At the southwest corner (in front of the Bank of America), there is an existing drain inlet such that the planting area is an opportunity for a significant stormwater-management feature (see pages 50-51).

JENEVEIN AVENUE RAIN GARDEN OPPORTUNITY



E. MOBILITY AND PARKING

While this plan essentially retains the existing mobility design for all transportation modes, the following modifications are recommended:

Curbside Management and Business Loading

- Convert the center stripe from “no passing” (double solid yellow line) to “passing allowed” (single dashed yellow line) where possible.
- Provide a white-curb loading zone in front of La Petite Baleen Swim School. Community input indicated that this business requires pick-up and drop-off of children, which is currently done unsafely from double-parked vehicles. This would require the removal of two on-street parking spaces.
- Provide three loading zones (yellow curbs) in widely-spaced locations along the street. This would require the removal of six on-street parking spaces. City Council can adjust the loading-only schedule from the times specified in the Municipal Code to provide customer parking during peak hours.

Public Transit

- Improve the existing bus stops along the project area by extending three bulb-outs 20 feet (two at Sylvan Ave and one at Kains Ave). This would bring these bus stops into compliance with SamTrans and accessibility requirements, and it would remove three parking spaces from the project area. (Bulb-out extensions shall be designed to accommodate street sweepers.) Additionally, SamTrans requested that bus shelters be installed at all four bus stops.

Pedestrian Safety and Accessibility

- Improve and maintain sightlines at crosswalks for pedestrian safety. For this reason, trees are not proposed in the bulb-outs on the approach sides of the crosswalks.
- Where sidewalk cross-slopes are in excess of 2% from the property line to the back-of-curb, provide a 2% maximum cross-slope in the path of travel and a slope greater than 2% in the furnishings zone.
- At a minimum, reconstruct non-compliant curb ramps as necessary to provide code-compliant curb-ramps. (All of the curb-ramps in the project area may be replaced to match the adjacent new paving.)
- Install high-visibility continental crosswalk striping and yield striping (“sharks teeth”). If artistic or unique crosswalk markings or paving are installed, they should comply with crosswalk striping safety standards.

Bicycle Facilities

- Paint “sharrows” in the travel lanes along the project area.
- Provide bike racks along the sidewalk at approximately 100-foot spacing. Regularly spacing bike racks along the sidewalk, rather than grouping them in limited locations allows cyclists to lock their bikes closer to their destinations.

Parking

- Provide wayfinding signage, including signage directing drivers to the off-street surface lots.
- Improve the paseos that lead to off-street surface parking lots, especially with additional lighting and wayfinding signage.
- Install parking meters, as recommended by the Downtown Parking Management Plan.
- Provide on-street accessible parking as recommended by the Accessibility Transition Plan, which is being developed concurrently with the San Mateo Avenue Streetscape Plan. There are two potential options for on-street accessible parking. If on-street accessible parking is to be provided on San Mateo Ave., then additional curb-ramps will be required, either at the inside of the bulb-outs closest to the parked car, or mid-block (see example photographs to the right). This would require the deletion of recommended planting areas, trees, seatwalls, benches, and other amenities where they conflict with the curb-ramp. A potential alternative would be to provide accessible parking stalls on the side streets, at the parking stalls closest to San Mateo Ave. In that case, the existing corner curb-ramps could serve those parking stalls, since the bulb-outs do not extend into the side streets. There are five parking stalls on side streets that might serve this purpose. Further study is required to determine if

ACCESSIBLE CURB-RAMP DESIGNS FOR ON-STREET ACCESSIBLE PARKING

these locations could meet the need for accessible parking stalls given accessibility standards and guidelines.

- Maintain the existing on-street parking, with the exception of the removal of eleven spaces total: one at each of the three bus stops, as required per SamTrans and accessibility code; two at La Petite Baleen Swim School, to provide a safe loading zone for children using that facility; and two at each of the three yellow-curb loading zones.

Striping Plan

The Proposed Striping Plan on the following pages illustrates the street and curb markings described above, with the exception of the yellow loading zones which will be determined through a separate engagement process.



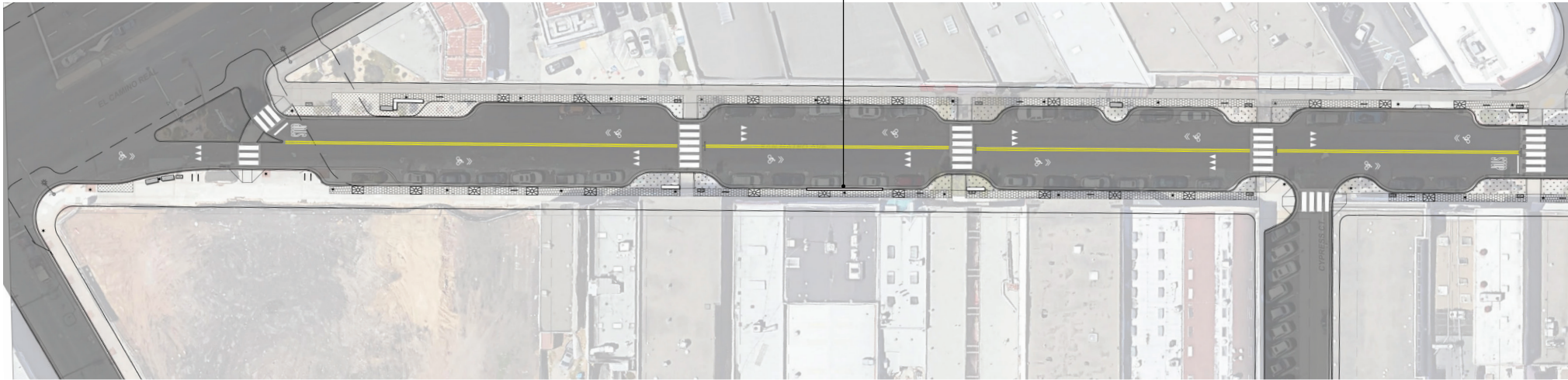
Curb ramp at bulb-out



Mid-block curb ramp

PROPOSED STRIPING PLAN

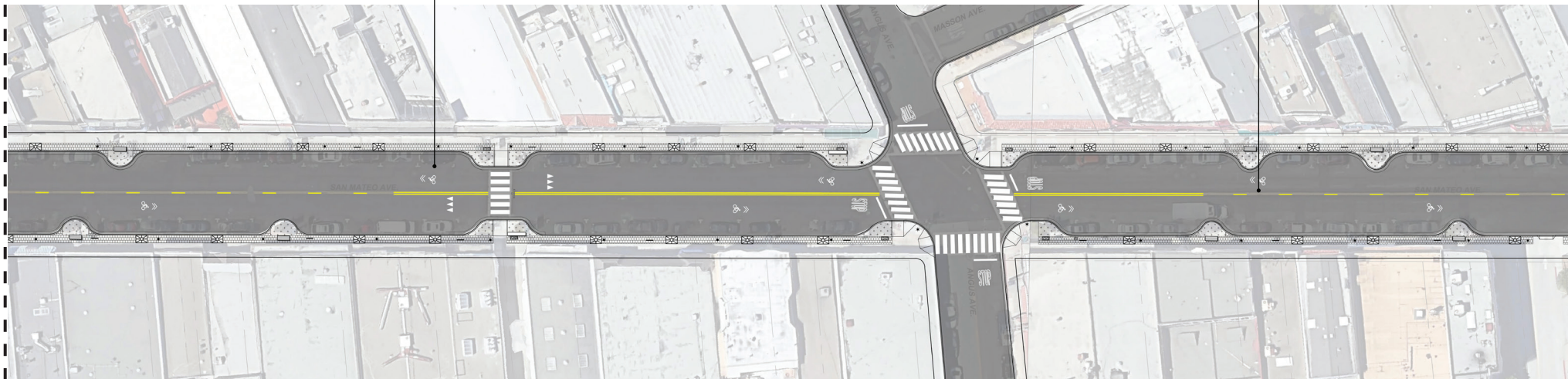
Proposed white painted 'passenger loading only' curb zone



Sharrows, typ.

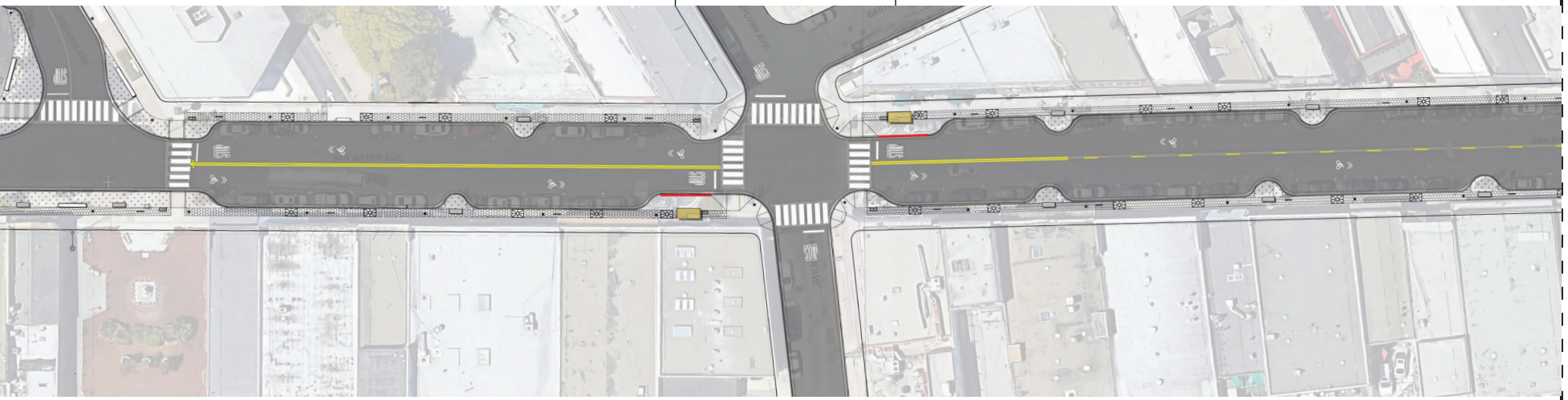
'Passing allowed' dashed yellow striping, typ.

MATCHLINE, SEE ABOVE



Proposed 'red' no parking, bus stop zone at existing bus stop

Proposed 'red' no parking, bus stop zone at existing bus stop



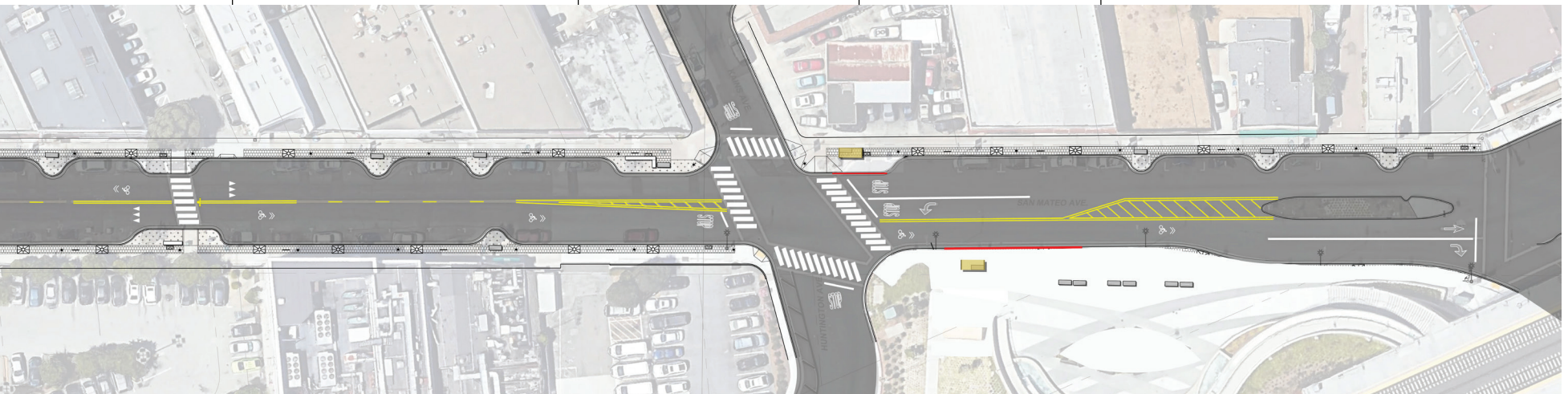
MATCHLINE, SEE BELOW

Yield striping, typ.

Existing bus stop relocated to NW corner of intersection

Proposed 'red' no parking bus stop zone, at bus stop (relocated from SW parking area)

Proposed 'red' no parking bus stop zone, at existing bus stop



F. LIGHTING

A broad consensus emerged through the community-engagement process that improved lighting should be a priority of the design. A photometric analysis confirms that the existing streetlights do not provide adequate light levels or uniformity. Improved lighting will help activate the street at night, when many community members feel the street is under-utilized. The proposed lighting design serves two functions: First, pedestrian-level pole-mounted light fixtures and lighting in the paseos will provide safety and overall illumination. Second, accent lighting will provide visual interest at night, enhancing the unique character and attractiveness of the street.

Pole-Mounted Streetlights

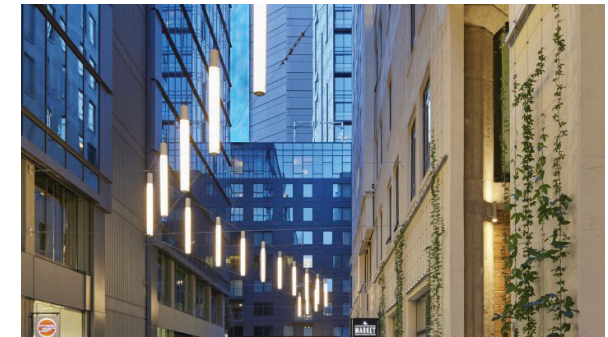
All of the existing streetlights along the corridor are recommended to be removed, and new pole-mounted streetlights are proposed at approximately 60-feet on-center. The streetlights should be approximately 16 feet high. This mounting height provides a pedestrian scale, while the spacing provides adequate light for vehicular as well as pedestrian safety. Taller fixtures at wider spacing tend to create a sense of a vehicular-scaled rather than pedestrian-scaled streetscape and are therefore not recommended. The light fixtures and poles should be metallic silver in color, and should be contemporary in style, with forms that convey the motif of movement. The IESNA standard of 1.2 average foot-candles and a uniformity

LIGHTING TREATMENT CONCEPTS



Pole-mounted streetlights

ratio within 2.0 of the 4.0 IESNA standard is recommended. (The wattage and optics of the light fixtures would be determined during the Design Development stage of the design process, and final fixture layout would depend of photometric analysis.)



Paseo lighting

Paseo Lighting

At a minimum, the paseos should be lighted to provide for safety and wayfinding to and from the off-street parking lots. As discussed earlier (see “Opportunities for Unique and Artistic Expression”), the lighting scheme for the paseos could also be unique and artistic.



Accent lighting

Seasonal lighting

Gateways lighting

Accent Lighting

To provide an additional lighting element along the streetscape, the plan recommends incorporating lighting into the benches and seatwalls throughout the corridor. This accent lighting should be consistent and visible as a series of elements that “flows” along the sidewalk. These lighting elements should be durable, long-lasting LED fixtures; and they should be recessed into the seatwalls and benches to prevent tampering.

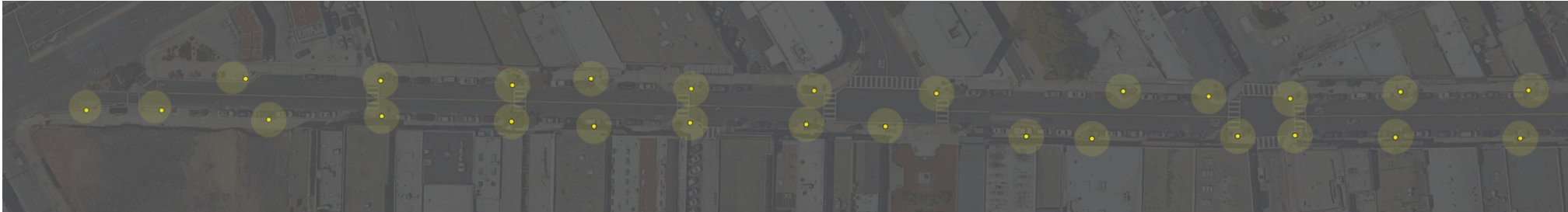
Seasonal Lighting

Seasonal lighting should be accommodated with electrical outlets and planting of deciduous trees with an open branching structure (*Ulmus parvifolia* is recommended, see “Greening”). Seasonal lighting during the winter can encourage shopping and brighten the evenings during the short days.

Gateways Lighting

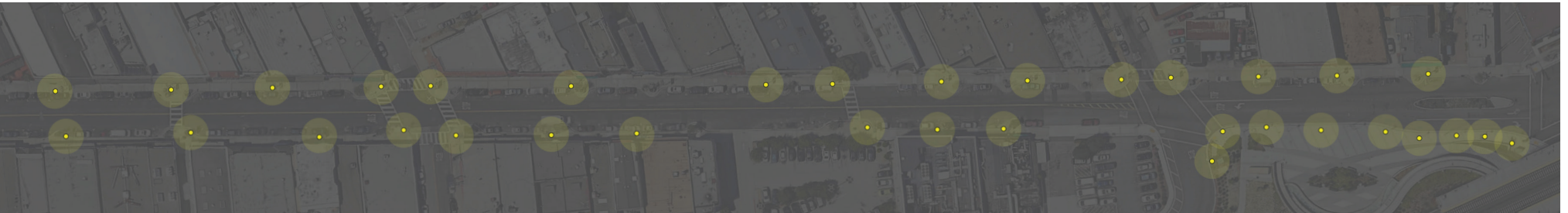
The gateways should be considered part of the overall lighting scheme, with lighted elements an integral part of their design. See “Wayfinding and Gateways” below.

EXISTING LIGHTING LAYOUT



PROPOSED LIGHTING LAYOUT





Accent lighting, typ.

Pole mounted streetlights, typ.

Paseo lighting, typ.

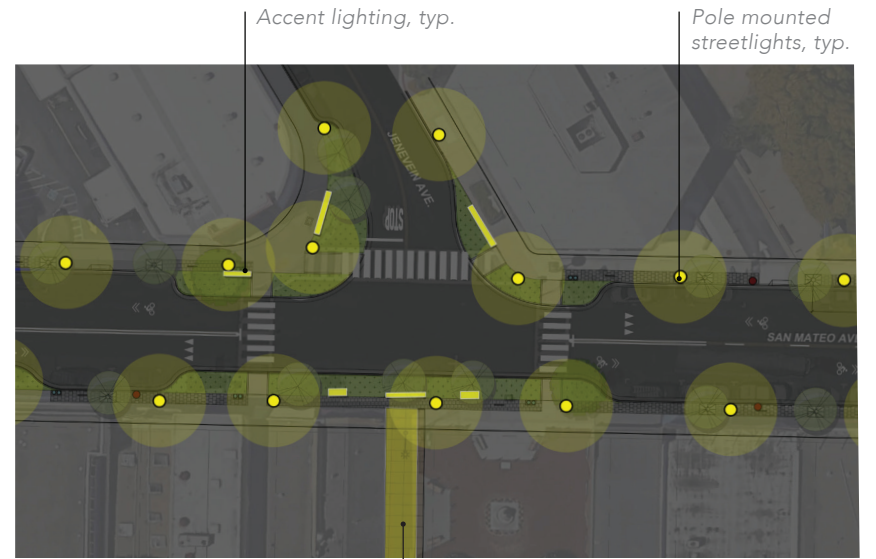
Gateway lighting, typ.



ENLARGEMENT PLANS



Existing lighting layout



Accent lighting, typ.

Pole mounted streetlights, typ.

Proposed lighting layout

Paseo lighting, typ.

G. SPECIAL PLACES

This plan recommends improvements to the following “special places”: Posy Park, Centennial Park and the paseos (alleyways) that connect the sidewalk to surface parking lots behind the buildings. Of these, the paseo improvements should be considered a priority, as they are integral to the parking demand-management strategy by increasing the visibility and safety associated with using the off-street parking lots. Depending on funding and the broader citywide priorities for park improvements, Posy Park and Centennial Park improvements may be incorporated into the streetscape construction

project, or they may be considered separate projects.

Paseos: Artistic Expression

In addition to their essential role in providing access to the off-street parking lots, the paseos are an opportunity for unique artistic expression along the corridor. During the community-engagement process, excitement emerged for the possibility that the paseos could be sites for artists’ installations. Artists could be selected through a competition, potentially with a preference for local artists. Because the paseos

can be a refuge from the wind, seating should be incorporated. The paseos are also opportunities for special paving, potentially designed as part of the art installations. So that they serve as connections to the off-street parking lots, lighting and signage are essential components of the paseo improvements.

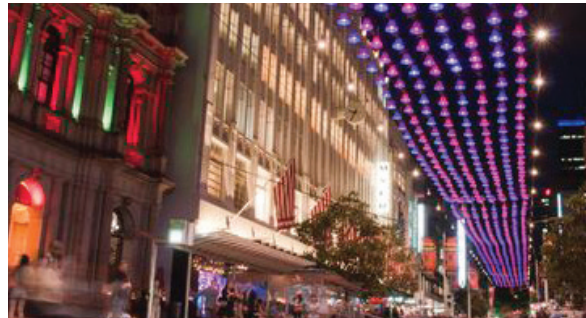
PASEO ACTIVATION OPPORTUNITY



PASEO CONNECTIONS TO EXISTING OFF-SITE PARKING LOTS



PASEO ACTIVATION CONCEPTS



Artistic Lighting



Shade Structure and Overhead Treatments



Murals and Distinctive Paving

Posy Park: A Green Gateway

Posy Park is part of the gateway experience for visitors approaching the corridor from the north. Currently, the park does not serve either as a visual gateway or as a well-functioning open space. Constructed as part of the Caltrain station and grade separation, the park is dominated by large, gray retaining walls. There is little shade. The central water feature has been turned off because it never served its intended purpose. The seating area is hidden from view, doesn't feel safe. The recommendations of this plan are intended to create a visual gateway feature and a more usable space, primarily by increasing the planting area and adding large trees. The sense

of safety would be increased by moving the usable seating area closer to the street. And the retaining walls that dominate the space would be re-painted with a beautiful color or pattern, and partially covered with plants. Amenities and programming would activate the space, as will increased foot-traffic as people walk from new residential developments to and from the Caltrain station.

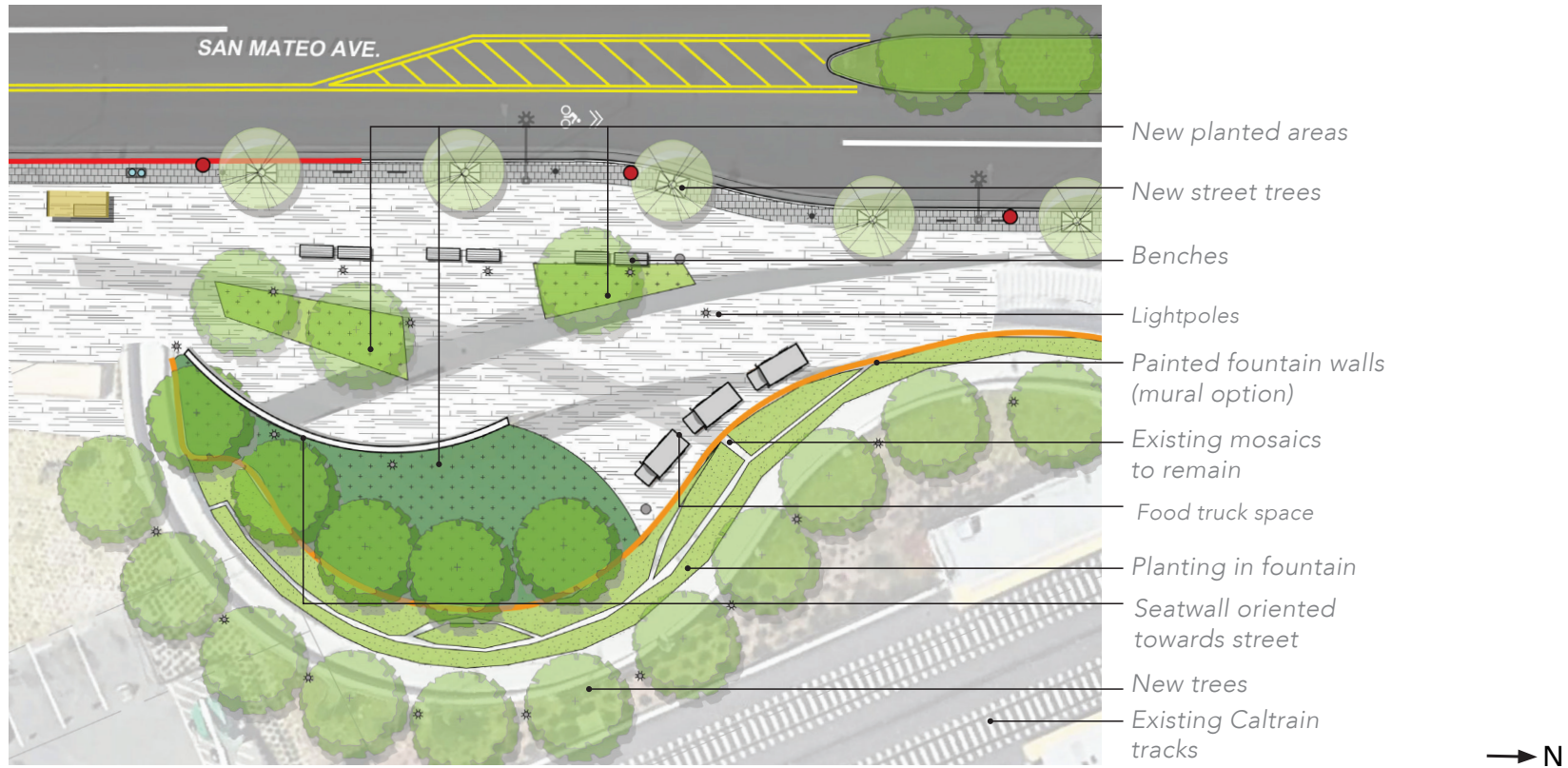
The key recommendations for Posy Park are:

- Reduce the amount of paving to provide more planting area.
- Plant very large shade trees.
- Convert the fountain to planters.
- Paint the existing walls with a bright color or artistic pattern.
- Move the seating zone closer to the street.
- Provide an area for food trucks, carts or kiosks.
- Maintain sightlines by keeping groundcovers and shrubs low and tree canopies high and orient furnishings so that they don't block views.
- Movable tables and chairs should be studied as an option to activate the space.
- A lawn area should be studied as a potential amenity for gathering, picnicking, relaxing, etc.

CONCEPT VIEW



CONCEPT PLAN



PROGRAM CONCEPTS



Food Trucks / Vendors



Cafe Tables and Seating



Shaded Seating Areas



Lawn

Centennial Park: An Active Event Space

Centennial Park should be made accessible for public use. Currently, the majority of the park is off-limits to people, barricaded with ropes, due to sub-surface rubble from the demolition of a building that once stood on the site. Community input made it clear that this is widely seen as a missed opportunity to provide an active space that would support many uses. One notable outcome of the community-engagement

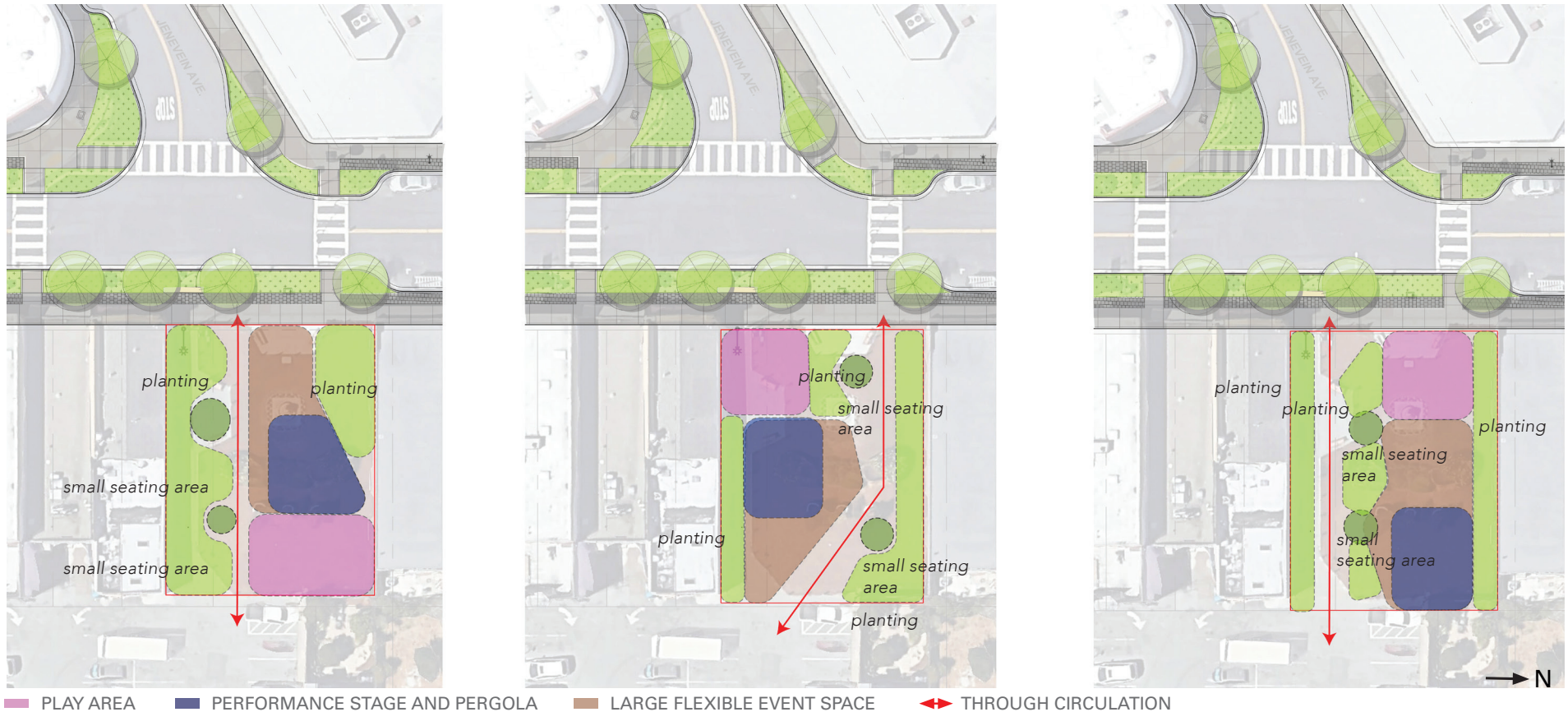
process was the need for children-oriented spaces. A play area would serve the children and families that come to San Mateo Ave. to patronize the cluster of child-oriented businesses including the swim school and tot play center. Also, Centennial Park provides an opportunity to create a performance space that would support arts programming along San Mateo Ave. and attract people during festivals, a farmers market,

or for regular small concerts at lunchtime or on weekends

Program elements that should be considered include:

- Large, flexible event space
- Performance area (a stage is not recommended, as it would reduce the flexibility of programming)

CENTENNIAL PLAZA ACTIVATION IDEAS



- Small seating areas
- Play area with climbing structure

The program diagrams on the previous page indicate the amounts of space recommended for each program zone, illustrate three possible layouts with different relationships between the zones and the street, and show different circulation patterns through the park between the street and the off-street parking.

CENTENNIAL PLAZA ACTIVATION IDEAS



Nightlife/food vendors



Performance stage



Event space



Pop-up markets



Pop-up amenities



Children's play

H. WAYFINDING AND GATEWAYS

Design Intent

The design intent for the wayfinding signage is to prioritize simplicity and legibility. The color scheme is intentionally minimal: blue backgrounds with white and black lettering and symbols. The signs should be rectangular in shape. Graphic embellishments should be minimized, if not avoided altogether. The typeface should be a modern, sans-serif style. The precedent images to the right show a range of styles from the least embellished, Modernist design aesthetic, to a more graphically complex design. (See "Signage Design Considerations" below for further discussion.)

Branding

A branding exercise will be a necessary stage of the wayfinding and gateways design process. In the past, San Mateo Avenue was branded "The Avenue" on gateway signage and smaller street signs. These gateway signs are no longer in place, however there are still "The Avenue" street signs in place along the corridor. During the community-engagement process, this branding was questioned, as several participants in the process felt that most people think of Burlingame when they hear the words "The Avenue." An alternative brand could be simply "Downtown San Bruno." Other ideas that were discussed include "The Heart of San Bruno."

WAYFINDING PRECEDENTS



Top Left: Tacoma Art Museum, Washington
 Top Right: Aotea Square, Auckland New Zealand
 Bottom Left: Des Moines, Iowa

SIGNAGE AND GATEWAY LOCATIONS



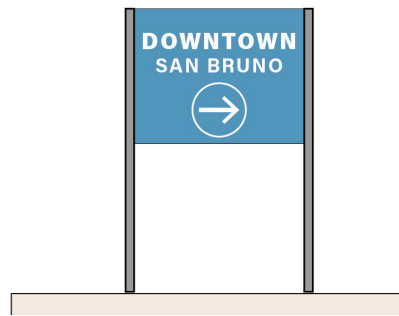
Vehicular Corridor Directional Signage

Vehicular corridor directional signs are intended to direct drivers entering downtown San Bruno from surrounding freeway exits and arterial streets to the San Mateo Avenue commercial corridor. These signs should be located off San Mateo Avenue at key locations. These signs should be positioned and oriented to be read from a vehicle, such as in the medians on San Bruno Avenue.

Parking Wayfinding Signage

To direct vehicular traffic to the off-street parking lots, these signs should be located along the San Mateo Avenue project area at key intersections as indicated. Additionally, "parking" signs should be located at the paseos to highlight these pedestrian paths from the sidewalks to the parking lots.

VEHICULAR WAYFINDING



MEDIAN WAYFINDING



PARKING WAYFINDING

LIGHTPOLE MOUNTED WAYFINDING

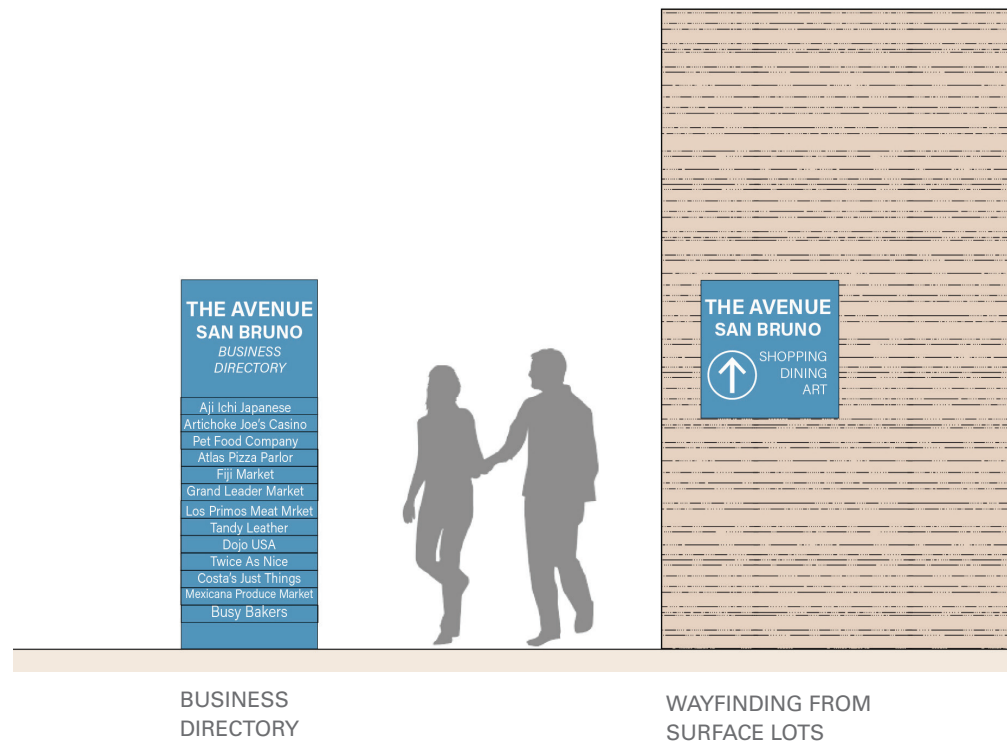
Business Directories

Business directories would indicate which businesses can be found on each block of the corridor. Business directories would be especially valuable at the paseos, as people walking from the parking lots behind the buildings might become disoriented with respect to their destinations. A funding and maintenance/management strategy would need to be developed for these signs. The funding for business directories of this type is typically provided by the businesses that advertise on these signs or by a business improvement district. It would need to be determined what entity (public or private) has responsibility for maintaining these signs, how many spaces for business names would be appropriate for each block, and how to adjust the quantity of businesses that are represented on each sign if there is more or less demand.

Pedestrian Wayfinding Signage

To direct people who have parked their cars in the off-street parking lots to the paseos and on to San Mateo Avenue, pedestrian wayfinding signage should be installed at the parking-lot ends of the paseos. At a minimum, these signs should be similar to the rest of the directional signage described in this section, however they may be replaced by paseo gateway monuments. (See "Paseo Gateway Monuments" below.)

PEDESTRIAN WAYFINDING





Gateway Monuments

As markers of the north and south entrances of the San Mateo Avenue commercial district, one major gateway monument is proposed for each end of the project area. The recommended locations for these monuments are shown on the overall illustrative plan. Several conceptual design alternatives are included in this plan. The final design will depend on budget, engineering, and fabrication and constructability constraints. Lighting should be integrated into the gateway elements, so that they become a key element of the nighttime environment.



Paseo Gateway Monuments

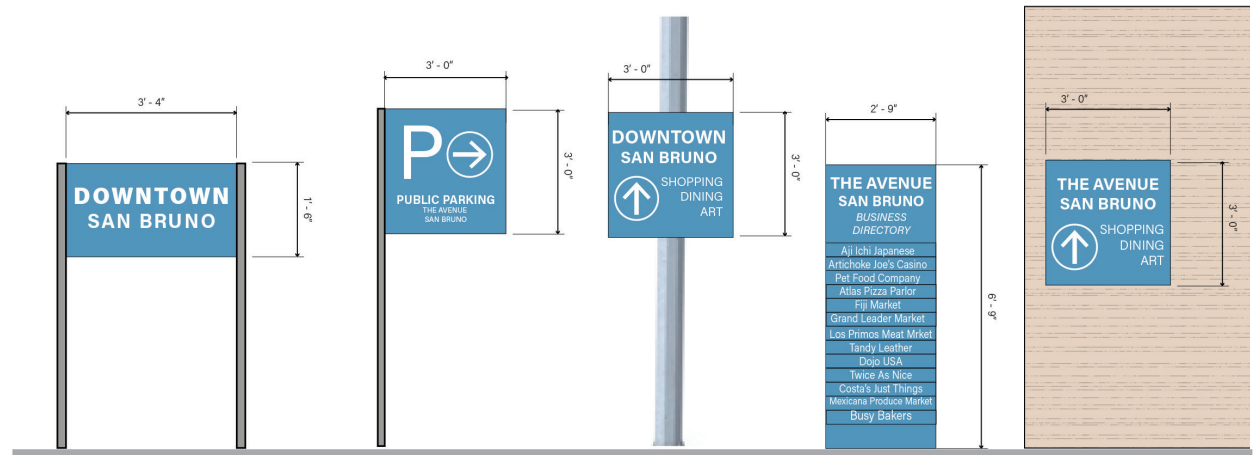
To enhance the wayfinding to the off-street parking lots and the unique place-making character of the paseos, smaller gateway elements are proposed for the entrances to the paseos from the sidewalk. These should be consistent in form and materials with the larger gateway monuments at each end of the corridor, and they should incorporate similar lighting elements. Additional paseo gateway monuments could be placed at the parking-lot ends of the paseos to provide an iconic directional beacon for people who have parked their cars in the parking lots.

Signage Design Considerations

This plan provides specific guidance for the types and locations of wayfinding signs and gateways and general guidance for the aesthetics of the signs and gateways. Further development of the sign designs should be completed with a signage-design consultant. The signage design should consider the full extent of the signage program for downtown San Mateo even if the initial implementation is limited to the San Mateo Avenue project area. Some of the considerations for further signage design development are:

- Legibility and conformance with accessibility requirements: Typeface selection and font size should be determined based on industry standards and requirements.
- Sign sizes and positions: Sizes and positions should be determined based on visibility for drivers and pedestrians as required.
- Aesthetics and design motif: Within the guideline of simple, modernist design, the level of graphic embellishment requires further study. Graphic motifs may reflect the final design for other unique aspects of the streetscape design that will be determined through the design-development process, such as the gateway elements, custom furnishings or art installations.
- Materials: The refinement and craft of the materials and construction techniques will be critical to the successful implementation of a simple, modern and elegant signage program.

SIGNAGE DIMENSIONS



POTENTIAL VARIATIONS



GATEWAY CONCEPT 1



GATEWAY CONCEPT 2



GATEWAY CONCEPT 3



I. ACTIVATION

In addition to the physical design improvements described in this Plan, the community engagement process highlighted the desire to activate the corridor with programming. Ideas that were encouraged during the community and stakeholder workshops included:

Farmers' Market

It was noted that a previous farmers' market was unsuccessful, but there is still interest in creating a better-managed farmers' market. It was noted that the farmers' market could be held in parking lots and plazas without closing the street.

Music Concerts

Music concerts could be programmed in Centennial Park for special events, regular occasions such as weekends, or lunchtimes during the week.

Games

Chess boards could be provided on public tables. A Bocce ball court could be located in Centennial Park or Posy Park. Other games that have become popular in open spaces are corn hole and large connect-four.

Art Events and Competitions

Art fairs could be supported in the plazas. Art competitions could be held to choose temporary art to decorate plazas, the paseos, or participating store fronts, similar to the San Jose Doors program, which selects student art to decorate utility doors.

Street Fairs

Regular street fairs could include a "Sunday Streets" event, which closes off the street to motor vehicles and encourages strolling and bicycling, or "First Fridays," modeled on Oakland's monthly street fair on Telegraph Avenue.

Seasonal Events

Seasonal events could include Halloween trick-or-treating, an Independence Day parade, or a back-to-school shopping promotion event. A scavenger hunt could encourage children and families to explore the street.

Special Dining Events

Dining events could be geared toward parents or senior citizens. Modeled on Oakland's Temescal Business Improvement District's annual Taste of Temescal, San Mateo Avenue's restaurants could organize a "culinary crawl," offering small portions of their food to ticket holders from sidewalk tables.

J. PRIORITIZATION AND COST ESTIMATE

At the time of this writing, a budget has not been identified for the streetscape project. The following ranking and rough-order-of-magnitude (ROM) cost estimates of the proposed improvements are provided to guide budgeting decisions in case funding for all of the recommendations is not available. These costs are for the initial project design and construction; they do not include ongoing maintenance and operations costs. The ranking is based on input during the community-engagement process.

Tier 1

The items listed under Tier 1 should be considered the bare minimum, lowest cost project that would provide the highest value.

Tier 2

The items listed under Tier 2 are the second-highest priority. These items support key goals of the project. Combined, Tiers 1 and 2 represent the “baseline” streetscape project, with each element providing a functional as well as aesthetic value.

Tiers 3 & 4

The items listed under Tiers 3 and 4 are either outside of the strict limits of the streetscape project (Centennial Park, Posy Park), are purely aesthetic in nature (artistic elements), or provide additional environmental benefit (additional stormwater management features).

The costs listed are cumulative. For example the cost to implement tiers 1, 2 & 3 would be the total of the costs listed for each tier. For items that represent add-on features, the cost listed for the add-on feature is the premium, or additional cost, for the additional feature. These are noted “premium.” For example, the cost for the custom benches listed under Tier 4 would be the total of the “basic product” bench listed under Tier 1 plus the additional cost for the “custom bench” under Tier 4. This means that subtraction of redundant items is not necessary to use the table.

PRIORITY RANKING OF PROPOSED IMPROVEMENTS

Tier	Recommended Improvement	ROM Cost
1	Sidewalk replacement, ramps	\$ 1,676,000
	Bus-stop improvements	\$ 77,000
	Street trees (with tree grates)	\$ 240,000
	Street lights (pole-mounted, 60' spacing)	\$ 396,000
	Trash and recycling receptacles	\$ 114,000
	Benches	\$ 36,000
	Seatwalls	\$ 76,000
	Bike racks	\$ 31,000
	Wayfinding signage	\$ 90,000
	Planting and Irrigation	\$ 45,000
	Water Line Relocation	\$ 225,000
	Demolition and earthwork	\$ 629,000
	General conditions (mobilization, etc.)	\$ 394,000
	Subtotal	\$ 4,633,000
	Contingency (35%)	\$ 1,622,000
Design (15% of total)	\$ 816,000	
Total Project Cost	\$ 6,254,000	
2	Permeable paving along sidewalks, additional cost (move to Tier 1 if required)	\$ 338,000
	Curb realignment and planting at Jenevien Ave.	\$ 65,000
	Suspended pavement for street trees	\$ 204,000
	Gateway monuments	\$ 750,000
	Paseo gateway monuments	\$ 100,000
	Roadway striping	\$ 15,000
	Planting and irrigation at existing bulbouts	\$ 111,000
	Paseo improvements/art installation	\$ 400,000
	Subtotal	\$ 1,983,000
	Contingency (35%)	\$ 694,000
	Design (15% of total)	\$ 402,000
Total Tier 2	\$ 3,079,000	
Total Project Cost (Tier 1 + Tier 2)	\$ 9,333,000	
3	Centennial Park improvements	\$ 524,000
	Artistic expression: Lighting at benches and seatwalls	\$ 41,000
	Additional stormwater management: suspended pavement and bioretention	\$ 1,383,000
	Soft Costs (design @ 15%)	\$ 292,000
	Subtotal	\$ 2,240,000
	Contingency (35%)	\$ 784,000
	Design (15% of total)	\$ 454,000
Total Tier 3	\$ 3,478,000	
Total Project Cost (Tier 1 + Tier 2 + Tier 3)	\$ 12,811,000	
4	Posey Park improvements	\$ 1,489,000
	Artistic expression: Crosswalks	\$ 371,000
	Artistic expression: Paving	\$ 969,000
	Artistic expression: Seatwalls	\$ 45,000
	Artistic expression: Benches	\$ 42,000
	Additional stormwater management: permeable vehicular pavement	\$ 1,136,000
	Subtotal	\$ 4,052,000
	Contingency (35%)	\$ 1,418,000
	Design (15% of total)	\$ 821,000
	Total	\$ 6,291,000
Total Project Cost (Tier 1 + Tier 2 + Tier 3 + Tier 4)	\$ 19,102,000	

CITY COUNCIL REVIEW DRAFT

APPENDIX

- A. UTILITY ASSESSMENT
- B. PHOTOMETRIC ASSESSMENT
- C. ACCESSIBILITY ASSESSMENT
- D. HYDROLOGY ASSESSMENT
- E. COST ESTIMATE
- F. TRANSPORTATION ASSESSMENT AND RECOMMENDATIONS
- G. ANGLED PARKING EVALUATION
- H. NOTES FROM COMMUNITY-ENGAGEMENT AND STAKEHOLDER MEETINGS

A. UTILITY ASSESSMENT



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Redwood City

CSW/Stuber-Stroeh Engineering Group, Inc.

Engineers | Land Planners | Surveyors

MEMORANDUM

DATE: July 30, 2019 **FILE:** 2019-20-013
TO: City of San Bruno, Community Development Department
FROM: Julia Harberson, CSW|ST2
RE: **UTILITY ASSESSMENT FOR SAN MATEO AVENUE, SAN BRUNO**

This memorandum addresses the assessment of the existing utility infrastructure within San Mateo Avenue, San Bruno. Below is a summary of the condition and capacity of the existing utilities (Water, Sanitary Sewer, Stormwater & PG&E). The area of interest for the San Mateo Avenue corridor is between El Camino Real and Huntington Avenue. Identified, as well, are suggested near term infrastructure improvements and rehabilitation that must be addressed to support future development along the corridor.



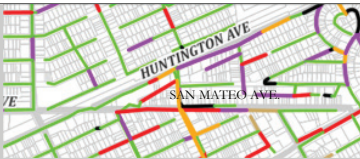

SANITARY SEWER**EXISTING CONDITIONS:**

The following information on the existing sanitary sewer system within San Mateo Avenue between El Camino Real and Huntington Avenue was obtained from the *City of San Bruno Sewer Master Plan – February 2014 (SSMP)*. Inspections of the existing sanitary sewer system was performed, with closed-circuit television (CCTV), and determined the structural grade of sanitary sewer pipes and condition assessments. Inspections of the existing sanitary sewer

was performed, with closed-circuit television (CCTV), and determined the structural grade of sanitary sewer pipes and condition assessments. Inspections of the existing sanitary sewer system with CCTV provided data used to develop a statistical probability of pipe “failure” based on sewer age, and using a Weibull distribution, a probability density function was developed to determine the mean pipe lifetime of 90 years. The existing sanitary sewer system was constructed in the 1930’s. The existing sanitary sewer system is nearing the end of it’s 90-year lifecycle. Modeling of the sewer trunk mains identified the vicinity of the intersection of Kains and San Mateo Avenues as deficient, where overflow and surcharge may occur. The condition and capacity of the sanitary sewer line within San Mateo Avenue between El Camino Real and Angus Avenue and South of Kains Avenue to Kains Avenue were upgraded as a part of **Capacity Improvement Projects (CIPs) C-5A – Kains Avenue Bypass, C5B Kains Avenue Improvements** and **C-7^a – San Mateo Avenue Bypass** identified in the SSMP. These projects are detailed further below in Table 2.

Included in Table 1, below, are excerpts and references from the *City of San Bruno Sewer Master Plan – February 2014* that have been utilized for this assessment to identify deficiencies in the sanitary sewer system. The current infrastructure within San Mateo Avenue consists of a 24-inch main located within the street between El Camino Real and Kains Avenue. The CIPs above abandoned a 6-inch sewer main in the sidewalk on the westside of San Mateo Avenue between El Camino Real and Angus Avenue.

Table 1: Utilized Existing Conditions Excerpts from the *City of San Bruno Sewer Master Plan – February 2014*

Reference Figure/ Table ID	Reference Figure/ Table Name	Description	Description (In Vicinity of San Mateo Avenue)	Pertinence
Figure 1-2	Existing Sewer System	The sizes of the pipes in the existing sanitary sewer system are illustrated in this figure.	The existing sanitary sewer pipe within San Mateo Avenue is less than 8-inches.	These pipes may have capacity issues.
Figure 1-3	Sewer Installation Dates	This figure shows the decade in which the sanitary sewer pipes were installed.	The sanitary sewer pipes within San Mateo Avenue were installed in the 1930's.	The lifetime of these pipes were found to be about 90-years. The pipes in San Mateo Avenue are approaching the end of their lifespan.
Figure 3-2	Predicted Areas of Sewer Surcharge under Design Storm Peak Wet Weather Flow	Sanitary sewer pipes and manholes where predicted surcharge or overflow are shown. Navy blue shows the modeled sewer. Orange indicates where backwater surcharge occurs. Red indicates where throttle surcharge occurs. The teal circles indicates manholes where overflow occurs.	 Surcharging is predicted within the sanitary sewer pipes within Kains Avenue and Huntington Avenue in the vicinity of San Mateo Avenue. Sanitary sewer overflow is predicted at the manholes in the vicinity of the intersection of San Mateo and Kains Avenue.	The vicinity of the intersection of Kains Avenue and San Mateo Avenue is an area susceptible to overflow and surcharging.
Figure 4-2	Highest Structural Grade of Inspected Sewers	The figure shows the structural grade of the inspected sanitary sewers where five (5) is the most severe. Five (5) is red, four (4) is purple, three (3) is orange, two (2) is pink, one (1) is green, and zero (0) is blue.	 The structural grade of the inspected sanitary sewers within San Mateo Avenue were found to be five (5), four (4) and three (3)	This indicates that based on structural grade, replacement or rehabilitation may be necessary for the sanitary sewers within San Mateo Avenue
Figure 4-3	Sewer Renewal Decision Analysis Results	The figure show where it is suggested that the pipe be replaced (red), lined (orange), localized repair (purple), replaced or lined for roots (brown) or maintained (green). Black is used to show where the pipe inspection is incomplete and need to be re-inspected.	 Along San Mateo Avenue, the figure above shows red, purple, brown and green.	This indicates that based on the condition assessment it is advised that sections of the sanitary sewer pipe along San Mateo Avenue be replaced, repaired or lined.
Figure 3-3	Overview of Capacity Improvement Projects	The figure shows the locations and IDs of the capacity improvement projects.	 The highlighted segments in the excerpt above show where the capacity improvement projects are located and what the project ID.	These projects have been completed. See Table 2 below for project details.

IDENTIFIED INFRASTRUCTURE IMPROVEMENTS:

The condition and capacity of the Sanitary Sewer Lines on San Mateo Avenue between El Camino Real and Huntington Avenue are addressed in the *City of San Bruno Sanitary Sewer Master Plan – February 2014*. The deficiencies in the condition and capacity of the Sanitary Sewer line within San Mateo Avenue between El Camino Real and Huntington Avenue have been resolved by the CIPs identified in Table 2 above.

The San Mateo Water and Sewer Replacement project abandoned the existing 6-inch sanitary sewer line in the sidewalk on the west side of San Mateo Avenue between El Camino Real and Angus Avenue. Portions of the abandon sanitary sewer will require removal, in the event green and sustainable infrastructure streetscape improvements are implemented along the San Mateo Avenue corridor. The existing abandoned sanitary sewer is identified in Figure 1 below.

Figure 1: Existing Abandon Sanitary Sewer Location



Table 2: Completed Capacity Improvement Projects from the *City of San Bruno Sanitary Sewer Master Plan – February 2014*

CIP ID	Project Name	Location	Description	Deficiency Addressed/ Comments	Resolution
C-5A	Kains Avenue Bypass	Kains Avenue from San Mateo Avenue to Huntington Avenue	Install a new 200-foot section of 15-inch PVC bypass pipe to divert flow to the new 18-inch sewer on Huntington Avenue	Addresses capacity deficiencies in existing sewers through Artichoke Joe's parking lot and along Huntington Avenue to Cupid Row that have resulted in Sanitary Sewer Overflows (SSOs).	This project has been constructed in conjunction with the Caltrain Huntington Avenue Improvement Project.
C-5B	Kains Avenue Improvement	Kains Avenue from Hensley Avenue to San Mateo Avenue; San Mateo Avenue south of Kains Avenue	Replace approximately 1,000 feet of 10-inch pipe with 12-inch pipe in Kains Ave; replace existing sewers in San Mateo Ave south of Kains Avenue with 10-inch pipe flowing north and connecting to the new 15-inch bypass sewer (Project C-5A)	Addresses capacity deficiency in existing sewer in Kains Avenue east of Masson Avenue and allows abandonment of portion of sewer through Artichoke Joe's parking lot (the remaining portion of sewer will become a lateral servicing Artichoke Joe's)	This project has been constructed.
C-7 ^a	San Mateo Avenue Bypass	San Mateo Avenue from Taylor Avenue to Angus Avenue	Install approximately 2,000 feet of new 18-inch pipe; install a weir to divert most flow at Taylor Avenue into the new 18-inch sewer; abandon the existing 6-inch pipe along west side of San Mateo Avenue and reconnect laterals to new sewer; install new pipes to connect flow from sewers in Jenevein Avenue and Angus Avenue to new 18-inch sewer.	Addresses capacity deficiencies in sewers in the Cupid ROW area that have resulted in SSOs, and allows abandonment of existing shallow 6-inch pipes in San Mateo Avenue sidewalk. Will also allow future abandonment of existing 6-inch easement sewers between San Mateo Avenue and Mastick Avenue	This project has been constructed as a part of the San Mateo Avenue Water & Sewer Replacement Project – Project No. 84151 & 84341. A 24-inch Sanitary Sewer was installed.

WATER SYSTEM

EXISTING CONDITIONS:

The following information on the existing water system was obtained from the *City of San Bruno Water System Master Plan – November 2012 (WSMP)*. The area of concern in this utility assessment, within San Mateo Avenue between El Camino Real and Huntington Avenue, is located in pressure zone 1/4. The water supply sources for pressure zone 1/4 are San Francisco Public Utilities Commission (SFPUC) (Tanforan (C1) and Whitman (C5) Turnouts), Pump Station 6 (Well 17), and Wells 16, 18, and 20. Storage **Tank T1 – Cunningham Drive**, with a total capacity of 2.5 million gallons, is the only storage tank serving pressure zone 1/4. Generally, the water system pipelines within the City's service area are made of cast iron (CI), asbestos concrete (AC), polyvinyl chloride (PVC) or galvanized steel (2-inch pipelines). The City has standardized on ductile iron cement lined (DICL) pipe. Based on recommendations from the City's 2001 Water System Master Plan, the City's Supervisory Control and Data Acquisition (SCADA) was installed in 2001. No significant issues have been identified from the City's SCADA system.

San Mateo Avenue between El Camino Real and Huntington Avenue is a part of the transit corridor where future development is planned. The predicted water demand (FY 2029/30) is 0.42 million gallons per day (292 gallons per minute) for the Transit Corridor is larger than the likely future demand along San Mateo Avenue between El Camino Real and Huntington Avenue. Pumping capacity evaluation, storage capacity evaluation, and pressure regulating station capacity evaluation analysis were performed for the *WSMP*. The pumping capacity evaluation found the existing total and firm guaranteed) pumping capacity to be less than the required firm pumping capacity. The storage capacity evaluation determined the storage for pressure zone 1/4 is sufficient having a storage surplus of 0.06 million gallons (MG). Pressure zone 1/4 is not depended on pressure regulating stations for supply; as such, the pressure regulating station capacity evaluation is not pertinent to this assessment.

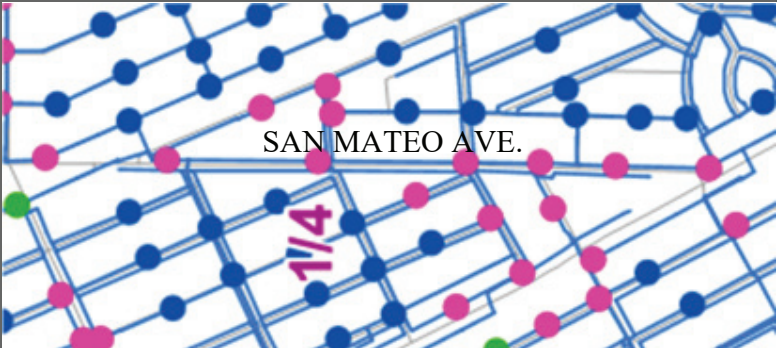
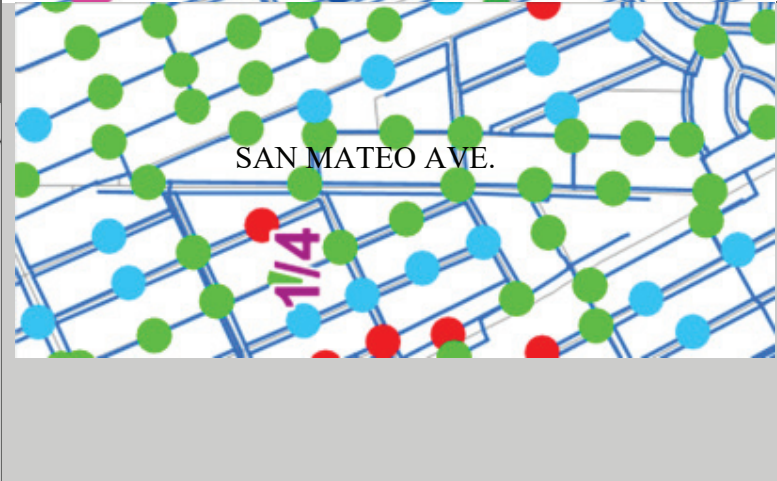
In the *WSMP*, hydraulic analysis was used to identify pipeline improvements. Using general pipeline age, material information and leak history data, a focused pipeline rehabilitation and replacement program was developed. In general, the leak history demonstrates that the number of leaks generally increases as the pipe diameter decreases, and the number of leaks increases as pipe age increases.


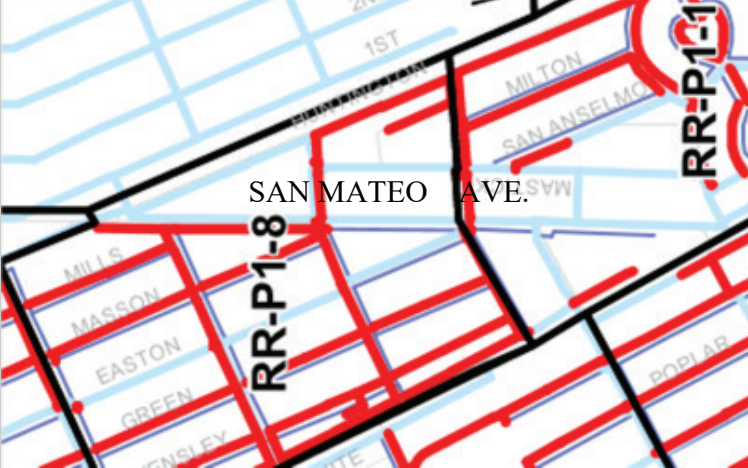
As shown in Figure 7-11: Recommended Existing System Rehabilitation and Replacement Improvements, *WSMP*, San Mateo Avenue between El Camino Real and Angus Avenue was identified as a pipeline rehabilitation and replacement project, **RR-3**, was completed as a part of the *San Mateo Avenue Water & Sewer Replacement Project – Project No. 84151 & 84341*. An 8-inch water line was installed within the street as a part of the project. The replacement project abandoned multiple waterlines within the sidewalks on both sides of San Mateo Avenue.

Table 4, below, identifies excerpts and references from the *City of San Bruno Water System Master Plan – November 2012* that have been utilized for this assessment, to determine the existing condition and capacity of the water system.

Table 4: Utilized Existing Conditions Excerpts from the *City of San Bruno Water System Master Plan – November 2012*

Reference Figure/ Table ID	Reference Figure/Table Name	Description	Description (In Vicinity of San Mateo Avenue)																			
Figure 2-2	Pressure Zones	Illustrates the different pressure zones in San Bruno	San Mateo Avenue between El Camino Real and Huntington Avenue are in pressure zone 1/4.																			
Table 2-1	Summary of Existing Pressure Zones	<table border="1"> <thead> <tr> <th>Pressure Zone</th> <th>Range of Service Elevations, feet msl</th> <th>HGL of Tank, Regulating Station or Turnout, feet msl</th> <th>Static Service Pressures, psi</th> <th>Water Supply Source(s)</th> </tr> </thead> <tbody> <tr> <td>Zone 1/4</td> <td>5-164</td> <td>247</td> <td>36-105</td> <td>SFPUC (Tanforan (C1) and Whitman (C5)) Turnouts^(b) Pump Station 6 (Well 17) Wells 16, 18, and 20</td> </tr> </tbody> </table>	Pressure Zone	Range of Service Elevations, feet msl	HGL of Tank, Regulating Station or Turnout, feet msl	Static Service Pressures, psi	Water Supply Source(s)	Zone 1/4	5-164	247	36-105	SFPUC (Tanforan (C1) and Whitman (C5)) Turnouts ^(b) Pump Station 6 (Well 17) Wells 16, 18, and 20										
Pressure Zone	Range of Service Elevations, feet msl	HGL of Tank, Regulating Station or Turnout, feet msl	Static Service Pressures, psi	Water Supply Source(s)																		
Zone 1/4	5-164	247	36-105	SFPUC (Tanforan (C1) and Whitman (C5)) Turnouts ^(b) Pump Station 6 (Well 17) Wells 16, 18, and 20																		
Table 2-6	Storage Tank Facilities	<table border="1"> <thead> <tr> <th rowspan="2">Storage Tank ID</th> <th rowspan="2">Pressure Zone</th> <th rowspan="2">Ground Surface Elevation, feet msl</th> <th rowspan="2">Diameter, feet</th> <th rowspan="2">Height, feet^(b)</th> <th colspan="3">Capacity, MG^(b)</th> </tr> <tr> <th>Total</th> <th>Operational Minimum</th> <th>Operational Maximum</th> </tr> </thead> <tbody> <tr> <td>T1 - Cunningham Drive</td> <td>1/4</td> <td>231</td> <td>116</td> <td>32</td> <td>2.5</td> <td>0.94</td> <td>1.95</td> </tr> </tbody> </table>	Storage Tank ID	Pressure Zone	Ground Surface Elevation, feet msl	Diameter, feet	Height, feet ^(b)	Capacity, MG ^(b)			Total	Operational Minimum	Operational Maximum	T1 - Cunningham Drive	1/4	231	116	32	2.5	0.94	1.95	
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T1 - Cunningham Drive	1/4	231	116	32	2.5	0.94	1.95															
Table 3-10	Water Demand at Buildout of General Plan and Transit Corridors Plan (FY 2029/30)	Table of predicted water use for future development.	Based on water use estimated using decision support system (DSS) model as a part of Transit Corridors Plan water supply agreement (WSA) analysis, the predicted total water use for the Transit Corridors Plan is 0.42 million gallons per day (mgd) and 292 gallons per minute (gpm)																			
Figure 7-8	Leak Statistics	This figure shows two graphs. One is Leaks Per 1,000 ft by Diameter. The other is Leaks per 1,000 ft by Age.	The number of Leaks increases as the pipe diameter decreases. The number of leaks increases as the pipe age increases.																			

<p>Figure 7-2</p>	<p>Recommended Existing System Fire Flow Criteria</p>	<p>This figure illustrates the existing fire flow criteria where navy blue represents 1,500 gallons per minute (gpm), green is 2,000 gpm, orange is 2,500 gpm, and pink is 3,000 gpm.</p>		<p>The recommended fire flow criteria in the vicinity of San Mateo Avenue is 3,000 gpm.</p>
<p>Figure 7-4</p>	<p>Comparison of Available Fire Flow and Fire Flow Criteria – Existing System</p>	<p>The following excerpt shows the criteria shown in this figure.</p> <p>LEGEND</p> <ul style="list-style-type: none"> ● AvailableFlow < 1/3 of Fire Flow Criteria ● AvailableFlow > 1/3 of Fire Flow Criteria and < Fire Flow Criteria ● Available Flow > Fire Flow Criteria Pressure Zone Boundary — Pipeline — Street 		<p>Along San Mateo Avenue, there is sufficient available flow greater than fire flow criteria. However, in the vicinity of Angus and San Mateo Avenues, the available flow is insufficient.</p>

<p>Figure 7-5</p>	<p>Recommended Water System Improvements Existing System</p>	<p>This figure illustrates where suggested improvements such as upsizing pipelines, new pipelines, new wells, new booster pump stations, new pressure regulating stations, and new storage tanks occur.</p> <ul style="list-style-type: none"> • Orange – Proposed 8-inch Upsize 		<p>Along San Mateo Avenue between Angus Avenue and Kains Avenue, it is recommended that an 8-inch pipe line upsized occur. There is a new well proposed in the vicinity of Taylor Avenue and Mastick Avenue.</p>
<p>Figure 7-11</p>	<p>Recommended Existing System Rehabilitation and Replacement Improvements</p>	<p>Red represents RR-P1 projects. Light blue represents RR-P3 projects.</p>		<p>A portion of project RR-3 is located along San Mateo Avenue between El Camino Real and Angus Avenue. This project is detailed further below.</p>
<p>Table 7-8</p>	<p>Recommended Rehabilitation and Replacement Improvement Projects</p>	<p>Capital Improvement Project ID RR-P3. This project involves the replacement of water pipes and upsizing 6-inch pipes to 8-inch pipes.</p>	<p>A portion of this project along San Mateo Avenue between El Camino Real and Angus Avenue was completed as a part of the <i>San Mateo Avenue Water & Sewer Replacement Project – Project No. 84151 & 84341</i>. A new 8-inch water line was installed.</p>	

Identified Infrastructure Improvements:

To address the storage capacity, supply capacity and pipeline condition, the *City of San Bruno Water System Master Plan – November 2012 (WSMP)* identified the following capital improvement projects (CIP) which pertain to the area of concern for this assessment: **EXCIP-P1-7**, **RR-P1-8** and **FUTCIP-P-2**. These CIPs are located and detailed further in Table 5 and Figure 2 below. These projects are required to resolve storage capacity, supply capacity and pipeline condition issues within the corridor. Portions of the abandon water mains will require removal, in the event green and sustainable infrastructure streetscape improvements are implemented along the San Mateo Avenue corridor. An existing 8-inch water main between Sylvan Avenue and Angus Avenue will require relocation in the event green and sustainable infrastructure streetscape improvements are implemented along the San Mateo Avenue corridor.

Figure 2: Water Capital Improvement Project Locations



Table 6: Capital Improvement Project to Remain

Capital Improvement Project ID	Improvement Type	Reason for Improvement	Improvement Description	Reference Table
EXCIP-P1-7/RR-P1-8	Upsized pipeline in some areas and new pipeline in others.	Fire Flow/ Rehabilitation and Replacement	Upsize pipeline to 8-inch in San Mateo Avenue from Kains to Angus Avenues.	Table 9-2: Recommended Reliability Capital Improvement Projects Table 9-3: Recommended Rehabilitation and Replacement Capital Improvement Projects
FUTCIP-P-2	New Pipeline	Provide looping in Transit Corridors Area	New 12-inch pipeline within San Mateo Avenue between El Camino Real and Huntingtin Avenue	Table 9-1: Recommended Capacity Capital Improvement Projects and Chapter 8: Evaluation of Future Water System, Section 8.5.4: Pipelines
RR-P3	Upsize Pipeline	Rehabilitation and Replacement	Replacement of water pipes and upsizing 6-inch pipes to 8-inch pipes.	Table 9-3: Recommended Rehabilitation and Replacement Capital Improvement Projects

STORM DRAIN SYSTEM

EXISTING CONDITIONS:

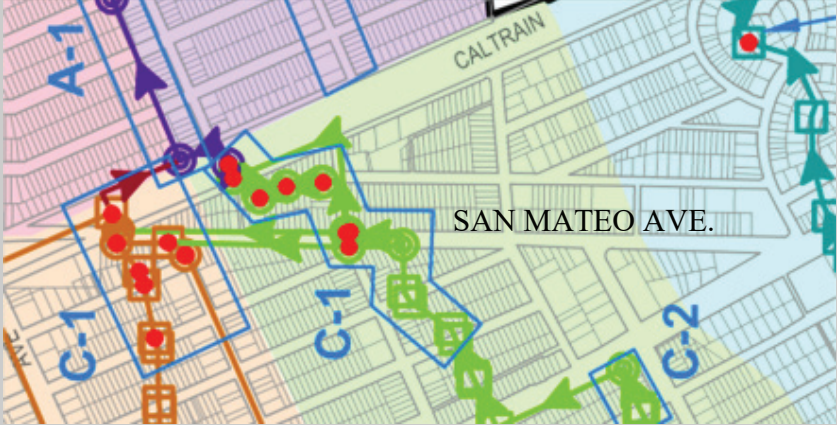
The following information on the existing storm drain system was obtained from the *City of San Bruno Storm Drain Master Plan – June 2014 (SDMP)*. The area of concern in this utility assessment, within San Mateo Avenue between El Camino Real and Huntington Avenue, is located in watersheds A, B, and C, as defined in the SDMP. The storm drain system for watershed A (1415.8 acres) consists of underground pipes, boxes and channels. The storm drain system for watershed B (504.6 acres) consists of underground pipes, boxes, channels, and a detention basin. The storm drain system for watershed C (648.5 acres) consists of underground pipes and boxes. Belle Air Boxes, located between Pine Street and San Bruno Avenue, collects runoff from both watershed A and C from Huntington Avenue before discharging to San Bruno Channel. Watershed B discharges to Cupid Row Canal, Crystal Spring Channel. Hydrologic analysis performed determined the peak 25-year storm flow in cubic feet per second (cfs) to be 1154.3 cfs for watershed A and 572.3 cfs for watershed C. Hydraulic analysis was used to estimate the existing storm water collection system hydraulic capacity, identify potential deficiencies, and assess hydraulic performance of proposed improvement options.

Hydraulic analysis, in the SDMP, determined the following areas, within the vicinity of San Mateo Avenue between El Camino Real and Huntington Avenues, where potential capacity deficiencies may occur: the intersection of Huntington Avenue and San Mateo Avenue, the intersection of Angus Avenue and San Mateo Avenue, and in the vicinity of 1st Avenue and Pine Street, Belle Air Boxes. The capacity of the storm drain system at the intersection of Huntington and San Mateo Avenues is 528.1 cfs while the maximum flow is 843.8 cfs. The maximum flow at the Belle Air Boxes was determined to be 1231.9 cfs while the capacity is 387.3 cfs. The capacity of the primary existing pipe channel within San Mateo Avenue between Sylvan and Angus Avenues was determined to be sufficient.

The existing storm drain infrastructure in San Mateo Avenue consists of a series of catch basins which drain to a 44-inch steel pipe under the sidewalk on the east side of the corridor.

Below in Table 6 are excerpts and references from the City of San Bruno Storm Drain Master Plan – June 2014 that have been utilized for this assessment to identify deficiencies in the storm drain system.

Table 6: Utilized Existing Conditions Excerpts from the *City of San Bruno Storm Drain Master Plan – June 2014*

Reference Figure/ Table ID	Reference Figure/Table Name	Description	Description (In Vicinity of San Mateo Avenue)	Pertinence													
Figure ES-1	Identified Problem Areas by Watershed	This figure shows the watersheds and associated pipe structures by color and identifies the problem areas using red dots. Orange is watershed A, blue is watershed B, and green is watershed C.	 <p>The map displays three watersheds: Watershed A (orange), Watershed B (blue), and Watershed C (green). San Mateo Avenue runs horizontally across the center. Caltrain tracks are visible above the street. Red dots, representing identified problem areas, are clustered at the intersections of San Mateo Avenue with Angus Avenue and Kains Avenue. Specific pipe structures are labeled with codes like A-1, B-1, C-1, and C-2.</p>	San Mateo Avenue between El Camino Real and Huntington Avenue is in Watershed A, B, and C. Identified problem areas occur near the intersection of San Mateo Avenue and Angus Avenue, and the intersection of San Mateo Avenue and Kains Avenue. This area is Identified C-1.													
Table 2.1	Watershed Summary	<table border="1"> <thead> <tr> <th data-bbox="499 911 905 943">Watershed</th> <th data-bbox="905 911 1268 943">Area (acres)</th> </tr> </thead> <tbody> <tr> <td data-bbox="499 943 905 976">Watershed A</td> <td data-bbox="905 943 1268 976">1415.8</td> </tr> <tr> <td data-bbox="499 976 905 1008">Watershed B</td> <td data-bbox="905 976 1268 1008">504.6</td> </tr> <tr> <td data-bbox="499 1008 905 1040">Watershed C</td> <td data-bbox="905 1008 1268 1040">648.5</td> </tr> </tbody> </table>		Watershed	Area (acres)	Watershed A	1415.8	Watershed B	504.6	Watershed C	648.5						
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<p>Table 4.1</p>	<p>Capacity Deficiency Summary for Watersheds A, B, C, E, and F</p>	<p>The capacity evaluation using hydraulic modeling determined capacity deficiencies based on a 25-year design storm.</p>	<table border="1"> <thead> <tr> <th data-bbox="821 420 961 451">Watershed A</th> <th data-bbox="968 420 1129 451">Capacity/Max Flow</th> <th data-bbox="1136 420 1276 451">Watershed C</th> <th data-bbox="1283 420 1444 451">Capacity/Max Flow</th> </tr> </thead> <tbody> <tr> <td data-bbox="821 456 961 503">Earl Avenue at Crosby Court</td> <td data-bbox="968 456 1129 503">25.9/301.3</td> <td data-bbox="1136 456 1276 503">Madison Avenue</td> <td data-bbox="1283 456 1444 503">39.6/58.7</td> </tr> <tr> <td data-bbox="821 508 961 579">Northeast of the intersection of Freeways 280 and 380</td> <td data-bbox="968 508 1129 579">525.1/532.2</td> <td data-bbox="1136 508 1276 579">Whitman Way</td> <td data-bbox="1283 508 1444 579">84.7/136.4</td> </tr> <tr> <td data-bbox="821 584 961 678">Cherry Avenue</td> <td data-bbox="968 584 1129 678">375.4/689.9</td> <td data-bbox="1136 584 1276 678">Jenevein Avenue</td> <td data-bbox="1283 584 1444 678">25.8/57.8</td> </tr> <tr> <td data-bbox="821 683 961 755">Commodore Drive</td> <td data-bbox="968 683 1129 755">557.8/549.5</td> <td data-bbox="1136 683 1276 755">Cypress Avenue</td> <td data-bbox="1283 683 1444 755">15.4/34.5</td> </tr> <tr> <td data-bbox="821 760 961 790">Grundy Lane</td> <td data-bbox="968 760 1129 790">336.8/539.7</td> <td data-bbox="1136 760 1276 790">Angus Avenue</td> <td data-bbox="1283 760 1444 790">N/A</td> </tr> <tr> <td data-bbox="821 795 961 859">Bayhill Drive</td> <td data-bbox="968 795 1129 859">522.4/588.1</td> <td data-bbox="1136 795 1276 859">Casio's Parking Lot</td> <td data-bbox="1283 795 1444 859">N/A</td> </tr> <tr> <td data-bbox="821 863 961 894">El Camino Real</td> <td data-bbox="968 863 1129 894">12.8/25.0</td> <td data-bbox="1136 863 1276 894">Huntington Avenue</td> <td data-bbox="1283 863 1444 894">528.1/843.8</td> </tr> <tr> <td data-bbox="821 899 961 963">Masson Avenue</td> <td data-bbox="968 899 1129 963">0/369.4 (negative/0 slope)</td> <td data-bbox="1136 899 1276 963">Belle Air Boxes</td> <td data-bbox="1283 899 1444 963">387.3/1231.9</td> </tr> <tr> <td data-bbox="821 967 961 998">Mills Avenue</td> <td data-bbox="968 967 1129 998">218.4/370.6</td> <td></td> <td></td> </tr> <tr> <td data-bbox="821 1003 961 1075">Huntington and San Mateo Avenue intersection</td> <td data-bbox="968 1003 1129 1075">528.1/843.8</td> <td></td> <td></td> </tr> <tr> <td data-bbox="821 1079 961 1110">Belle Air Boxes</td> <td data-bbox="968 1079 1129 1110">387.3/1231.9</td> <td></td> <td></td> </tr> </tbody> </table>				Watershed A	Capacity/Max Flow	Watershed C	Capacity/Max Flow	Earl Avenue at Crosby Court	25.9/301.3	Madison Avenue	39.6/58.7	Northeast of the intersection of Freeways 280 and 380	525.1/532.2	Whitman Way	84.7/136.4	Cherry Avenue	375.4/689.9	Jenevein Avenue	25.8/57.8	Commodore Drive	557.8/549.5	Cypress Avenue	15.4/34.5	Grundy Lane	336.8/539.7	Angus Avenue	N/A	Bayhill Drive	522.4/588.1	Casio's Parking Lot	N/A	El Camino Real	12.8/25.0	Huntington Avenue	528.1/843.8	Masson Avenue	0/369.4 (negative/0 slope)	Belle Air Boxes	387.3/1231.9	Mills Avenue	218.4/370.6			Huntington and San Mateo Avenue intersection	528.1/843.8			Belle Air Boxes	387.3/1231.9			<p>From the excerpts, the intersection of Huntington and San Mateo Avenues is under capacity. Along Angus Avenue, in the vicinity of San Mateo Avenue, was identified as an area where capacity issues may occur.</p>
Watershed A	Capacity/Max Flow	Watershed C	Capacity/Max Flow																																																				
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Identified Infrastructure Improvements:

To address the capacity deficiencies affecting San Mateo Avenue between El Camino Real and Huntington Avenue, the *City of San Bruno Storm Drain Master Plan – June 2014* identified the following CIP, **CD-1**. **CD-1** is located and detailed further in Figure 3 and Table 7 below. The **CD-1** capital improvement project should be completed prior to streetscape improvements, to prevent future impacts to the San Mateo Avenue corridor. A 44-inch steel storm drain is located under the sidewalk, between El Camino Real to Sylvan Avenue, which will impact green and sustainable infrastructure streetscape improvements proposed for the San Mateo Avenue corridor. An existing concrete box culvert is located beneath the parking strip and sidewalk in the 500 block of San Mateo Avenue that will impact green and sustainable infrastructure streetscape improvements proposed for the corridor. Based on information from the City, the existing concrete culvert from 555 to 715 San Mateo Avenue must be replaced due to the condition of the roof.

Figure 3: Storm Drain Capital Improvement Project Location

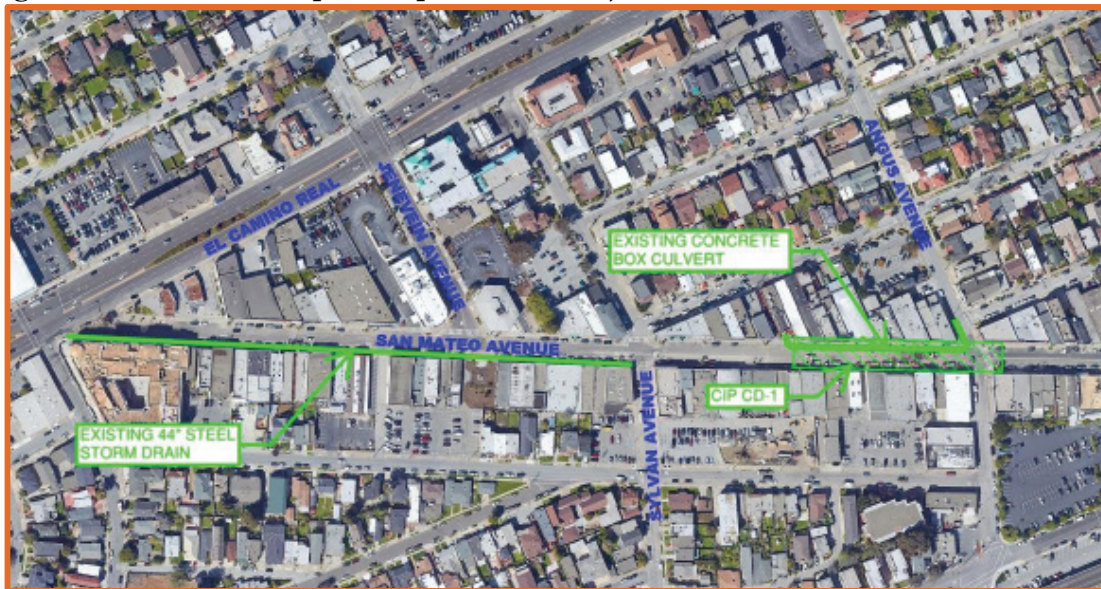


Table 7: Capital Improvement Project to Remain

Project ID	Description & Location	Constructed	Deficiency	Improvements	Resolution
CD-1	Bolt Manholes & Install Catch Basin Backflow Preventers in the vicinity of San Mateo, Huntington, Angus and Kains Avenues	1900s	Flat area; existing ground elevations are low; existing pipe has very shallow cover	<u>Existing:</u> Gravity main trunk collect local runoff <u>Proposed:</u> Pressurized main trunk at peak flow	This project is to remain a CIP to be completed in the near future.

PACIFIC GAS AND ELECTRIC SYSTEM:

EXISTING CONDITIONS:

Based on information provided by the Pacific Gas and Electric Company (PG&E), there is sufficient capacity for the current demand on the gas and electrical systems. PG&E identified sections of empty conduits within San Mateo Avenue between El Camino Real and Huntington Avenue intended for future use, if necessary.

IDENTIFIED INFRASTRUCTURE IMPROVEMENTS:

At this time, PG&E does not recommend additional conduits.

B. PHOTOMETRIC ASSESSMENT



45 Leveroni Court
Novato, CA 94949
www.cswst2.com

415.883.9850
Fax: 415.883.9835

Novato
Petaluma
Sacramento
Redwood City

CSW/Stuber-Stroeh Engineering Group, Inc.

Engineers | Land Planners | Surveyors

MEMORANDUM

DATE: July 15, 2019 **FILE:** 2019-20-013
TO: City of San Bruno, Community Development Department
FROM: Julia Harberson, CSW | ST2
RE: **PHOTOMETRIC ASSESSMENT FOR SAN MATEO AVENUE, SAN BRUNO**

This memorandum addresses our assessment of the photometric analysis along the San Mateo Avenue corridor is summarized below using standards from the American National Standards Institute (ANSI) and the Illuminating Engineering Society of North America (IESNA). The analysis specifically focuses on standards for vehicles, pedestrians and bicyclists.

FACTORS OF ANALYSIS:

- Pedestrian conflict is defined as high, medium or low over a one-hour peak night time period:
 - high pedestrian conflict will have approximately 100 or more pedestrian,
 - The road corridor of this project falls under this category
 - medium pedestrian conflict will have 11 to 99 pedestrians and
 - low pedestrian conflict will have under 10 pedestrian
- Pavement is classified as R1, R2, R3, or R4:
 - R1 = Portland-cement concrete

- R2 & R3 = Asphalt, rough textured (typical roadway)
 - The San Mateo Avenue corridor falls under this category
- R4 = Asphalt, smooth textured
- Road type shown below is defined as a Collector.
- ‘Lux’ is lumens per square meter
- ‘FC’ (foot-candles) is lumens per square foot. This is the preferred unit for California.

The table below shows an abridged Table 2 from IESNA RP-8:

Road and Pedestrian Conflict Area		Pavement Classification <small>(Minimum Maintained Average Values)</small>			Uniformity Ratio E_{avg}/E_{min}	Veiling Luminance Ratio L_{vmax}/L_{avg}
Road	Pedestrian Conflict Area	R1 lux/ftc	R2 & R3 lux/ftc	R4 lux/ftc		
Collector	High	8.0/0.8	12.0/1.2	10.0/1.0	4.0	0.4
	Medium	6.0/0.6	9.0/0.9	8.0/0.8	4.0	0.4
	Low	4.0/0.4	6.0/0.6	5.0/0.5	4.0	0.4

San Mateo Avenue: El Camino Real to Huntington Avenue

- Curb to curb = approximately 44 feet
- Street lights are on both side of San Mateo Avenue.
- Street lights are spaced approximately 120 feet with additional light poles at the intersections
- PHOTOMETRIC CALCULATIONS: El Camino Real to Huntington Avenue
 - Average foot-candles = 0.78
 - Max Foot Candles (FC) = 4.1
 - Min Foot Candles = 0.1
 - Minimum to Maximum FC Ratio: 0.01
 - Maximum to Minimum FC Ratio = 78.55
 - Average to Minimum FC Ratio = 15.02
- Table summary of photometric calculations comparing existing and preferred IESNA standards:

	IESNA Standard	Existing Condition	Compliance (Yes or No)
FC (average value)	1.2	0.78	No
Uniformity Ratio	4.0	15.02	No

- Conclusion: Additional light poles spaced appropriately throughout the entire corridor will increase the minimum FC Ratio and as a result lower the Uniformity Ratio closer to standard IESNA value of 4.0. Ideally the Uniformity Ratio is within 2.0 of the set IESNA Standard. Furthermore, adding additional light poles will have an adverse effect on the Average FC, but not substantial enough to increase the value so high as to be noncompliant. An additional light pole is required midblock between Angus Avenue and Kains Avenue as the foot candle levels fall below the minimum level.



Typical Street Light along San Mateo Avenue

C. ACCESSIBILITY ASSESSMENT



45 Leveroni Court
Novato, CA 94949
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Fax: 415.883.9835

Novato
Petaluma
Sacramento
Redwood City

CSW/Stuber-Stroeh Engineering Group, Inc.

Engineers | Land Planners | Surveyors

MEMORANDUM

DATE: June 21, 2019 **FILE:** 2019-20-013
TO: City of San Bruno, Community Development Department
FROM: Julia Harberson, CSW | ST2
RE: **ACCESSIBILITY ASSESSMENT FOR SAN MATEO AVENUE, SAN BRUNO**

This memorandum addresses our assessment of accessibility analysis along the San Mateo Avenue corridor. Summarized below are the applicable standards from the Americans with Disabilities Act (ADA), Part 2 of Title 24 of the California Building Code and California Disabled Accessibility Guidebook (CalDAG) used in this analysis. The analysis specifically focuses on standards for pedestrian access and future streetscape improvements and is limited to the existing sidewalk within the public right of way. Analysis of building entries outside the public right of way are not included in this analysis.

FACTORS OF ANALYSIS:

- Accessible Routes:
 - Accessible routes shall consist of one or more of the following components
 - Walking surface with a running slope not steeper than 1:20
 - Doorways
 - Ramps
 - Curb ramps, excluding the flared sides.
 - All components of an accessible route shall comply with Title 24.
- Cross Slope:
 - Maximum = 1/4" per foot or 2.0%
 - Recommended Design = 1.75%
- Doorway Landings: all doorways shall provide a 48" level (maximum 1/4" per foot slope) landing. Note this analysis only applies when building doorways are located at the back of sidewalk (not recessed).



Typical Street Section along San Mateo Avenue

ANALYSIS OF EXISTING CONDITIONS:

General Sidewalk Accessibility Condition

- Standard City of San Bruno 6" Curb and Gutter along entire San Mateo Avenue corridor.
- Typical Sidewalk Section: sidewalk generally slopes from back of sidewalk towards the street (note: various locations along the corridor, the concrete flags have broken at the construction

joints preventing a continuous slope across section and causing warped and reverse slope sidewalk).

- Typical Section at Bulb-outs: sidewalk generally slopes from back of sidewalk towards curb line of street, then reverse slope in the bulb-out following cross slope of street.

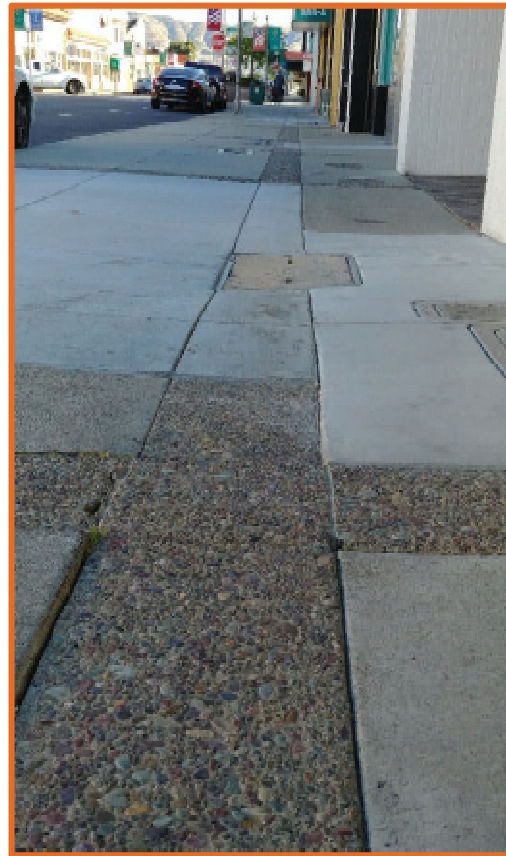
East Side of San Mateo Avenue Corridor

El Camino Real to Cypress Court:

- Average Cross Slope = approximately 0.7%
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Compliant
- Conclusion: existing street and back of sidewalk elevations allow for construction of ADA compliant corridor improvements.

Cypress Court to Sylvan Avenue:

- Average Cross Slope = approximately 1.0%
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Compliant
- Conclusion: existing street and back of sidewalk elevations allow for construction of ADA compliant corridor improvements.



Warped and Reversed Slope Sidewalk

Sylvan Avenue to Angus Avenue:

- Average Cross Slope = approximately 0.9%
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Compliant
- Conclusion: existing street and back of sidewalk elevations allow for construction of ADA compliant corridor improvements.



Existing Ramp without Detectable Warning

Angus Avenue to Kains Avenue:

- Average Cross Slope = approximately 1.0%
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Non-compliant
- Conclusion: existing street and back of sidewalk elevations allow for construction of ADA compliant corridor improvements.

West Side of San Mateo Avenue Corridor

El Camino Real to Jenevien Avenue:

- Average Cross Slope = approximately 1.8% (with segments in excess of 2%)
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Compliant
- Conclusion: Sidewalk cannot be reconstructed as a continuous surface and maintain ADA compliant cross slopes. Installation of a Furniture Zone of Street Side Bioretention Area can be utilized to absorb grade differential and create an ADA compliant Pedestrian Zone.



Sidewalk Section with Furniture Zone

Jenevien Avenue to Sylvan Avenue:

- Average Cross Slope = approximately 1.9% (with segments in excess of 2%)
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Compliant
- Conclusion: Sidewalk cannot be reconstructed as a continuous surface and maintain ADA compliant cross slopes. Installation of a Furniture Zone of Street Side Bioretention Area can be utilized to absorbed grade differential and create an ADA compliant Pedestrian Zone.



Sylvan Avenue to Angus Avenue:

- Average Cross Slope = approximately 1.9% (with segments in excess of 2%)
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Non-compliant
- Conclusion: Sidewalk cannot be reconstructed as a continuous surface and maintain ADA compliant cross slopes. Installation of a Furniture Zone of Street Side Bioretention Area can be utilized to absorbed grade differential and create an ADA compliant Pedestrian Zone.

Street Side Bioretention (Source: City of Seattle, WA)

Angus Avenue to Kains Avenue:

- Average Cross Slope = approximately 1.7% (with segments in excess of 2%)
- Doorway Landings: Recessed outside of public right of way.
- Ramps: Non-compliant
- Conclusion: Sidewalk cannot be reconstructed as a continuous surface and maintain ADA compliant cross slopes. Installation of a Furniture Zone of Street Side Bioretention Area can be utilized to absorbed grade differential and create an ADA compliant Pedestrian Zone.

Hydraulic analysis, in the *City of San Bruno Storm Drain Master Plan – June 2014*, determined the following areas, within the vicinity of San Mateo Avenue between El Camino Real and Huntington Avenues, where potential capacity deficiencies may occur: the intersection of Huntington Avenue and San Mateo Avenue and the intersection of Angus Avenue and San Mateo Avenue. The capacity of the primary existing pipe channel within San Mateo Avenue between Sylvan and Angus Avenues was determined to be sufficient.

IDENTIFIED INFRASTRUCTURE IMPROVEMENTS:

To address the capacity deficiencies affecting San Mateo Avenue between El Camino Real and Huntington Avenue, the *City of San Bruno Storm Drain Master Plan – June 2014* identified the following capital improvement project (CIP), **CD-1**, to address these deficiencies. **CIP CD-1**, which encompasses bolt manholes and the installation of catch basin backflow preventers, will need to remain a near future CIP and be completed prior to streetscape improvements. Figure 1 below, shows the location of CIP CD-1.

Figure 1: Storm Drain Improvement Project Locations



GREEN AND SUSTAINABLE INFRASTRUCTURE:

The San Mateo Countywide Water Pollution Prevention Plan (SMCWPPP) C.3 Stormwater Technical Guidance (Version 5 – June 2016) states that C.3 Regulated projects are defined to include “public and private projects that create and/or replace 10,000 square feet or more of impervious surface ...” The San Mateo Avenue Corridor has a total watershed of approximately 326,300 square feet. The project proposes to replace existing impervious sidewalk. This will result in the creation and/or replacement of 49,360 square feet and trigger the C.3 regulations. However, Table 2-1: Projects Excluded from Provision C.3 Requirements, of the SMCWPPP C.3 Stormwater Technical Guidance manual, specifically excludes sidewalks. As such, the Priority 1 scope of the project (sidewalk demolition, sidewalk construction, street trees and lighting) would result in an exempt project. However, as the scope of the project is increased to include additional Priority Items (Priority 2 to 4, identified in the San Mateo Avenue Streetscape Plan) the scope of the project expands beyond a sidewalk project and will trigger the C.3 regulations.

SMCWPPP C.3 Stormwater Technical Guidance, suggested Low Impact Development (LID) design methods applicable to the San Mateo Avenue street scape improvements include interceptor trees (Figure 2 below), bioretention areas (Figure 3 below) and pervious pavement (Figure 4 below).

Interceptor Trees

Trees perform a variety of functions that reduce runoff volumes and improve water quality. Leaf canopies intercept and hold rainwater on the leaf surface, preventing it from reaching the ground and becoming runoff. Root systems create voids in the soil that facilitate infiltration. Trees also absorb and transpire large quantities of groundwater, making the soil less saturated, which allows more stormwater to infiltrate. Through the absorption process, trees remove pollutants from stormwater and stabilize them. Finally, tree canopies shade and cool paved areas.

A project may earn stormwater treatment credits by planting new trees and preserving existing trees at the project site. For each qualifying tree that is planted or preserved, the project earns stormwater treatment credits, which reduce the surface area (measured in square feet) of the project that must receive stormwater treatment. In other words, the stormwater treatment credit can be subtracted from the amount of impervious surface area requiring treatment.

As shown in Table 1, different amounts of stormwater reduction credit are assigned to new evergreen and new deciduous trees, and existing trees receive credit for the square footage that is under the existing tree canopy.

	New Evergreen Trees	New Deciduous Trees	Existing Trees
Credits for new and existing trees that meet interceptor tree minimum requirements	200 square feet	100 square feet	Square footage under the tree canopy for trees with an average DBH of 12 inches or more.
*DBH: Diameter at breast height (4.5 feet above grade) Source: BASMAA LID Feasibility Criteria Report, 2011 (based on the tree credit system in the State Construction General Permit standards for post-construction stormwater control)			

Load-bearing modular grid products, such as the Silva Cell, have also been developed to allow the planting of trees in uncompacted native soils, fill soils, or stormwater treatment soils, extending under sidewalks and other areas of pavement. With the Silva Cell product, for example, each cell is composed of a frame (or frames) and a deck (see Figure 2). The frames can be stacked one, two, or three units high before they are topped with a deck to create a maximum amount of soil volume for tree root growth and stormwater infiltration. Cells can be installed laterally as wide as necessary. Void space within the cells may accommodate the surrounding utilities.

Figure 2: Interceptor Tree Cross Section

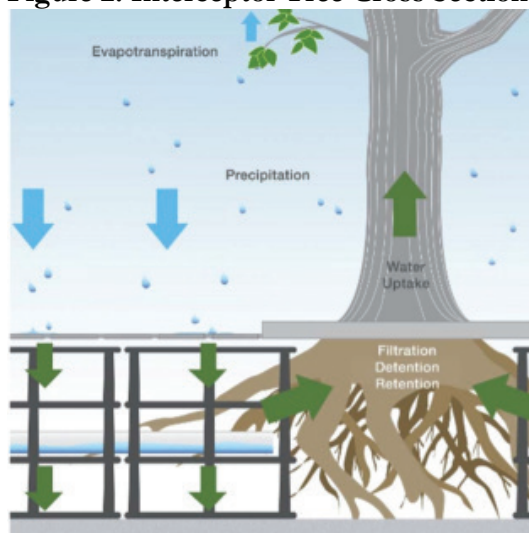
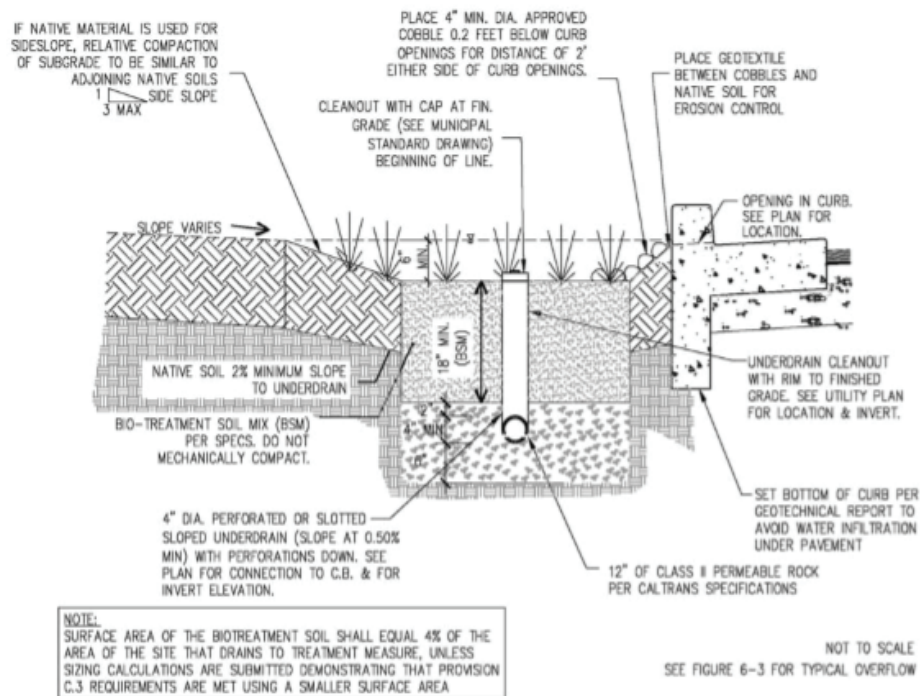


Figure 4-1: Silva Cells, stacked three units high. (Source: Deep Root Technologies, www.deeproot.com). The use of this photograph is for general information only, and is not an endorsement of this or any other proprietary product.

Bioretention Areas

Bioretention areas, or “rain gardens,” are concave landscaped areas that function as soil and plant-based filtration devices that remove pollutants through a variety of physical, biological, and chemical treatment processes. Bioretention areas can be any shape. Bioretention areas normally consist of the following layers, starting from the top: a surface ponding area, a layer of mulch, planting soil and plants, and an underlying rock layer with an underdrain that connects to the municipal storm drain system. The recommended sizing method for bioretention areas is the 4% method where the required surface area of the treatment method is 4 percent of the impervious area that drains to it. The installation of bioretention areas will require the demolition of curb, gutter and sidewalk in the vicinity of the planter.

Figure 3: Bioretention Area Cross Section

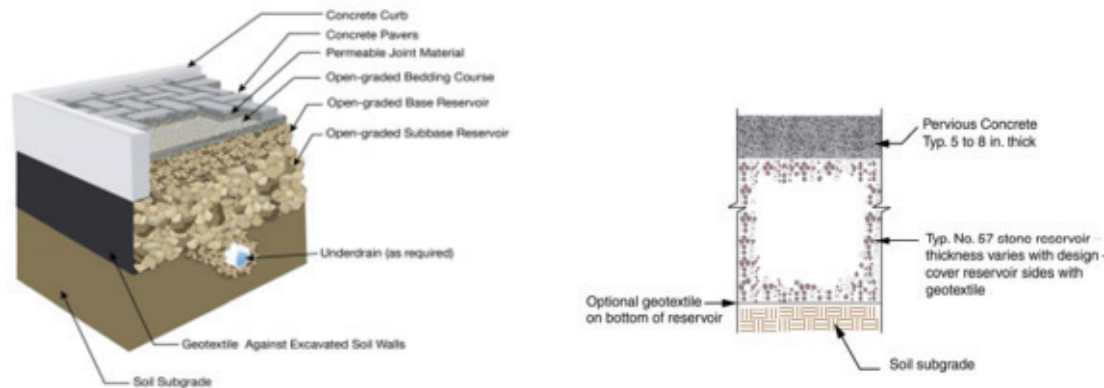


Pervious Pavement

Pervious pavement types include pervious concrete, porous asphalt, permeable concrete pavers and permeable interlocking concrete pavers (PICP). Permeable pavers allow infiltration across the entire surface of the paver while PICP utilize the joint space between the pavers for infiltration. Except for PICP, pervious pavement is generally used for areas with light vehicle loading and/or lightly trafficked areas, such as automobile parking areas. The term pervious pavement describes a system comprised of a load-bearing, durable surface constructed over a subbase/base structure typically consisting of compacted, open-graded aggregate. This layer or layers temporarily stores water prior to infiltration or drainage to a controlled outlet. The surface is porous such that water infiltrates across the entire surface of the material, or in the joints, at a high rate. If an area of pervious pavement is underlain with pervious soil or pervious storage material, such as a gravel layer sufficient to hold at least the Municipal Stormwater Regional Permit Provision C.3.d volume of rainfall runoff, it is not considered an impervious surface and can function as a self-treating or self-retaining area.

Pervious pavements are used as a self-retaining treatment method, the maximum acceptable ratio of pervious area to the receiving pervious area, pervious pavers, is two-to-one (2:1). The installation of pervious pavement will require asphalt removal and shallow excavation to create the reservoir.

Figure 4: Pervious Pavement Section



Analysis

Due to the limited available area within the San Mateo Avenue Corridor, for implementation of green infrastructure, it is not feasible to fully implement a single LID treatment measure to meet the SMCWPPP C.3 stormwater treatment requirements for the entire watershed. Implementation of a combination of interceptor trees, bioretention areas and pervious pavement can be integrated to achieve a maximum feasible percentage of the SMCWPPP C.3 stormwater treatment requirements.

Four (4) scenarios were analyzed to meet the SMCWPPP C.3 stormwater treatment requirements. Scenario 1 would meet the C.3 requirements and treat the sidewalk that is created and/or replaced. Scenarios 2 and 3, would exceed the minimum C.3 requirements by treating the sidewalk that is created and/or replaced as well as a portion of the greater watershed without any revisions to the existing curb line. Scenario 4 studies the possibility of treating the entire San Mateo Avenue Transit Corridor watershed. Each scenario will meet the minimum requirements of the SMCWPPP C.3 Guidance manual. Below is a description of LID treatment measure that would be implemented with each scenario.

LID Scenarios Analyzed:

- Scenario 1: Implements LID measures to manage runoff from the created and/or replaced sidewalk with the decorative pavement band in the sidewalk constructed of permeable pavement sized to act as self-retaining areas and utilizes interceptor trees.
- Scenario 2: Implements LID measures to manage runoff from the created and/or replaced sidewalk with the decorative pavement band in the sidewalk constructed of permeable pavement sized to act as self-retaining areas and portions of the existing watershed pavement with permeable pavement, within the 6-foot wide parking strip, sized to act as self-retaining areas and utilizes interceptor trees.
- Scenario 3: Implements LID measures to manage runoff from the created and/or replaced sidewalk with the decorative pavement band in the sidewalk constructed of permeable pavement sized to act as self-retaining areas and portions of the existing watershed pavement with permeable pavement, within the 6-foot wide parking strip, sized to act as self-retaining areas and utilizes interceptor trees. Note, the use of Silva Cells under the permeable pavement is included in this scenario.
- Scenario 4: Implements LID measures to manage the entire San Mateo Avenue corridor watershed with the combined LID measures of permeable pavement sized to act as self-retaining areas, utilizing Silva Cells, interceptor trees and bioretention areas.

Results

The Drainage Management Areas (DMA) tributary to the San Mateo Avenue Transit Corridor and potential sustainable green infrastructure are identified in Green Infrastructure Exhibits located in Appendix A. Table 2, below, identifies the DMA sub-watershed areas and created and/or replaced impervious surfaces within each sub-watershed.

Results of the analysis for Scenario 1 are identified below in Table 3. The use of the permeable pavement strip in the sidewalk and interceptor trees meets the SMCWPPP C.3 stormwater treatment requirements to manage runoff from the created and/or replaced impervious surface in the San Mateo Avenue Transit Corridor.

Scenario 2 results are depicted in Table 4, below. The use of the permeable pavement strip in the sidewalk and interceptor trees meets the SMCWPPP C.3 stormwater treatment requirements to manage runoff from the created and/or replaced impervious surface in the San Mateo Avenue Transit Corridor. However, there is insufficient permeable area to treat the entire impervious surface of the San Mateo Avenue watershed.

Results of the calculations for Scenario 3 are shown in Table 5. The use of the permeable pavement strip in the sidewalk and interceptor trees meets the SMCWPPP C.3 stormwater treatment requirements to manage runoff from the created and/or replaced impervious surface in the San Mateo Avenue Transit Corridor. However, there is insufficient permeable area to manage the treatment storm event for the entire impervious surface of the San Mateo Avenue watershed with permeable pavers and Silva Cells.

Scenario 4 results are depicted in Table 6, below. The use of the permeable pavement strip in the sidewalk and interceptor trees meets the SMCWPPP C.3 stormwater treatment requirements to manage runoff from the created and/or replaced impervious surface in the San Mateo Avenue Transit Corridor. However, there is insufficient bioretention area, permeable pavement area and Silva cells to manage the treatment storm event for the entire impervious surface of the San Mateo Avenue Transit Corridor watershed. Reducing parking to allow for larger bioretention areas, approximately 3,300 square feet of additional bioretention area, will allow for 100% management of the entire watershed.

Table 2: Summary of San Mateo Avenue Watershed Areas

Drainage Management Area (DMA) ID	Sub-Watershed Area (sf)	Created/Replaced Impervious Area w/o Treatment Measures (sf)
1	8999	2755
2	17582	3019
3	21294	3200
4	22966	2867
5	5016	671
6	24669	2970
7	7184	634
8	6125	1165
9	11750	1431
10	5274	441
11	6329	1023
12	4331	1005
13	10198	1262
14	7094	1191
15	8457	1097
16	16978	1726
17	9371	1376
18	16927	1831
19	17372	3242
20	6387	1012
21	7706	1346
22	4500	644
23	11173	1433
24	11524	1191
25	5058	1400
26	10204	1547
27	6297	2244
28	4187	427
29	6889	1051
30	4182	1531
31	13552	1704
32	1876	473
33	2946	560
34	1823	391

Table 3: Summary of Scenario 1 Results (Management of the created/replaced pavement only)

DMA ID	Created/Replaced Impervious Area w/o Permeable Pavement Area (sf)	Permeable Pavement Area (sf)	Interceptor Trees	Percent of Area Treated (%)
1	1763	992	6	100
2	1931	1088	5	100
3	2200	500	6	100
4	1587	1280	9	100
5	511	160	2	100
6	1938	1032	6	100
7	530	104	1	100
8	645	520	3	100
9	911	520	4	100
10	265	176	1	100
11	739	284	3	100
12	677	328	2	100
13	814	448	3	100
14	675	516	3	100
15	705	392	3	100
16	982	744	4	100
17	896	480	3	100
18	1271	560	4	100
19	2430	812	5	100
20	712	300	2	100
21	894	452	3	100
22	372	272	2	100
23	921	512	3	100
24	679	512	3	100
25	908	492	3	100
26	1067	480	4	100
27	1624	620	4	100
28	255	172	1	100
29	691	360	3	100
30	979	552	3	100
31	1068	636	3	100
32	273	200	2	100
33	320	240	2	100
34	239	152	1	100

Table 4: Summary of Scenario 2 Results (Management of portion of San Mateo Ave Corridor)

DMA ID	Sub-Watershed Area (sf)	Permeable Pavement Area (sf)	Interceptor Trees	Percent of Area Treated (%)
1	8999	1952	6	73.3
2	17582	2138	5	35.8
3	21294	1580	6	19.5
4	22966	2810	9	31.1
5	5016	460	2	24.1
6	24669	1722	6	24.5
7	7184	104	1	7.6
8	6125	1420	3	53.5
9	11750	1750	4	33.8
10	5274	326	1	16.2
11	6329	650	3	24.4
12	4331	628	2	33.0
13	10198	1018	3	23.1
14	7094	1194	3	40.4
15	8457	896	3	25.0
16	16978	1806	4	23.8
17	9371	480	3	12.9
18	16927	1508	4	20.1
19	17372	812	5	12.6
20	6387	738	2	28.5
21	7706	1148	3	35.2
22	4500	590	2	33.0
23	11173	1220	3	25.0
24	11524	1202	3	23.8
25	5058	1200	3	59.8
26	10204	1260	4	29.7
27	6297	1418	4	58.5
28	4187	400	1	24.3
29	6889	726	3	27.3
30	4182	1230	3	78.4
31	13552	1488	3	24.8
32	1876	500	2	77.9
33	2946	516	2	47.2
34	1823	302	1	48.0

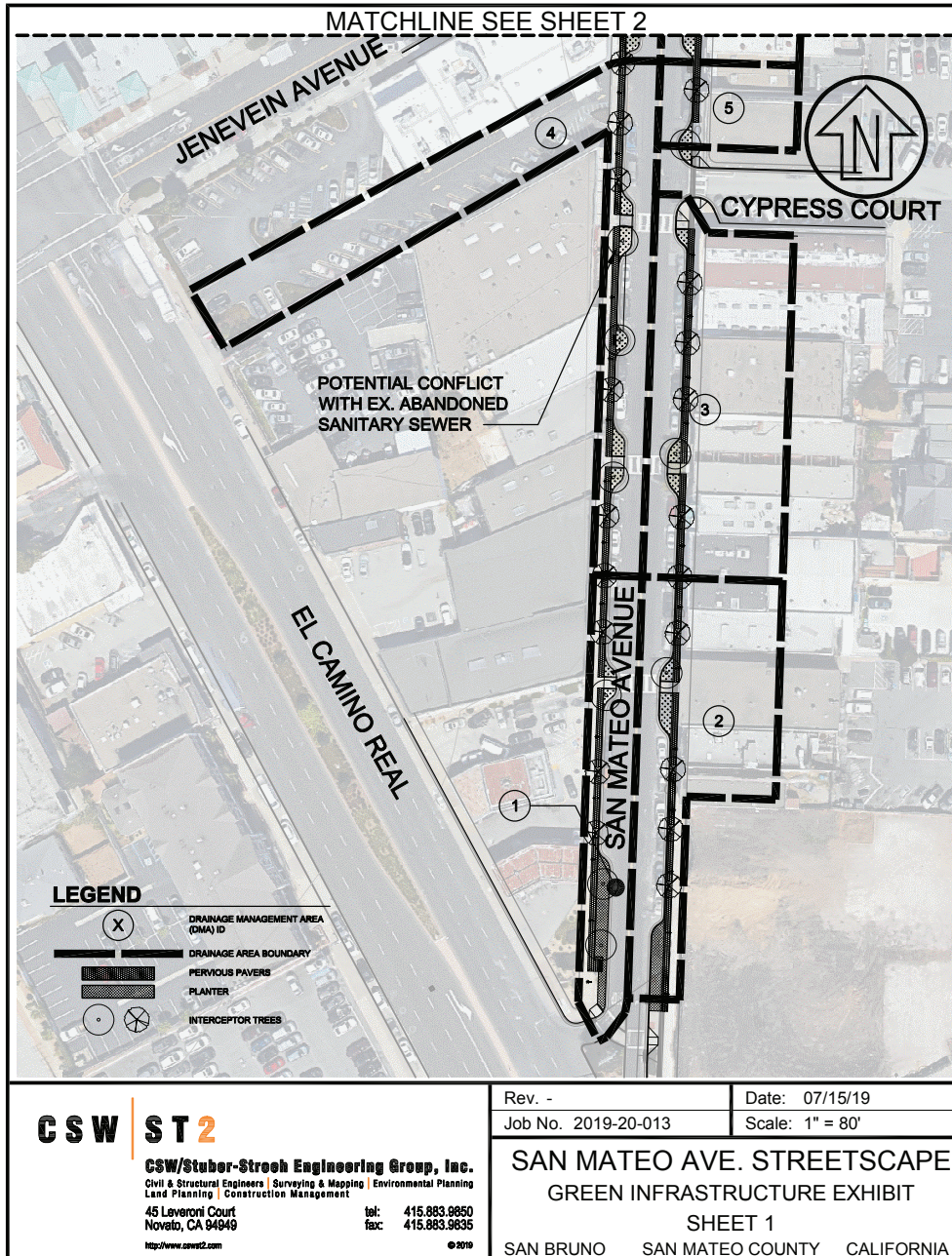
Table 5: Summary of Scenario 3 Results (Management of the San Mateo Ave Corridor)

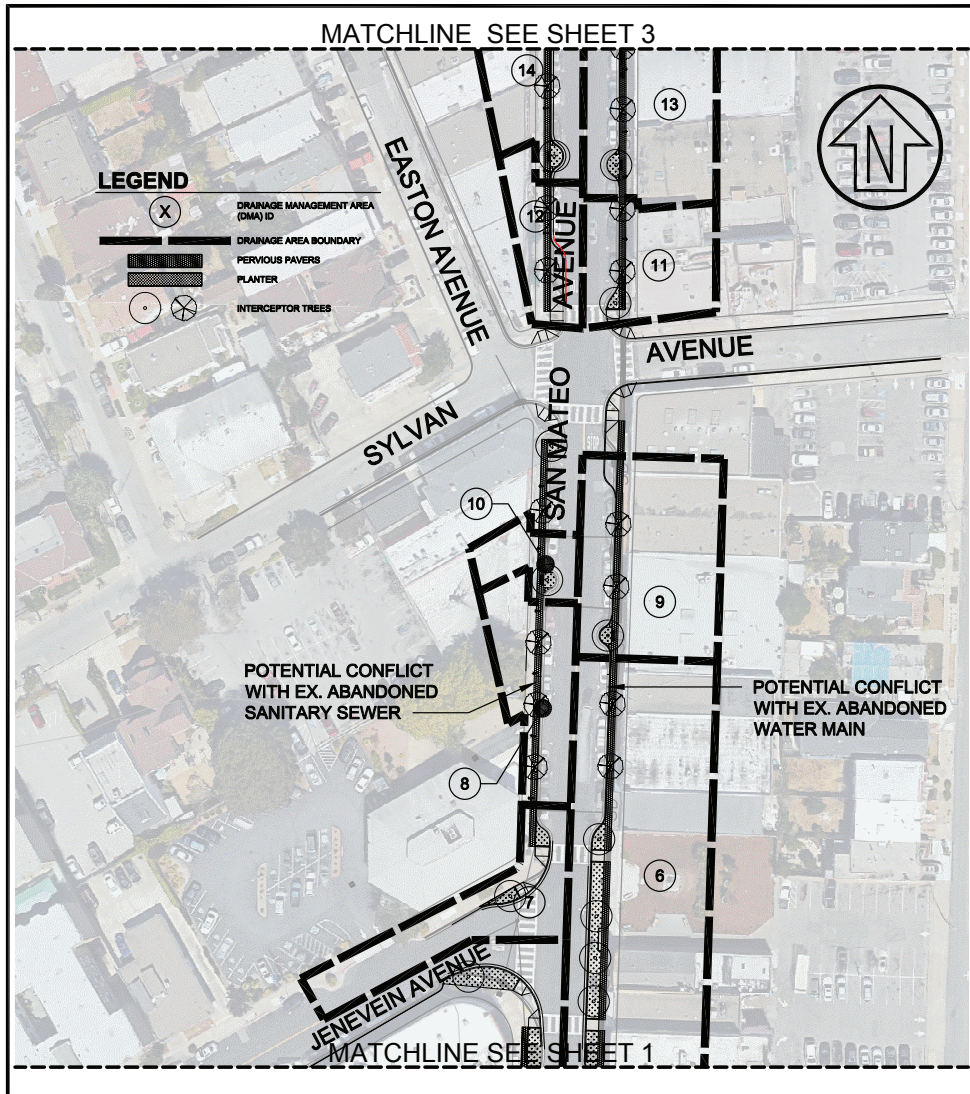
DMA ID	Sub-Watershed Area (sf)	Permeable Pavement Area (sf)	Silva Cell	Interceptor Trees	Percent of Area Treated (%)
1	8999	1952	42	6	100.6
2	17582	2138	88	5	62.2
3	21294	1580	65	6	35.0
4	22966	2810	118	9	58.4
5	5016	460	19	2	43.6
6	24669	1722	72	6	39.5
7	7184	104	5	1	11.0
8	6125	1420	52	3	100.6
9	11750	1750	73	4	66.1
10	5274	326	13	1	28.7
11	6329	650	27	3	46.9
12	4331	628	26	2	65.8
13	10198	1018	42	3	44.5
14	7094	1194	50	3	78.7
15	8457	896	37	3	47.9
16	16978	1806	75	4	46.5
17	9371	480	0	3	12.9
18	16927	1508	63	4	39.1
19	17372	812	0	5	12.6
20	6387	738	30	2	52.9
21	7706	1148	48	3	68.3
22	4500	590	25	2	62.8
23	11173	1220	50	3	48.2
24	11524	1202	50	3	46.2
25	5058	1200	36	3	100.3
26	10204	1260	52	4	56.5
27	6297	1418	46	4	100.3
28	4187	400	17	1	45.1
29	6889	726	30	3	50.4
30	4182	1230	15	3	100.0
31	13552	1488	60	3	47.6
32	1876	500	7	2	100.7
33	2946	516	21	2	87.4
34	1823	302	13	1	87.7

Table 6: Summary of Scenario 4 Results (Management of the San Mateo Ave Corridor)

DMA ID	Sub-Watershed Area (sf)	Permeable Pavement Area (sf)	Silva Cell	Bioretention Area (sf)	Interceptor Trees	Percent of Area Treated (%)
1	8999	1952	0	764	6	100.0
2	17582	2138	0	721	5	100.0
3	21294	1580	65	391	6	63.2
4	22966	2810	81	427	9	100.0
5	5016	460	19	100	2	89.1
6	24669	1722	0	1095	6	100.0
7	7184	104	5	160	1	14.5
8	6125	1420	39	0	3	100.0
9	11750	1750	73	80	4	87.6
10	5274	326	13	80	1	43.3
11	6329	650	27	50	3	58.6
12	4331	628	26	0	2	67.6
13	10198	1018	42	75	3	55.5
14	7094	1194	35	75	3	100.0
15	8457	896	37	75	3	62.8
16	16978	1806	75	75	4	54.2
17	9371	480	0	75	3	13.7
18	16927	1508	63	100	4	46.8
19	17372	812	0	200	5	14.0
20	6387	738	30	100	2	92.6
21	7706	1148	48	75	3	98.2
22	4500	590	18	75	2	100.0
23	11173	1220	50	75	3	59.9
24	11524	1202	50	75	3	56.9
25	5058	1200	0	75	3	100.0
26	10204	1260	52	125	4	86.2
27	6297	1418	0	125	4	100.0
28	4187	400	17	75	1	84.5
29	6889	726	30	125	3	95.2
30	4182	1230	0	75	3	228.4
31	13552	1488	60	75	3	57.5
32	1876	500	0	75	2	100.0
33	2946	516	0	75	2	100.0
34	1823	302	0	75	1	100.0

APPENDIX A





CSW | ST2

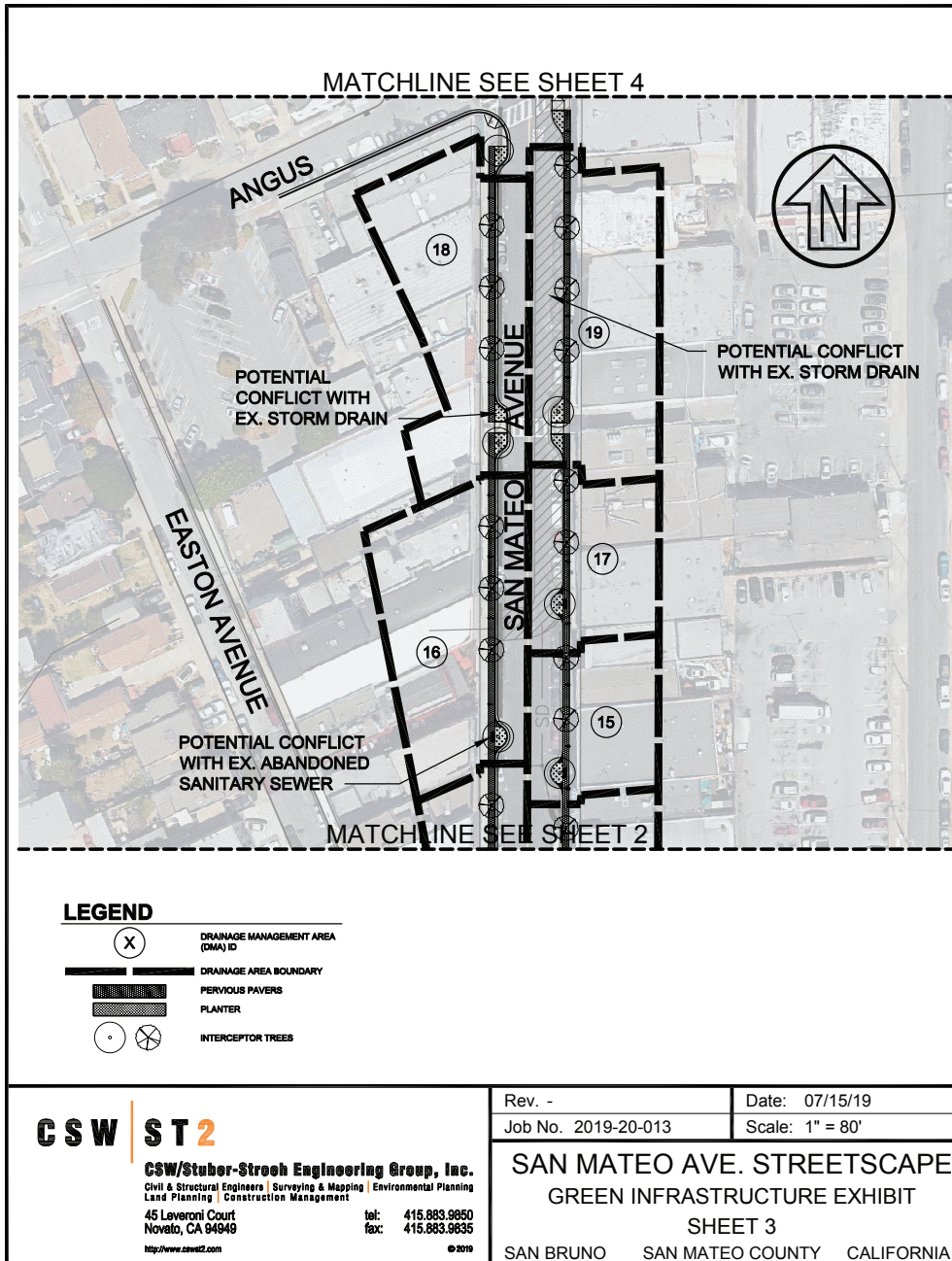
CSW/Stuber-Stroeh Engineering Group, Inc.
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 Land Planning | Construction Management
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 Novato, CA 94949
 tel: 415.883.9850
 fax: 415.883.9835
<http://www.cswst2.com>

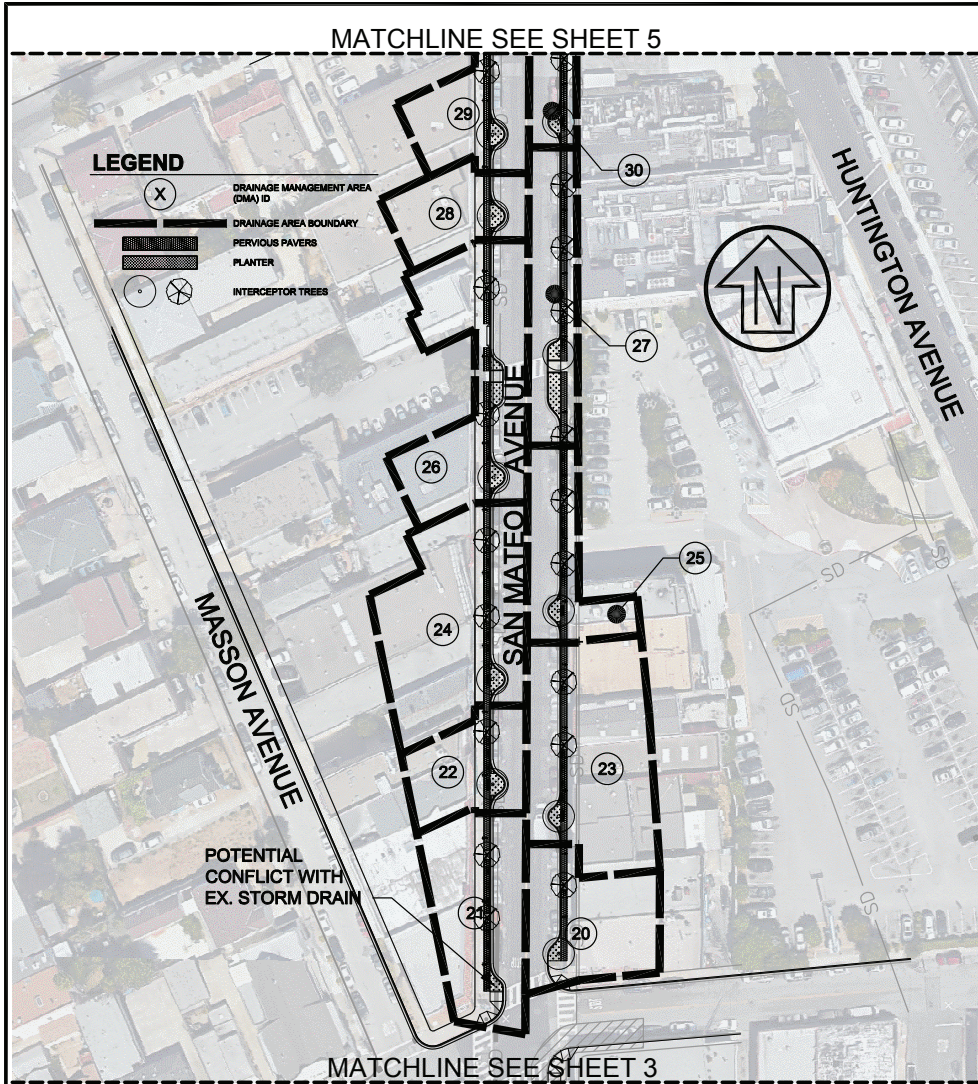
Rev. -	Date: 07/15/19
Job No. 2019-20-013	Scale: 1" = 80'

SAN MATEO AVE. STREETScape
GREEN INFRASTRUCTURE EXHIBIT

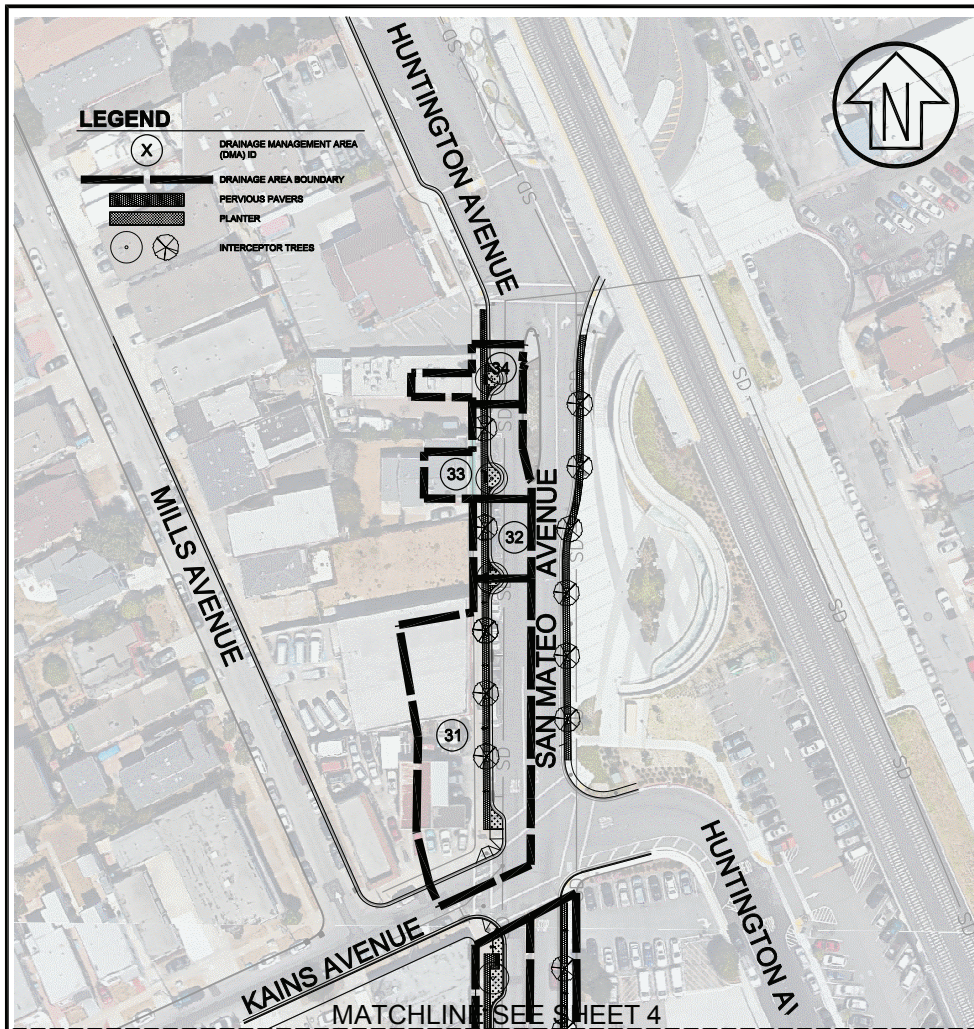
SHEET 2

SAN BRUNO SAN MATEO COUNTY CALIFORNIA





<p>CSW ST2</p> <p>CSW/Stuber-Stroeh Engineering Group, Inc. Civil & Structural Engineers Surveying & Mapping Environmental Planning Land Planning Construction Management</p> <p>45 Leveroni Court tel: 415.883.9850 Novato, CA 94949 fax: 415.883.9835</p> <p>http://www.cswst2.com © 2019</p>	Rev. - Job No. 2019-20-013	Date: 07/15/19 Scale: 1" = 80'
	<p>SAN MATEO AVE. STREETScape GREEN INFRASTRUCTURE EXHIBIT</p> <p>SHEET 4</p> <p>SAN BRUNO SAN MATEO COUNTY CALIFORNIA</p>	



<p>CSW ST 2</p> <p>CSW/Stuber-Stroeh Engineering Group, Inc. Civil & Structural Engineers Surveying & Mapping Environmental Planning Land Planning Construction Management</p> <p>45 Leveroni Court tel: 415.883.9850 Novato, CA 94948 fax: 415.883.9835</p> <p>http://www.cswe2.com © 2019</p>	Rev. -	Date: 07/15/19
	Job No. 2019-20-013	Scale: 1" = 80'
<p>SAN MATEO AVE. STREETSCAPE</p> <p>GREEN INFRASTRUCTURE EXHIBIT</p> <p>SHEET 5</p> <p>SAN BRUNO SAN MATEO COUNTY CALIFORNIA</p>		

E. COST ESTIMATE

CSW | ST 2

PRIORITY 1

ITEM	DESCRIPTION	QTY.	UNIT	UNIT COST	AMOUNT
100	GENERAL CONDITIONS				
101	Mobilization/ Demobilization	1	LS	\$ 338,000.00	\$338,000
102	Water Pollution Control	1	LS	\$ 16,000.00	\$16,000
103	Traffic Control	1	LS	\$ 40,000.00	\$40,000
				Task 100 Subtotal	\$394,000
200	DEMOLITION				
201	Remove Concrete	49,860	SF	\$ 6.00	\$300,000
202	Remove Trees	10	EA	\$ 1,500.00	\$150,000
203	Abandon Utility Removal (Sewer & Water)	2,500	LF	\$ 40.00	\$100,000
				Task 200 Subtotal	\$450,000
300	EARTHWORK				
301	Cut and Export Soil	790	CY	\$ 100.00	\$79,000
				Task 300 Subtotal	\$79,000
400	STREETS AND SIDEWALKS				
401	Accessible Ramp	3,500	SF	\$ 40.00	\$140,000
402	4" PCC Sidewalk (incl. base rock)	38,400	SF	\$ 40.00	\$1,536,000
403	Bus Stop Improvements (Bus Shelters, Bulb-out Extension)	3	EA	\$ 25,500.00	\$76,500
				Task 400 Subtotal	\$1,676,000
500	UTILITIES				
501	Waterline Relocation: Remove Existing Water Line	660	LF	\$ 60.00	\$40,000
502	Waterline Relocation: 8" Water Line (incl. trenching & b	680	LF	\$ 250.00	\$170,000
503	Waterline Relocation: Asphalt Overlay (trench only)	74	TONS	\$ 195.00	\$15,000
504	Street Lights (assumes no upgrade to)	99	EA	\$ 4,000.00	\$396,000
				Task 500 Subtotal	\$621,000
600	IRRIGATION				
601	Irrigation	1,510	SF	\$ 10.00	\$15,000
				Task 600 Subtotal	\$15,000
700	PLANTING				
701	Trees	120	EA	\$ 2,000.00	\$240,000
702	Planting (incl. soil preparation)	1,510	SF	\$ 20.00	\$30,000
				Task 700 Subtotal	\$270,000
900	SITE FURNISHINGS				
901	Benches	28	EA	\$ 1,300.00	\$36,000
902	Bike Racks	62	EA	\$ 500.00	\$31,000
903	Trash and Recycle Receptical Replacement	76	EA	\$ 1,500.00	\$114,000
904	Wayfinding Signage & Business Directories (Allowanc	1	LS	\$ 90,000.00	\$90,000
905	Seatwalls	151	LF	\$ 500.00	\$76,000
				Task 800 Subtotal	\$347,000
				Construction Cost (2019 Dollars):	\$3,852,000
				Contingency (35%):	\$1,348,200
				Total Construction Cost (2019 Dollars):	\$5,200,200
				Design Cost (15% of Total Construction Cost)	\$780,030

Note: All Total Amounts have been rounded to the nearest \$1,000.

PRIORITY 2

ITEM	DESCRIPTION	QTY.	UNIT	UNIT COST	AMOUNT
1000	Priority 2 Improvements				
1001	Suspended Pavement at Tree Wells (Silva Cells)	120	EA	\$ 1,700.00	\$ 204,000.00
1002	Gateway Monuments	1	LS	\$ 750,000.00	\$ 750,000.00
1003	Paseo Gateway Monuments	10	EA	\$ 10,000.00	\$ 100,000.00
1004	Roadway Striping (Thermoplastic)	2,900	LF	\$ 5.00	\$ 15,000.00
1005	Planting (incl. soil prep) & Irrigation at Existing Bulbouts	3,700	SF	\$ 30.00	\$ 111,000.00
1006	Paseo Improvements/Art Installation (Allowance)	1	LS	\$ 400,000	\$ 400,000.00
1007	Alt. Stormwater Management#1 (see breakdown) ²	1	LS	\$338,000	\$ 338,000.00
				Task 1000 Subtotal	\$1,918,000
				Contingency (35%):	\$671,300
				Total Task 1000 Subtotal	\$2,589,300

Notes: 1. Alternative Stormwater Management #1 includes Permeable Pavement in the sidewalk, refer to Breakdown below and Hydrology Assesment Memorandum for further description.
 2. Alternative Stormwater Management #1 unit cost above represents the increase in cost above concrete sidewalk assumed in Priority 1, the breakdown below includes the total cost

PRIORITY 3

ITEM	DESCRIPTION	QTY.	UNIT	UNIT COST	AMOUNT
1100	Priority 3 Improvements				
1101	Centennial Park Improvements	7478	SF	\$ 70.00	\$524,000
1102	Artistic Expression: Accent Lighting (bench)	28	EA	\$ 400.00	\$11,000
1103	Artistic Expression: Accent Lighting (seatwall)	151	LF	\$ 200.00	\$30,000
1104	Alt. Stormwater Management#4 (see breakdown) ²	1	LS	\$1,382,580	\$1,383,000
				Task 1100 Subtotal	\$1,948,000
				Contingency (35%):	\$681,800
				Total Task 1100 Subtotal	\$2,629,800

Notes: 1. Alternative Stormwater Management #4 includes Silva Cells and Bioretention Areas, refer to Breakdown below and Hydrology Assesment Memorandum for further description.
 2. Alternative Stormwater Management #4 unit cost above represents the increase in cost above Alternative Stormwater Management #2, and removes the installation of vehicular

PRIORITY 4

ITEM	DESCRIPTION	QTY.	UNIT	UNIT COST	AMOUNT
1200	Priority 4 Improvements				
1201	Curb Realignment & Planting at Jenevien Ave.				
1201.1	Curb & Gutter/ Pavement Demolition	160	LF	\$ 15.00	\$3,000
1201.2	Curb & Gutter (incl. base rock)	180	LF	\$ 60.00	\$11,000
1201.3	4" PCC Sidewalk (incl. base rock)	1,100	SF	\$ 40.00	\$44,000
1201.4	Planting (incl. soil prep)	700	SF	\$ 20.00	\$14,000
1201.5	Irrigation	700	SF	\$ 10.00	\$7,000
1202	Posey Park Improvements	21272	SF	\$ 70.00	\$1,489,000
1203	Artistic Expression: Crosswalks	7412	SF	\$ 50.00	\$371,000
1204	Artistic Expression: Paving	19372	SF	\$ 50.00	\$969,000
1205	Artistic Expression: Seatwalls	151	LF	\$ 300.00	\$45,000
1206	Artistic Expression: Custom Benches	28	EA	\$ 1,500.00	\$42,000
1207	Alt. Stormwater Management#2 (see breakdown) ¹	1	LS	\$1,136,356	\$1,136,000
				Task 1200 Subtotal	\$4,131,000
				Contingency (35%):	\$1,445,850
				Total Task 1200 Subtotal	\$5,576,850

Notes: 1. Alternative Stormwater Management #2 includes Permeable Vehicular Pavement, refer to Breakdown below and Hydrology Assessment Memorandum for further description.

ALTERNATIVE STORMWATER MANAGEMENT BREAKDOWNS

1007	ALTERNATIVE STORMWATER MANAGEMENT #1				
1007.1	Permeable Pavement (Pedestrian, incl. base rock)	16,900	SF	\$ 60.00	\$1,014,000
				Task 1007 Subtotal	\$1,014,000

1207	ALTERNATIVE STORMWATER MANAGEMENT #2				
1207.1	Asphalt Removal/Demolition	20,400	SF	\$ 2.00	\$40,800
1207.2	Cut and Export Soil	756	CY	\$ 100.00	\$75,556
1207.3	Permeable Pavers (Vehicular, incl. base rock)	20,400	SF	\$ 50.00	\$1,020,000
				Task 1207 Subtotal	\$1,136,356

1104	ALTERNATIVE STORMWATER MANAGEMENT #3 (NOT USED)				
1104.1	Asphalt Removal/Demolition	20,400	SF	\$ 2.00	\$40,800
1104.2	Cut and Export Soil	756	CY	\$ 100.00	\$75,600
1104.3	Permeable Pavers (Vehicular, incl. base rock)	20,400	SF	\$ 75.00	\$1,530,000
1104.4	Silva Cells	1,370	EA	\$ 700.00	\$959,000
				Task 1104 Subtotal	\$2,605,400

1104	ALTERNATIVE STORMWATER MANAGEMENT #4				
1104.1	Asphalt Removal/Demolition	23,700	SF	\$ 2.00	\$47,400
1104.2	Cut and Export Soil	756	CY	\$ 100.00	\$75,600
1104.3	Permeable Pavers (Vehicular, incl. base rock)	20,400	SF	\$ 75.00	\$1,530,000
1104.4	Silva Cells	955	EA	\$ 700.00	\$668,500
1104.5	Demolition of Curb & Gutter	550	LF	\$ 15.00	\$8,250
1104.6	Curb & Gutter (incl. base rock)	550	LF	\$ 60.00	\$33,000
1104.7	Bioretention Soil	511	CY	\$ 90.00	\$46,000
1104.8	Class II Permeable	341	CY	\$ 250.00	\$85,185
1104.9	Storm Drainage (allowance)	1	EA	\$ 25,000.00	\$25,000
				Task 1104 Subtotal	\$2,518,935

F. TRANSPORTATION ASSESSMENT AND RECOMMENDATIONS



Memo

To: Jacob Tobias; WRT Design, Rivka Weinstock; WRT Design
From: Patrick Golier, Jasmine Stitt, Josh Handel; Parisi Transportation Consulting
Date: July 31, 2019
Subject: San Mateo Avenue Streetscape Plan: Transportation Memo

The purpose of this memorandum is to describe design recommendations for transportation on the San Mateo Avenue, located in San Bruno between the intersections of El Camino Real and Huntington Avenue. Specifically, this memorandum presents the recommendations and findings related to the following:

1. Bicycle Parking
2. Centerline Markings
3. Loading Zones
4. Bus Stop Recommendations
5. ADA Parking Recommendations
6. Bike Network Treatment
7. Pedestrian Treatments

BICYCLE PARKING

Summary:

- Provide a bicycle parking rack approximately every 100 feet along the corridor;
 - Each rack can accommodate two bicycles; one on either side of the rack
- Bicycle racks can be placed on the sidewalks and bulb outs

As part of the City of San Bruno's Walk 'n Bike Plan, a survey was conducted to identify the challenges and obstacles to biking in the City of San Bruno. Few or no bike-parking racks was identified as an obstacle by 72% of respondents¹. An open-ended question also asked what specific locations the public would like to see bike-parking racks and one of the most commonly cited locations was Downtown in General and San Mateo Avenue more specifically.

¹ City of San Bruno Walk 'n Bike Plan (2016) Retrieved from <https://www.sanbruno.ca.gov/civicax/filebank/blobload.aspx?blobid=27455>

Please use the following guidance in the siting of bike racks along the corridor:

General Recommendations

- A minimum of 57 bike racks should be sited along the corridor. This number represents an average of one rack roughly every 100 feet on each side of the street. The Federal Highway Administration, National Association of City Transportation Officials, and the Association of Pedestrian and Bicycle Professionals all recommend that bike parking be located no more than 50 feet from building entrances. By placing racks at least every 100 feet, the corridor will conform to this standard. ²
- Single racks accommodating two bicycles (one on either side) dispersed throughout the corridor are preferable to fewer multiple bike rack sites because they allow a bicyclist to park closer to their destination, reducing the distance they need to walk to their destination. Widely distributed bike parking is also preferable for security, as it allows the rider to be closer to their bicycle.
- Bike racks should be placed in locations near the main entrances of businesses, in well-lit spaces in view of a window whenever possible.
- Rack placement should be prioritized in front of businesses that people are likely to bike to, such as convenience stores, coffee shops, restaurants, and gyms. These locations should have at least two racks (accommodating 4 bicycles) within 50 feet of the main entrances.
- Locations that customers are less likely to bike to on a regular basis (such as the gas station and the furniture store) do not need racks immediately in front of them.

Recommended Placement

- When the sidewalk is 10'-14' in width (the majority of the corridor), bike racks should be placed parallel to the street
- Bike racks placed parallel to the street should be sited at least 24" from the curb face, at least 36" from driveways, mailboxes, trash cans, or other street furniture, and at least 72" from fire hydrants.
- Where the sidewalk is at least 14' wide (such as at bulb-out locations), bike racks can be placed perpendicular to the street.

² <https://www.fhwa.dot.gov/publications/research/safety/pedbike/05o85/chapt17.cfm>
<https://nacto.org/publication/transit-street-design-guide/station-stop-elements/stop-elements/bike-parking/>
https://cdn.ymaws.com/www.apbp.org/resource/resmgr/Bicycle_Parking/EssentialsofBikeParking_FINA.pdf

- A bicycle rack placed parallel to the curb must be placed at least 48" from an adjacent rack; 72" is recommended. A bicycle rack placed perpendicular the curb must be placed at least 36" from an adjacent rack – 48" is recommended.
- When sited adjacent to on-street parking spaces, racks should be placed roughly aligned with painted parking tees, if possible, so that parked bicycles do not interfere with the opening of car doors.

In addition to short term bicycle parking, long term parking near the Caltrain station is also recommended. The Caltrain Bike Parking Management Plan found that the utilization for electronic lockers is much higher than keyed lockers and recommends replacing keyed lockers with electronic lockers as funding is identified. Current bike parking at Posey Park has 12 keyed lockers; replacing these with electronic lockers can increase the number of cyclists that can access the Caltrain station.

CENTERLINE MARKINGS

Summary:

- Double yellow centerlines should only be used within 50 feet of each mid-block crosswalk and at 100 feet from the approaches to the intersection;
- Broken yellow centerlines should be used in the mid-block sections;
- The exception to this guidance is on the block of San Mateo Avenue between Sylvan Avenue and El Camino Real, where double yellow centerlines should be maintained given the multiple mid-block crossings and short block lengths along this section of the corridor.

San Mateo Avenue is currently striped with a double yellow centerline which indicates a no passing zone along the entire length of the street in the project area.

Per the California Manual on Uniform Traffic Control Devices (CA MUTCD 2014 Revision 4), centerline markings are required along San Mateo Avenue, consistent with the direction that these markings "shall be placed on all paved urban arterials and collectors that have a traveled way of 20 feet or more in width and an ADT of 6,000 vehicles per day or greater." There are two centerline pattern options presented by the CA MUTCD that are applicable to San Mateo Avenue, including the double yellow (CA MUTCD Figure 3A-101) and the broken yellow centerline (CA MUTCD Figure 3A-104). Caltrans' Standard Plans for centerline striping vary based on facility speed. The lowest speed centerline detail (Dashed Yellow) is Caltrans Detail 1 (no reflector) and Detail 2 (reflector).³

³ 2018 Caltrans Standard Plan 2018 A20A

Section 3B.02 of the CA MUTCD recommends striping a no-passing zone (i.e., double yellow striping) between 100 and 300 feet in length at the approach to an intersection placed in a pattern as shown in Figure 3A-109(CA). No-passing zones should also be established on two-way, two-lane roadways at vertical and horizontal curves and other locations where an engineering study indicates that passing must be prohibited because of inadequate sight distances or other special conditions (CA MUTCD 3B.02.03).

Based on the CA MUTCD it is recommended to restripe San Mateo Avenue as such:

- between Sylvan Avenue and Huntington Avenue,
 - restripe to a broken centerline (Caltrans Detail 1) along the corridor and
 - restripe to a double yellow centerline (Caltrans Detail 21) 50 feet from the approaches to crosswalks and 100 feet at the approach to intersection,
- between Sylvan Avenue and El Camino Real,
 - maintain the existing double yellow centerline.

Because San Mateo Avenue has a straight horizontal alignment and no vertical curves that adversely affect sight distance, provision of a broken yellow centerline within 50 feet of the mid-block crossings is recommended.

Figure 1 shows a summary of the proposed centerline markings.



Figure 1: Centerline Marking Recommendations

LOADING ZONES

Summary:

- Install yellow loading zones approximately every 1,000 feet along the corridor to encourage commercial loading activities by delivery vehicles to take place along the curb rather than in the travel lane;
- Yellow zones should be placed at the beginnings of each block, ideally, to facilitate easier access for these larger vehicles to the curb;
- The exact locations of any new yellow zones should be planned in consultation with the local business community who can help to provide input on balancing the various user demands for the curbside;
- A passenger vehicle white zone should be installed in front of La Petite Baleen Swim School to facilitate safe passenger drop-off at the curb rather than in the travel lane

Yellow zones are designated areas where commercial and passenger loading activities only are permitted (San Bruno Municipal Code 7.16.020). These zones apply between 7am and 6pm on any day except Sunday. While there is no formal guidance to the location and number of loading zones along commercial corridors, it is generally common for commercial corridors to have yellow zones to formalize commercial activities. Designating a specified loading zone could decrease the amount of illegally double-parked delivery trucks. Trucks that are double-parked can restrict sightlines and cause unnecessary congestion.

It is for these reasons that yellow zones are recommended for San Mateo Avenue. One loading zone every 1,000 feet may be sufficient for the amount of deliveries on San Mateo Avenue, which would result in three loading zones along the project corridor. These loading zones should be 40 feet long (2 parking stalls) to ensure adequate space for larger delivery vehicles and to incentivize delivery trucks to pull over to the curb. Locating these zones on the east side of the street, in front of businesses where there are off street parking areas, may help to minimize the impact of parking loss for businesses that are close to other parking. Ideally, they should be located at the beginnings and ends of the blocks to make it easier for trucks to pull into the curb.

The exact locations of any new yellow zones should be determined with the local business community to determine the most effective locations along the street for yellow zones at the expense of general vehicle parking.

Per San Bruno Municipal Code yellow zones are time-restricted (i.e., 7am – 6pm Monday – Saturday), but the hours of the zone should also be considered under consultation with the business community who can help to provide input on balancing the various demands for curbside use along the corridor. Any deviation from Municipal Code would require City Council action.

In addition to yellow loading zones, passenger loading zones (i.e., white zones) are recommended in front of businesses with heavy drop-off activity, particularly after school activities for children. This recommendation is consistent with the Parking Management Recommendations as part of the San Bruno Parking Management Plan. Currently drivers often stop in the middle of the street to let children out directly in front of the business rather than taking the time to find parking further away. It is recommended to restripe two parking stalls as a time-restricted passenger loading zone in front of the La Petite Baleen Swim School during after-school hours. San Bruno Municipal Code sets the hours for white zones at 7am to 6pm Monday – Saturday, but this zone could be restricted for passenger loading between the hours of 1pm to 5pm Monday to Saturday, subject to City Council action, since this represents the times-of-day with the largest number of swim classes.



Figure 2: Time Restricted Passenger Loading Zone in front of La Petite Baleen Swim School

BUS STOP RECOMMENDATIONS

Summary:

- Bus stops should be designed so that passengers with limited mobility can embark and alight buses directly from the sidewalk;
- A 24' x 8' accessible boarding area is required to allow passengers to exit from the rear of a 40' bus;
- A level landing area of at least 5' x 8' should be provided so that a wheelchair lift can be deployed, if necessary. The sidewalks around the bus stops should be made clear of any obstacles (seafing, poles, planters, etc.) where a wheelchair lift would deploy;
- Curbs should be painted red at all bus stops

SamTrans line 141 provides transit service along San Mateo Avenue and uses 29', 35', and 40' buses. Forty-foot buses require a 20' out-taper when located in a parking bay, and 10' in-taper. When stopped, the bus should be located 10' from the crosswalk (at both near side and far side stops). A 24' x 8' accessible boarding area is required to allow passengers to exit from the rear door of a 40' bus.

- **Sylvan Avenue/San Mateo Avenue:** Both the northbound and southbound stops at this location are currently not ADA accessible. They lack a place for a bus to deploy a wheelchair ramp, and passengers exiting from the rear of the bus must exit into the parking lane.
 - At a minimum, the bus must have access to the curb to deploy a curb ramp and allow passengers to exit from the front door. This can be achieved by installing a curb extension in the first parking stall behind each of the bus stops to create a 24' x 8' boarding area. The existing potted plants at these locations may need to be relocated. This would allow passengers the ability to access the curb from both doors of the bus and eliminate the need for a bus to pull out of the traffic flow.
 - Alternatively, ADA-compliant bus stops can be facilitated if three street parking spaces are removed behind each of the bus stops and red zones installed in the parking bay to allow a 40' bus to access the curb. This would be the least expensive, short-term option, though less preferable from a parking supply and transit operations perspective.
 - In the longer term, the southbound bus stop could be a candidate location for a curb extension and placemaking elements. The southbound stop is located immediately in front of a Starbucks, and already has a small amount of outdoor seating. We recommend installing a combination bus stop – parklet here (Figure 3: Combination Bus Stop Parklet, Image from AC Transit Multimodal Corridor Design Guide)
- **Kains Avenue/San Mateo Avenue:** The north/eastbound stop is used to connect passengers to Caltrain and is located on the far-side of the intersection adjacent to Posy Park. The plaza is entirely concrete and provides no shade for users in the hot summer months or shelter in wet weather. The seating available in the plaza is far from the bus stop.

The south/westbound stop is located mid-block, with the flag in front of Ninja Sushi & Tofu (681 San Mateo Avenue).

- **North/Eastbound stop:** Recommended improvements include installation of red curb to better define the bus stop location, and a bus shelter with seating.

- o **South/Westbound stop:** The existing south/westbound bus stop is located far from the intersection, in front of Ninja Sushi & Tofu at 681 San Mateo Avenue. The bus stop should be relocated to the existing bulb-out at the far side of the intersection so that bus passengers can board from the curb and a wheelchair ramp can be deployed.

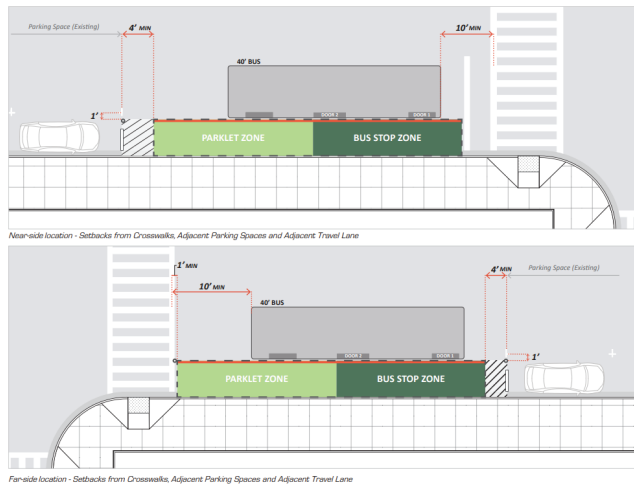


Figure 3: Combination Bus Stop Parklet, Image from AC Transit Multimodal Corridor Design Guide

ACCESSIBLE PARKING

Summary:

- A total of 6 accessible on-street parking stalls, including 1 van-accessible stall, must be provided throughout the project area;
- The exact locations of the accessible parking to be provided will be recommended as part of the City of San Bruno's draft *ADA Transition Plan*, currently under development;
- The implementation of accessible parking along the corridor may necessitate the construction of additional curb ramps that would provide wheelchair users the ability to access the sidewalk from their vehicles without encroaching in the vehicle travel lane

San Mateo Avenue has 162 on-street parking spaces and thus a minimum of 6 total accessible parking spaces must be provided along the corridor. Guidance for accessible parking within

public rights-of-way is generally defined in the "Proposed Guidelines for Pedestrian Facilities in the Public Right-of-Way" (PROWAG), prepared by the United States Access Board and dated July 26, 2011. These requirements are currently under development and have not been adopted by the Department of Justice. Once adopted they will become enforceable under Title II of the Americans with Disabilities Act.

However, in September 2014, the US Court of Appeals for the Ninth Circuit issued an opinion in *Fortyone v. City of Lomita*, which holds that local governments have an obligation under Title II of the Americans with Disabilities Act to provide accessible on-street parking spaces where on-street parking spaces are provided for the ambulatory public even though the Department of Justice has yet to adopt technical design standards for such parking. Given the limited guidance related to implementing accessible parking within the public right-of-way, the use of PROWAG guidance is recommended.

According to Section R214 of PROWAG the number of accessible stalls a project should implement are as follows "Where on-street parking is provided on the block perimeter and the parking is marked or metered, accessible parking spaces complying with R309 shall be provided in accordance with Table R214." On-street accessible parking requirements from PROWAG's Table R214 are detailed in Table 1, below.

Table R214	
Total # of Marked Parking Spaces on the Block Perimeter	Minimum Required # of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 and over	4% of total

Table 1: On-Street Accessible Parking Requirements

PROWAG guidance on the design and locations of accessible parking stalls along San Mateo Avenue that is relevant to San Mateo Avenue is as follows:

- Accessible Spaces (x02.06.1.1). Where on-street public convenience parking is provided in commercial districts and at civic facilities, accessible on-street parking spaces shall be

included in the total provided in the project or project area in accordance with proposed ADAAG Section 208.2 and shall be dispersed within the project area.⁴

- Location – Exceptions (x02.06.1.2) Accessible on-street parking shall be permitted to be combined with off-street parking if equal or greater access is provided in terms of distance from an accessible entrance, user cost and convenience. Since off-street parking in the study area is located behind the storefronts, this threshold, while subjective, would not apply to all store-front entrances.
- General (R309.1). The technical requirements for accessible on-street parking spaces are contained in R309 and adapt the technical requirements for accessible parking spaces in the 2004 ADA and ABA Accessibility Guidelines to the public right-of-way.³
- Narrow Sidewalks (R309.2.1). Where the adjacent sidewalk or available right-of-way is less than or equal to 4.3 meters (14 feet) wide, an access aisle is not required, but accessible parallel parking spaces must be located at the end of the block face. This applies to the length of the San Mateo Avenue corridor.³
- Narrow Sidewalks (R309.2.2). Vehicle lifts or ramps can be deployed on a 2.4 meter (8 foot) sidewalk if there are no obstructions.³
- Curb Ramps and Blended Transitions (Advisory R309.4). Parking spaces at the end of block face can be served by curb ramps or blended transitions at the pedestrian street crossing.³
- Van Parking Spaces (502). For every six or fraction of six parking spaces required by Section 208.2, at least one shall be a van parking space.⁴

⁴ Proposed Right of Way Guidelines from the United States Access Board (PROWAG), x02.6 Vehicular Ways and Facilities

<https://www.access-board.gov/guidelines-and-standards/streets-sidewalks/public-rights-of-way/background/access-advisory-committee-final-report/x02-6-vehicular-ways-and-facilities>

³ Proposed Right of Way Guidelines from the United States Access Board (PROWAG), Chapter R3: Technical Requirements

<https://www.access-board.gov/guidelines-and-standards/streets-sidewalks/public-rights-of-way/proposed-rights-of-way-guidelines/chapter-r3-technical-requirements>

⁴ Proposed Right of Way Guidelines from the United States Access Board (PROWAG), Chapter 5: Parking Spaces

<https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/guide-to-the-ada-standards/chapter-5-parking>

- The dimension requirements for accessible spaces required for conditions along San Mateo Avenue are found in the Proposed Right of Way Guidelines from the United States Access Board (PROWAG, Section 502)⁵:
 - Car parking spaces shall be 8' wide minimum.
 - All of the general (non-accessible) parking spaces along the corridor are recommended to be within the same range of dimensions as they are presently – 19 to 21' long (depending on space available) by 8' wide.
 - Van-accessible spaces are permitted (and recommended) to be the same dimensions as general parking spaces on the corridor. An access aisle is not required due to the sidewalk being less than 14 feet wide. Where possible, van-accessible spaces are recommended to be 21' long.
 - The area of the sidewalk immediately adjacent to the on-street accessible spaces holds the same requirements for clear space as an access aisle – the 8' of sidewalk area adjacent to the on-street accessible space must be free of any obstructions.
- Identification (502.6). Accessible spaces must be identified by signs with the International Symbol of Accessibility. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches minimum above the finish floor or ground surface measured to the bottom of the sign.⁵

Accessible Parking Locations

Based on the total on-street supply of 162 parking spaces, a total of 6 accessible on-street parking spaces should be made available, including 1 van accessible space. This increase in accessible parking stall supply is in addition to the 23 accessible parking stalls located in the adjacent off-street public parking lots.

The exact locations of the accessible parking stalls to be installed along San Mateo Avenue will be determined via the City of San Bruno's draft ADA Transition Plan, currently under development. However, the following criteria can help in the siting of the accessible parking:

- Seek to install approximately 3 spaces on each side of San Mateo Avenue given that the street's retail destinations are located on the east and west sides of the street

⁵ Proposed Right of Way Guidelines from the United States Access Board (PROWAG), Chapter 5: General Site and Building Elements

<https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/ada-standards/chapter-5-general-site-and-building-elements>

⁵ Proposed Right of Way Guidelines from the United States Access Board (PROWAG), Chapter 5: Parking Spaces

<https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/guide-to-the-ada-standards/chapter-5-parking>

- Provide geographical distribution of accessible parking stalls throughout the corridor
- Site the accessible spaces on the far side of the intersections
- Van parking – ensure that 8' of sidewalk area adjacent to the van-accessible parking spaces are free of obstructions for side loading.
- Endeavor to provide 8' of clear area behind the accessible parking spaces for rear loading.

One of the key challenges to siting accessible parking stalls along San Mateo Avenue is the presence of bulb outs along the corridor at each of the intersections and mid-block crossings. The bulb outs, which are built into San Mateo Avenue, prevent passengers in wheelchairs from accessing the sidewalk via a curb ramp without encroaching in the travel lane. Designing and constructing additional curb ramps onto the existing and planned bulb outs that would provide access to passengers in wheelchairs would be challenging given the presence of utility boxes, drainage inlets, fire hydrants and other utility obstructions that would be expensive to move. Additionally, the accessible spaces on San Mateo Avenue may preclude plantings, trees, bike racks and other amenities that are being considered for the sidewalks and bulb outs as part of the San Mateo Avenue Streetscape Plan. It is for these reasons that accessible parking on the side streets are advised, if potential locations can otherwise meet PROWAG guidance.

Examples of curb ramps that have been designed to provide wheelchair users with sidewalk access and that may be applicable to San Mateo Avenue are illustrated in Figures 4-5, below:

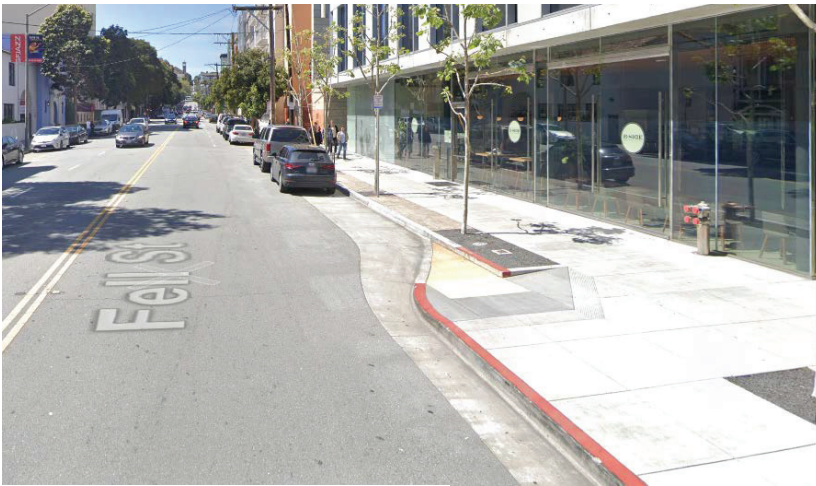


Figure 4: 201 Fell Street, San Francisco



Figure 5: 1498 Bridgeway, Sausalito

Further technical guidance on curb ramp design is provided in Section R304 of the United States Access Board guidance (<https://www.access-board.gov/guidelines-and-standards/streets-sidewalks/public-rights-of-way/proposed-rights-of-way-guidelines/chapter-r3-technical-requirements>).

BIKEWAY NETWORK TREATMENTS

Summary:

- Shared-lane markings (sharrows) should be placed approximately every 200' along the corridor in both directions of travel
- "Bicyclists May Use Full Lane" signs (R4-11) should be installed at each approach to the corridor

The City of San Bruno Walk 'n Bike Plan identifies San Mateo Avenue as a Class III Bike Route. Traffic speeds on the street are slow enough along the roadway so that, in the near-term, changes to this classification are not recommended, and no physical changes are required to improve bicycle safety. However, drivers should be aware that they share the roadway with people bicycling.

CAMUTCD provides the following guidance on the placement of shared-lane markings:

- If used, the Shared Lane Marking should be placed immediately after an intersection and spaced at intervals not greater than 250 feet thereafter (Section 9C.07 section 6)

The National Association of City Transportation Officials (NACTO) *Urban Bikeway Design Guide* provides the following additional guidance:

- Frequent, visible placement of markings is essential. The number of markings along a street should correspond to the difficulty bicyclists experience taking the proper travel path or position. SLMs used to bridge discontinuous bicycle facilities or along busier streets should be placed more frequently (50 to 100 feet) than along low traffic bicycle routes (up to 250 feet or more). SLMs used along low volume routes can be staggered by direction to provide markings closer together.

Given its proximity to both Caltrain and BART and the street's designation as San Bruno's Central Business District, San Mateo Avenue represents a key bicycle route in the city and a lower-stress cycling option to El Camino Real. The corridor contains daily vehicle volumes of approximately 11,000 and has high on-street parking turnover, contributing to potential conflicts between drivers and cyclists. Thus, more frequent placement of sharrows than is provided as part of MUTCD guidance is recommended.

- Shared-lane markings (sharrows) should be placed immediately after each intersection, immediately after the mid-block crosswalks and the bulb outs at the approaches to each intersection with an approximate spacing of 200 feet along the corridor in both the northbound and southbound directions. Shared lane markings assist with bicyclist lane position, reduce sidewalk bicycling, and reinforce the legitimacy of bicycle travel in the roadway. More specific to San Mateo Avenue, shared lane markings help to position cyclists in the travel lane rather than toward the bulb outs at the mid-block and intersection locations.
- At a minimum, two "Bicyclists May Use Full Lane" signs (R4-11) should be installed on the corridor. One should be visible to northbound traffic near the intersection of San Mateo Avenue and El Camino Real, and one should be visible to southbound traffic near the intersection of San Mateo Avenue and Kains Avenue. These locations represent the start of densely spaced retail activity.

PEDESTRIAN TREATMENTS

- Ensure that all crosswalks are striped with a high visibility 'Continental' crosswalk design
- Install in-road pedestrian yield signs at uncontrolled crossing locations that don't currently have them installed

- Install yield limit lines (shark teeth) at the approach to uncontrolled pedestrian crossings
- Install post-mounted diagonal arrow (W16-7P) plaque at the mid block crosswalk locations beneath existing pedestrian crossing (W11-2) warning signs
- Install stop bars at the approach to stop-controlled intersections

Traffic speeds and volumes on San Mateo Avenue are low enough, such that additional pedestrian countermeasure beyond existing and basic treatments aren't necessary. A few basic enhancements are recommended at existing crosswalks such as high-visibility markings, other striping enhancements and in-road pedestrian yield signs.

Much of the corridor already has high visibility crosswalk markings. All crosswalk locations along the corridor should be updated to California Standard Plans' "Continental" crosswalk design. The continental crosswalk markings per Caltrans standard plans (A24F) are not required but are recommended as they provide increased visibility of crossing pedestrians and have been shown to improve yielding behavior.

Artistic and creative crosswalks can be used in combination with the continental crosswalk design but must include the thick white bars to provide contrast with the surround black asphalt and pedestrians, particularly at night.

Much of the corridor already has in-road pedestrian yield signs at uncontrolled crossing locations, except for two locations: the mid-block crossing near Artichoke Joe's, and the north leg of Jenevein Avenue. Install in-street pedestrian crossing signs (R1-6) at the locations where they don't exist.

It is also recommended to install advanced yield lines at the uncontrolled crossings and advanced stop bars at the stop-controlled intersections and signalized intersections. Finally, post-mounted diagonal arrow (W116-7P) plaque at the mid-block crosswalk locations beneath existing pedestrian crossing (W11-2) warning signs should also be installed.

G. ANGLED PARKING EVALUATION

Memo

To: Jacob Tobias; WRT Design, Rivka Weinstock; WRT Design
From: Patrick Golier, Jasmine Stitt; Parisi Transportation Consulting
Date: ~~July 31, 2019~~ September 13, 2019 *
Subject: Angled Parking Analysis for San Mateo Avenue

This memorandum describes the results of a technical study to consider the potential conversion of on-street parallel to angled parking along San Mateo Avenue, from El Camino Real to Huntington Avenue as part of the San Mateo Avenue Streetscape Plan.

SUMMARY

Parisi Transportation Consulting conducted an analysis regarding the feasibility of converting on-street parking along the corridor from parallel to a 60- and 45-degree configuration. The result of the analysis indicates that the conversion is not recommended due to the resulting narrow roadway width. More specifically, the implications of the reduced roadway width include an inability to:

- Meet San Mateo County Fire Code;
- Meet City of San Bruno Municipal Code Section 12.100.080 *Design Standards for Parking Facilities*;
- Maintain two travel lanes along the corridor; and
- Maintain on-street loading activity as a stopped vehicle or truck would block other vehicles

SIXTY-DEGREE PARKING

Parisi considered the installation of sixty-degree parking on one side of San Mateo Avenue from Kains Avenue to El Camino Real. The road width of San Mateo Avenue is 43-feet. The implications of this parking configuration are as follows:

- The width of the unobstructed roadway width would be 14-feet assuming an 8-foot-wide parallel parking configuration on the opposite side of the street, from approximately 27-feet under existing conditions, which would provide an insufficient drive aisle width to support parallel parking on the opposite side of the street. A 14-foot drive aisle width

Angled Parking Analysis for San Mateo Avenue

does not provide an adequate amount of maneuvering room for vehicles to back out of an angled parking space and does not meet San Bruno's parking code requirements as depicted in Municipal Code Section 12.100.080.

- The San Mateo County Fire Code states that "fire apparatus access shall have an unobstructed road width of at least 20 feet and vertical clearance of 13 feet 6 inches...Fire apparatus shall not be obstructed in any manner including vehicle parking or vegetation intrusion." Under a 60-degree angled parking design, San Mateo Avenue would provide 14-foot unobstructed road width. In residential areas with low parking use this may be less of an issue for the Fire Department. However, since the parking on this street is well-used and the business uses include restaurants which could carry a higher risk of fire, parking conversion is unlikely to be approved by the San Bruno Fire Department;
- To meet fire clear-width requirements, parallel parking and bulb outs on the opposite side of the street from the angled parking would need to be removed. This would result in a total of 22-feet of roadway width. In addition, while this road width would allow for sufficient drive aisle width to facilitate angled parking on one side of the street, two-way traffic would be precluded for an insufficient drive aisle width for two-way circulation. A minimum of 24-feet of drive aisle width for two-way traffic is recommended to allow drivers backing out of angled parking spaces sufficient maneuvering room without conflicting with moving traffic in the opposite direction;
- Circulation on the street would be required to change from two-way to one-way given the narrow drive aisle width. This would also require the rerouting of the SamTrans 141 line and bicycle circulation in one direction. A circulation scheme through the neighborhood would need to be designed that provides intuitive vehicular and bicycle routing for traffic in the opposite direction of the one-way travel along San Mateo Avenue. This would also result in an increase in traffic volumes on adjacent residential streets;
- Removal of parking on the opposite side of the street would result in a total parking supply of 151 spaces. If the existing bulb outs from the side of the street with the angled parking were also removed in addition to the removal of the parallel parking, total on-street parking supply would be approximately 162.
- The City of San Bruno's Municipal Code Section 12.100.080 requires drive aisle widths for on-street angled parking to be 28 feet and stall depth to be 19.8 feet. The code, as currently written, would preclude the ability to fit 60-degree angle parking on San Mateo Avenue, even with the removal of on-street parking from the opposite side of the street, removal of all bulb outs, and the conversion of the roadway from two-way to one-way.

FORTY-FIVE DEGREE PARKING

Parisi also assessed the implications of a forty-five-degree parking scheme on San Mateo Avenue:

- The width of the remaining travel lanes would be 16-feet, from approximately 27-feet under existing conditions, assuming an 8-foot-wide parallel parking on the opposite side of the street;
- Circulation would be required to change from two-way to one-way;
- Parallel parking would need to be removed from one side of the street to accommodate the angled parking;
- The width of the one-way vehicle travel lane along the roadway would be 24-feet, which would not meet on-street requirements which require an aisle width between stall lines of 28 feet; and
- Total on-street parking supply after the removal of parallel parking from the opposite side of the street would decrease to approximately 115 along the corridor. If the existing bulb outs on the same side of the street as the angled parking were also removed in addition to the removal of the parallel parking, total on-street parking supply would decrease to 126.
- The City of San Bruno's parking codes for on-street parking require drive aisle widths to be 28 feet and stall depth to be 18.7 feet would preclude the ability to fit 45-degree angle parking on San Mateo Avenue.

DRIVE AISLE WIDTH

ON-STREET REQUIREMENTS

Drive aisles allow for vehicle circulation and provide sufficient area for motorists to back out of parking stalls. The City of San Bruno requires an additional 12-feet to the drive aisle width for 60-degree and an additional 16-feet for 45-degree parking from their off-street drive aisle regulations. The results of this drive aisle width requirements are detailed below in Figure 1 from the San Bruno Municipal Code Section 12.100.080 and Table 1, below. These drive aisle requirements would preclude the ability to fit any angled parking stalls on San Mateo Avenue, which has a curb to curb width of 43-feet.

Angled Parking Analysis for San Mateo Avenue

FIGURE 1
PARKING DIMENSION TABLE

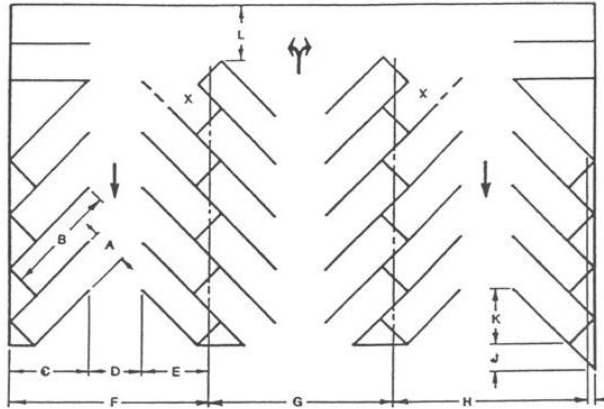


Table 1: San Mateo's Standards for Aisle Width for On-Street and Off-Street Parking Stalls

Dimension	Diagram	45 Degree		60 Degree	
		Off-Street	On-Street	Off-Street	On-Street
Stall Depth	C	18.70	18.70	19.80	19.80
Aisle Width Between Stall Lines	D	12.00	28.00	16.00	28.00
For on-street parking use the above dimensions, adding 16 feet to dimension "D" for 45-degree parking and 12 feet for 60-degree parking					

* Memo has been updated on September 13, 2019 to reflect corrected parking supply numbers.

H. NOTES FROM COMMUNITY ENGAGEMENT AND STAKEHOLDER MEETINGS

Public Engagement Summary

March 12, 2019 - 'Walk'shop – a walking tour, Community Workshop #1
March 28, 2019 - Stakeholder Meeting #1
March 30, 2019 - Drop-in Community Workshop #2
March 11 - April 10, 2019 – Survey. We received 92 responses during this time.
May 7, 2019 - Planning Commission Meeting #1
May 8, 2019 - Stakeholder meeting #2
May 15, 2019 - Parks and Recreation Meeting
May 16, 2019- Arts Commission Meeting
May 22, 2019 - Community Meeting #3
July 16, 2019 - Planning Commission Meeting #2

Summary of Comments from All Community Engagement Meetings:

General Aesthetics:

- The spirit and texture of the design, including the transportation theme and the light, playful aesthetic, feels authentic and fitting for the site (Planning Commission #1).
- Agree with simplified design, with key unique features (Stakeholder #2).
- Ensure that the aesthetic intent is elegant and not 'fancy' or Victorian (Stakeholder #2).
- Put something on the avenue so that everybody likes to come! (CW #2: Bulb out board)
- Now desolate, dreary (CW #1)
- The charm is missing here - look at Broadway in Burlingame – should be a place to linger. (CW #1)
- Old san Bruno, cool, diverse, should encourage people to be here. (CW #1)
- Eclectic/Interesting/Artistic (CW #1)
- Family/community/diversity/yum/charm/potential/revitalized/we have roots/city with a heart. (CW #1)
- San Mateo Avenue is the heart of our community/Inclusive. (CW #1)
- "the heart of san Bruno is the people" (CW #2 Prioritization Exercise)
- Small town charm (CW #2 Character Concepts Board)
- Artistic/Unique, if done well (CW #2 Character Concepts Board)
- To red bench - artistic, but maybe uncomfortable (CW #2 Character Concepts Board)
- Keep it simple – not too cluttered! (CW #2 Character Concepts Board)
- Keep it approachable (CW #2 Character Concepts Board)
- Like modern, but buildings aren't modern – prefer simple (CW #2 Character Concepts Board)
- Airport elements, or general transportation (railroad history then airport etc) (SM #1)

- Historic figures, murals of people who had a part in building San Bruno (SM #1)
- Artistic and unique (SM #1)
- Should feel more modern (SM #1)
- Classic Modern/Contemporary (SM #1)
- Charm, highlight multi-cultural (SM #1)
- Working Class (SM #1)

Furnishing and Paving:

- Choose a different trash can without the grass theme (Planning Commission #1 and Community Meeting #3).
- Ensure trash can has a cover plate (Stakeholder Meeting #2).
- Trash can looks small; ensure it works with the requirements of Recology (i.e. large enough, etc.) (Arts Commission).
- Look into solar compactor trash cans (Arts Commission).
- Custom curved benches aren't working as shown due to tripping hazard and lack of seating. They should be reconceptualized (Planning Commission #1).
- Paving design could include concrete with unit paver banding for the tree amenity strip (Stakeholder Meeting #2).
- In streetscape plan, could include both unit pavers and concrete, and indicate positives and negatives (Stakeholder Meeting #2).
- Suggestion that existing sidewalks and pavers could be painted to provide visual continuity at a lower cost than replacing all pavements (Community Meeting #3).
- Wheelchair ramps (CW #2: Table runner map)
- Raised crosswalks, especially mid-block (CW #2: Greening Opportunities board)
- Wheelchair access- no pedestal tables (CW #2: Activation Opportunities board)
- Need ADA access to businesses. (CW #1)
- Outdoor seating should generally be near the building not on the bulb out, though it may work in certain places. (CW #1)
- It's too cold to eat outdoors, too windy, maybe it would work in the building recesses. (CW #1)
- Outside seating is great, even if it's cold – can have heaters. (CW #1)
- People do sit and stop along the avenue where they can. (CW #1)
- The existing seating element/benches are ugly and not maintained well – the vegetation is overgrown, so it's hard to sit there. (CW #1)
- Provide benches for storefronts – provide seating. (CW #1)
- New lights, trash cans, bins – recycle-compost. (CW #1)
- Water bottle filler, water fountain, bike racks, fountains for dogs, signage, trash bags. (CW #1)
- Keep newspaper racks. (CW #1)
- More outside seating!! (CW #2: Activation Opportunities board)
- No wood, more concrete to avoid vandalism (CW #2: Activation Opportunities board)
- Ledge seating in the angular building setbacks (CW #2: Activation Opportunities board)

- Need more seating for elderly - to take breaks walking from one end to the other (CW #2 Prioritization Exercise)
- Welch family/Marshall family - benches named/paid for by families of former city leaders (CW #2 Prioritization Exercise)
- Need a better paving design than the current one, because now, if a spot on the distinctive strip breaks, they fix it with regular concrete and it looks ugly. (CW #1)
- Uneven, patchy sidewalk. Need a complete replacement. (CW #1)

Planting, Green Infrastructure and Drainage:

- Excited about the planted bulb outs and trees (Planning Commission #1).
- Place trees so that they don't interfere with opening car doors (Stakeholder Meeting #2).
- Look at drainage issues and bulb outs (Stakeholder Meeting #2).
- Don't propose flow-through planters because they collect trash (Stakeholder Meeting #2).
- Look at raised crosswalks and drainage (Stakeholder Meeting #2).
- Greening the bulb outs is a great idea (Arts Commission).
- Ensure that the right tree species is recommended to be successful on this corridor (Arts Commission).
- Concerned about utility conflict with trees (Community Meeting #3).
- Get rid of the pots – they're ugly! Also, drivers can't see people crossing the street behind the pots – bad for visibility. (CW #1)
- The street should be lined with trees! There used to be trees here but they weren't doing well, so they removed them. (CW #1)
- Make sure the tree species work well for visibility. (CW #1)
- More trees! (CW #1)
- Green infrastructure. yes! (CW #1)
- Trees. Yes! (CW #1)
- Ceremonty/target center – they did a nice job with the landscaping. (CW #1)
- Anywhere where there is too much street, we should reclaim. (CW #1)
- Need to plant the right species so that they don't pull out the sidewalk. (SM #1)
- More trees!! (CW #2: Table runner map)
- You could add some flowers, trees, etc. (CW #2: Table runner map)
- Plant trees that are native – the existing list is not native (CW #2: Table runner map)
- Like option #3 (removing bulb outs) (CW #2: Greening Opportunities board)
- Trees block our wheelchair van ramp – access needed - our ramp comes out on the right side of our van (CW #2: Greening Opportunities board)
- Big trees, wind tolerant – see San Carlos Ave in San Carlos (CW #2: Greening Opportunities board)
- Bring back the trees (CW #2: Greening Opportunities board)
- Need a wind break (CW #2 Prioritization Exercise)
- Please no bulb-outs between Kains ave and Caltrain station! – bicycle hazard. (CW #2: Greening Opportunities board)
- Could have parklet – an occupiable space instead of a planter (CW #1)

- Stormwater treatment will be important for future (CW #2: Bulb out board)
- Like green and bench, concerned about constraining sidewalk width (CW #2: Bulb out board)
- Like balance of planting and seating (CW #2: Bulb out board)

Lighting:

- Should have fluidity of lighting along the corridor: lighting should go along the street, or even over the street, if possible (Planning Commission #1).
- It's dark at night; need to do a photometric analysis. Attractive, nighttime lighting should be incorporated along corridor, paseos and side streets (Stakeholder Meeting #2 and Arts Commission).
- In-ground lighting will be difficult for maintenance (Stakeholder Meeting #2).
- Should have consistency in the light poles (Stakeholder Meeting #2).
- It's dark at night, need to lighten up the street, neon at night. (CW #1)
- Need lighting at night. (CW #1)
- Current street lights are ugly, historic looking, don't give a lot of light. (CW #1)
- Lights hanging from one pole to another (CW #2 Prioritization Exercise)

Bicycle infrastructure:

- Biking is important and in general plan; ensure bike infrastructure opportunities (Planning Commission #1).
- Sharrows would be good here (Stakeholder Meeting #2).

Centennial Plaza:

- Centennial Plaza is important to develop. (Planning Commission #1).
- It would be nice to include quiet, shaded seating opportunities (Planning Commission #1)
- Instead of a stage, have an open flex space with power access for different types of events (Stakeholder Meeting #2).
- Do not need to keep fountain (Stakeholder Meeting #2).
- Ideas for the plaza could include: children's play area, bocce ball, a pergola with vines, art sales, artistic play structure, a usable urban space, pop-up amenities (Arts Commission).
- Existing art is temporary; remove and save the art (Arts Commission).
- It's useless now because it's gated off. People do sit on those two benches though. Should open it up so that you can sit everywhere – (there is supposedly a contamination issue). (CW #1)
- Bioswale – could get funding for this, for beautification. (CW #1)
- Want to use this space, is a nice wind shelter. (CW #1)
- It's closed off because the center is just loose soil on old basement – not structurally sound. (CW #1)
- Pergola for shade (CW #2: Table runner map)
- Should be a community garden!! (CW #2: Table runner map)

- Improve the park – should be inviting, something for kids, it does get windy (CW #2: Table runner map)
- Simple stage for music opportunities (CW #2: Table runner map)
- ‘the non-park park’ (SM #1)
- Art there doesn’t fit with the character of the rest of the street. (SM #1)
- Could be an event space – look at Downtown Half Moon Bay (well lit, stage, benches, restaurants – hold concerts there) (SM #1)

Posy Park:

- A Posy Park redesign should ensure visibility and include trees (Planning Commission #1).
- Could have a farmer’s market here (Planning Commission #1).
- Ensure tensile structures can stand up to wind (Planning Commission #1).
- Ensure visibility and safety. Perhaps reorient the benches so that they are perpendicular to the street. Perhaps include a blue emergency beacon phone (Stakeholder Meeting #2).
- Can include café seating and a coffee cart (Stakeholder Meeting #2).
- Cigarette butts an issue here; install a cigarette receptacle (Arts Commission).
- Seating should not encourage homeless (Arts Commission).
- Install lighting at the mosaics (Stakeholder Meeting #2).
- Do not install lighting at the mosaics; they are fading (Arts Commission).
- Tent structures are good, as they do not take away from artwork, if they can be kept clean and will stand up to the wind (Art Commission).
- Like the idea of a shade shelter at Posy Park (Community Meeting #3).
- Would like to see signage or other means of identifying Posy Park – most people don’t know that name (Community Meeting #3).
- Will benches be utilized? Moveable tables and chairs could be an option (Parks and Recreation Commission).
- Make a place for trucks to park. Can do Off the Grid food trucks; useful for riders and the community (Parks and Recreation Commission).
- Add an area for skateboarding (Parks and Recreation Commission).
- Posy Park is underutilized; looks like a plaza (Parks and Recreation Commission).
- Park could have wind issues (Parks and Recreation Commission).
- Make it more friendly, add more grass. It is currently very cold (Parks and Recreation Commission).
- Dog park is not recommended (Parks Commission).
- In favor of landscape instead of the fountain. (CW #1)
- The white walls are boring. (CW #1)
- Too much concrete, ugly (SM #1)
- Redevelopment happening where Huntington meets San Bruno across from Posy Park (American Legion and Huntington Liquor Grocery) – going to be residential units with café and convenience store at ground floor. (SM #1)
- Too much concrete, more green in this park (CW #2: Table runner map)

Paseos:

- Paseos could be a place – murals, lighting, tables and chairs, chessboards, heaters, bocce ball, (plaza at Citibank also a good place for bocce ball). (CW #1)

- Is less windy here, so many better for seating? (CW #1)
- Stores usually have back entrances. (CW #1)
- Should be parking signage, make people aware, lighting, inviting, parking management. (CW #1)
- Could add plants, green walls, decorative light poles, planters, baskets, murals. (CW #1)

Wayfinding and Branding:

- The gateways need to stand up to the wind (Planning Commission #1).
- Could have signage that shows the medallions of all the organizations at Caltrain overpass (Stakeholder #2).
- Should have a marquee somewhere that shows events (the current one on El Camino Real is outdated, some thought it was charming, but the content is good) (Arts Commission).
- Concerned about 'The Avenue' branding. It speaks to the street, but not to the culture. Also concerned about redundancy of that name with Burlingame (Community Meeting #3, Arts Commission and Parks and Recreation Commission).
- A heart sculpture, creative gateway (not in a hokey way) (CW #1)
- Need a noted entrance on both ends; doesn't have to be an arch. Could use the walls (where the arches were supposed to be) for something. (CW #1)
- Maybe have store directories in the paseos. (CW #1)
- The current situation at El Camino real could be called 'the electric transformer gateway' because of all of the electric transformers – ugly, should remove. (CW #1)
- Need Wayfinding, not gateway (CW #2 Prioritization Exercise)
- Add one sign at Hazel Ave and Crystal Springs Ave, at City Park (CW #2 Wayfinding and Gateway Opportunities)
- Add one sign at Jenevein Ave and San Mateo Ave (CW #2 Wayfinding and Gateway Opportunities)

Art:

- Paseos are a great opportunity for art (Stakeholder Meeting #2 and Arts Commission)
- Could have an art competition: different artists can design different paseos. (Stakeholder Meeting #2).
- Art commission can help support this initiative (Stakeholder Meeting #2).
- Ensure murals are not 'bad art' (Palo Alto Murals are great) (Stakeholder Meeting #2).
- City should coordinate with property owners to see what can be done in Paseos (Stakeholder Meeting #2).
- Proposed artwork strategy is successful: having artwork in the paseos, posy and centennial park, and not along the street. (Arts Commission)
- Painted crosswalks are a good idea (Arts Commission).
- Don't like the aesthetics of the piano (Arts Commission).
- Could have murals on the storm drains, like City of Thornton (Arts Commission).

- Not enough art. (CW #1)
- The art that's out here is very old fashioned – the fountain and murals - it dates our downtown, but I know that some people do really like it. (CW #1)

Programming:

- Include street vendors, art shows, first Fridays, family friendly downtown, scavenger hunts (like San Jose downtown doors) (Planning Commission #1).
- Bring artwork along the street, children's competitions, farmer's markets (Stakeholder Meeting #2).
- closing off streets from vehicular traffic once a month.
- Encouraging activation by young people would be a good goal (Community Meeting #3).
- Should attract businesses and address these (Parks and Recreation Commission).
- Dog friendly downtown (CW #2: Activation Opportunities board)
- Dog friendly downtown! (CW #2: Activation Opportunities board)
- Dog potty stations w/ bags (CW #2: Activation Opportunities board)
- Could do 'Sunday Streets' where you close off the corridor for pedestrians only. (CW #1)
- Vibo Music (music store that also has performances) could have their performances outdoors. (CW #1)
- Could fundraise for the street by having sidewalk pavers of trees named for community members. (CW #1)
- There was a farmer's market here but it wasn't successful (wasn't run well), but people were excited about it. (CW #1)
- Farmer's Market (CW #2: Existing Corridor Character)
- Farmer's market: use parks and plaza spaces instead of closing the street (CW #2: Activation Opportunities board)
- Weekend events (CW #2: Activation Opportunities board)
- Music (CW #2 Prioritization Exercise)

Citibank Plaza:

- Citibank plaza is another open space opportunity (Planning Commission #1).

Parking and Loading:

- Should be sensitive to parking, and ensure that the design does not reduce parking (Stakeholder Meeting #2, Community Meeting #3). (WRT has responded that SamTrans bus accessibility requirements will remove 3 parking spots).
- City should coordinate with individual business owners to assess whether white and yellow striped zones are necessary and where (Stakeholder Meeting #2).
- Children from the swim school walk from the rear street – people park at Artichoke Joe's lot (Parks and Recreation Commission).
- There is no parking (Parks and Recreation Commission).
- More blue zones for handicapped (CW #2: Table runner map)

- Public parking space (CW #2: Table runner map)
- Add school-zone type signage near swim school (CW #2: Greening Opportunities board)
- Loading zone at swim school (CW #2: Greening Opportunities board)
- Everyone wants slower traffic, less cars. There's a lot of double parking, loading, creates traffic. (CW #1)
- The planters took parking away. (CW #1)
- Agree with diagonal parking, but don't want to lose sidewalk width – maybe look into making it a one way street? (CW #1)

Implementation and Maintenance:

- A piecemeal approach may work well, as opposed to trying to do all of the improvements in one go. Perhaps include a suggestion that landowners will have to pay into this, in addition to other sources. It will incentivize movement and responsibility from the landowners (Stakeholder #2).
- Need to consider maintenance costs and needs (Community Meeting #3).
- What is the status of the budget? (Community Meeting #3).
- Street improvements should be prioritized over Posy Park (Parks Commission).
- Who will maintain the landscape? (CW #2: Bulb out board)
- Who will maintain/take responsibility for the trees? (SM #1)
- Who will clean garbage? (CW #2: Bulb out board)
- Clean up the floors, trash cans, etc. (CW #2 Wayfinding and Gateway Opportunities)
- Maintenance is the most important element. (SM #1)
- Fewer, simpler elements that are well maintained is better than (SM #1)
- The city needs to maintain any planting that goes in. (The existing planters are being abused and not taken care of.) (SM #1)
- Maybe business owners should look into making a BID or Downtown Business association. (SM #1)
- Angus Avenue's sidewalk is still broken. (SM #1)
- The property owners here are required to fix the sidewalk. (SM #1)
- Once this streetscape is designed, the onus is on the city to really on the city to enforce it. (SM #1)
- If they're going to repave, think about maintenance – cost of repaving and how to make sure it looks nice when they have to repave for utility replacements etc. (SM #1)
- Need to wash the sidewalk. (CW #1)
- City should confirm what is most reasonable budget for building + maintenance (SM #1)
- The 'parking lieu fee' that comes from the 400 units on the corner of San Mateo Avenue and El Camino Real - where does that money go? Can it go into the street redesign? (SM #1)

Outside scope of this project:

- Absentee owners are a problem here. They are not maintaining them, and they are keeping storefronts vacant (Planning Commission #1).
- Ensure that something is in the building setbacks so that they are not used for encampment/trash (Stakeholder #2).
- Enforce trash clean up in front of stores, and/or provide trash enclosures (Stakeholder #2).
- Parking is still a major issue. If Caltrain builds a parking lot, it will relieve a lot of the parking issues (Stakeholder #2).
- Just by painting the buildings, could really help upgrade the downtown (like in Los Altos and Los Gatos, we can dictate the color of the buildings) (Stakeholder Meeting #2).
- Car Repair shop across from Posy Park is an eyesore (Community Meeting #3).
- Need to enforce ordinances to ensure that businesses keep up with maintenance (Community Meeting #3).
- Parking program for business owners (CW #2: Table runner map)
- Build a parking structure - then would support removing street parking for added green (CW #2: Greening Opportunities board)
- On Angus Avenue, Mason, and San Mateo, after 6 PM, cars are allowed to park on Mason (and Angus?), and it creates tons of traffic jams on corner of Angus and San Mateo. (CW #1)
- Caltrain parking costs money, but streets are free, so it backs up. (CW #1)
- Police Substation (CW #2: Table runner map)
- Police on bike 2x a day would make a big difference (CW #2: Table runner map)
- Less littering (CW #2: Table runner map)
- We could use a wider range from low cost to upscale restaurants (CW #1)
- Should be restaurants/entertainment here. Would be great to have more art/cultural uses/amenities, more social and community oriented places to gather, seems to be working well here. (CW #1)
- NW corner of Kains Ave + San Mateo should be a café, not an auto shop. (CW #1)
- Vacancies – one on nears Kains Ave, lease is too high. Maybe the city needs to do something with the vacant space while it's vacant? (CW #1)
- Is signage on buildings a part of this scope? In Bay Hill they have a standard and it looks good. Question about allowed building height on the street – 3-4 story buildings are allowed. (CW #1)
- The existing architecture is all cut up – it should feel continuous. (CW #1)
- Lack of transparency to the street – need a toolkit. Guide for businesses to have a good retail environment. (CW #1)
- There are a lot of businesses that remain closed/ closed blinds - is there zoning/code enforcement that can be done? (SM #1)
- More Colorful buildings – the buildings look dull (CW #2: Table runner map)
- Clean up store windows that are full of junk (CW #2: Table runner map)
- Paint buildings – pick out 3-4 tasteful colors (CW #2: Table runner map)
- Businesses should open their windows (CW #2: Existing Corridor Character)
- More poke stop! (CW #2: Existing Corridor Character)

Dots exercise from 3/30.19 Community Workshop**Existing Corridor Character**

Dot on rotary clock, newell's bar, starbucks sidewalk seating, and bunch of dots on 'more poke' and 'farmer's market' (both write ins)

Greening Opportunities

Most dots on the third tree placement option (remove bulb-outs), dots on the precedent image of the tree lines corridor

Bulb Out Opportunities

Most dots went to the bulb-out option that balances planting and seating/hardscape amenities

Option 3: (Combined planting + seating) – 13

Option 2: (Maximize hardscape) - 8

Option 1: (Maximize planting) - 7

Small mid-block bulb-out concept - 7

Activation Opportunities

Outdoor Café seating: People most liked the wood slat aesthetic (fig and sparrow), but were generally in favor of outdoor café seating

Paseo activation: People most liked the arbor/planting idea and the art/mural idea, but were generally in favor of activated paseos

Night Lighting: people seemed to like all of the ideas – but most dots went to the lighted spheres

Parklets: people most liked a parklet that incorporates planting

Character Concepts:

Most dots were in the "artistic, unique" category, but there was also an emphasis on simple and uncluttered design and people put dots on a bunch of the "simple, modern" images. In the "traditional" category: The image of Burlingame's paving got many dots as well.

Wayfinding and Gateway:

Gateway: Most dots were placed on pleasanthill's over the street gateway – one person noted that they liked the fact that it was over the street.

Wayfinding: Most dots on the Des Moines Iowa downtown wayfinding signage - people liked simple clear graphics, with some people liking the more craft/artistic like signage like Castro valley. One person wrote, that they need wayfinding, not a gateway

Should have gateway on Kains leading to downtown, (ppl would be coming from El Camino and Huntington) Kains is the main road from the library and city hall (Survey)

Prioritization exercise

31 trees and planting

27 pedestrian safety features

22 seating/outdoor dining

21 wayfinding and gateway markers

21 art

17 Special paving and custom elements

15 shade and comfort

7 stormwater management

5 bike parking

Survey: results as of 4/2/19 – 64 respondents

1. What brings you to San Mateo Avenue? (Please check all that apply)

Most respondents Live in the area, and eat and drink there. Second most do shopping/errands and drive on it to go elsewhere.

47 – **eat and drink**

40 - **live in the area**

26 – **shopping/errands**

25 – **drive on it to go elsewhere**

12 – strolling and exercise

10- take Caltrain

3 – other

3 – I own a business on san mateo ave

3- I work on san mateo ave

2. How often do you spend time on San Mateo Avenue?

Most respondents are there about once a week, second most are there once a month. Third, every day.

3. Which category below includes your age?

80 % of respondents are either 30-50 or 50-70.

4. What do you feel would be the most important to improve for the future of San Mateo Avenue? Please rank each of the following elements in the order of importance from 1 to 6. (1 is the most important and 6 is the least important):

Successful businesses ranked highest, then safety and beauty/character/identity.

4.81 – successful businesses

4.18 – beauty/character/identity

4.10- Safety

3.22- socially engaging and inclusive

2.44- comfort

2.43-Environmentally sustainable

5. What elements do you feel are most important to you?

Please rank each of the following elements in the order of importance from 1 to 9. (1 is the most important and 9 is the least important):

Car parking received the most importance, with trees and planting a close second. Lighting, came in third, with seating/outdoor dining and pedestrian safety features very close after that.

6.62 Car Parking

6.26 Trees and planting

6.13 Lighting

5.68 seating/outdoor dining

5.61 pedestrian safety features

4.71 Art

4.03 Special Paving

3.63 Wayfinding Signage

2.72 Bike Parking

6. What do you like most about San Mateo Avenue?

Diversity and quality of stores and restaurants: Locally owned business, good food, good selection, west coast café, culturally diverse, tandy leather, restaurants, variety, interesting, diversity, kid specialty shops, eclectic, affordable food,

Pedestrian amenities: wide sidewalks, stop signs at every block, close to public transport, walking distance from my home, close by, proximity, walkability, concentration of businesses, human-scale, contrasts well with El Camino real's speed and car centric character, generally safe,

Charm/ unique character: quaint, architecture/character of older buildings, history of area, small town feel, character, hidden gem, not overly crowded, small town feel

Free parking, free time limited parking

Potential: essentially a blank slate, potential destination for the city

7. What general attributes would you most like to change about San Mateo Avenue?

Any additional comments?

Businesses that do not appeal to me/ need new and different businesses: strive for economic sustainability, need more than restaurants, need better businesses, some recognizable businesses, some big name businesses, look at Burlingame and Grand Ave – better businesses, businesses need to be updated, tanforan mall took away a lot of the business, now there are too many restaurants, need businesses that are not restaurants, city should work with new businesses to help them get started and give more support, city needs a new business program, need more successful businesses, too many empty businesses, often in disrepair, general facades/appearance need upgrade, and uniformity, update architecture, too many Chinese restaurants, need high caliber restaurants, do not require businesses to install expensive signage, require businesses to install nice signage, stores look cheap and junky, need better stores, need more mainstream businesses, more variety, more big name stores, like trader joes and jamba juice will help bring more people and help smaller businesses. More family friendly, better stores. A bookstore! No papered storefronts.

Run down: Dirty, embarrassing, Needs to be more attractive, bleak, look at Burlingame – needs to be more pleasing and inviting, look at Burlingame and Grand Ave - looks better., clean it up, it's an eyesore! Avenue is dirty, not aesthetically pleasing, look and feel needs to be updated, neglected, unpleasant, need to better hide garbage, grease clutter from businesses, get people to stop feeding birds behind Hon Lin, get rid of birds and bird

poop on sidewalks and parking lots, tear it all down and start over, add street lights or hanging baskets to encourage people to turn off el camino real, area looks depressed, more modern looking buildings, more conformity in look. Should be guidelines for storefront signage, paint color, need road repaving, looks old, outdated, dumpy, needs to look better, having streets cleaned, less vacancies, cleanliness, dilapidated businesses

Ped/bike safety: safer for moms with kids to walk, more ped friendly, bike lanes for Huntington and San Bruno Ave, more safety, keep bikes off sidewalks, not ped friendly (almost get hit by cars crossing the street), more walkable, crosswalk lights, bring public transport from hills

Parking is difficult: can't express this enough, need more parking, more parking is needed, need more free parking, need parking meters and a parking structure, code enforcement, not convenient to park, monthly parking structure or lot, find a way to make parking for 400 block easier, parking is a nightmare, parking can be challenging, parking parking, citizens have been asking about this for ages, charge for parking, Everyone knows more parking is needed. It's why so few people go downtown. Stop pretending it's not the main problem!! Better parking. Need to deal with double parking

Aesthetics/ Street furniture/outdoor activity: used to have produce outside across from starbucks but that went away, need more curb appeal, more outdoor dining, make it a street people would want to stroll on, more outdoor seating, add nice pavers, more art, plants, more places to sit, need fresh modern appearance, improved surfaces and textures, Finish centennial plaza, improve centennial plaza – could be a gem! More outdoor seating

Wayfinding/Signage: Should have gateway on Kains leading to downtown, (ppl would be coming from El Camino and Huntington), updated entry pt sign off camino real, signs directing cars to lots

Wider audience: would like to see downtown bring more visitors from all of the peninsula

Safety: people drive unsafely down the street, police station downtown, more lighting and safety, at night, seems unsafe and dark

Ecological/Planting- more trees, strive for ecological sustainability, need trees, planting, there's scarce greenery, taller more substantial trees, more/larger trees,

Activation/ Marketing: A Website page that describes the different restaurant's cuisines and perhaps weekly/monthly specials. It feels very unwelcoming to me as a new resident. 8. Create some events like Halloween trick or treating to bring families to the street. Or can you tie something to Celebrate Independence Day and have a street party? Or perhaps a "Back-to-School" shopping alternative to the mall. Perhaps you can create some special dining experiences for parents who visit some of the several businesses focused on children? OR maybe a special shopping event or experience for senior citizens. Most of them would likely remember life before Tanforan mall and might enjoy shopping along San Mateo Ave.



Summary of Planning Commission Comments - August 20, 2019 Meeting
Draft San Mateo Avenue Conceptual Streetscape Plan (Plan)

Summary:

On the August 20, 2019 regularly scheduled meeting, the Planning Commission accepted the report, received public testimonies and provided the following comments and questions to staff as they relate to the Draft San Mateo Avenue Conceptual Streetscape Plan. The motion was made by Commissioner Tom Hamilton and seconded by Commissioner Rick Biasotti. The Commission voted 6-0. The following summary includes 1) Staff's recommended modifications, 2) Planning Commission's inputs that have been addressed and 3) Comments that are not recommended by staff to be incorporated in the Plan.

1. Staff's Recommended Modifications:

- **Lighting:** The Plan proposes to install new efficient streetlights on San Mateo Avenue and hanging lights in the paseos. While the Commission agreed with having more lighting installed, they cautioned the feasibility and stability of the proposed hanging lights as San Bruno can be very windy at times. The Commission further suggested to create a visual flow in the downtown with installing new lights in the sidewalks, seating area and on trees. The Commission recommended to keep seasonal tree lighting permanent as it looks very majestic and inviting.

Staff explanation: Staff recommends adding specific requirement to exclude the use of hanging light fixtures.

2. Planning Commission's inputs that have been addressed:

- **Centennial Plaza Design:** The Plan recommends three layout options for the use of the Centennial Plaza, including a children's play area, an outdoor performance area, additional seating and landscaping. The Commission agreed that today's Centennial Plaza serves minimal community benefits. The Commission recommended to open the Centennial Plaza for more public benefits. The Commission recommended to have a performance space and a safe children's play area. However, the Commission did not reach a consensus of the final location inside the Centennial Plaza for a child's play area given its proximity to Florida Park, safety concerns related to the orientation of Jenevein Avenue or in front of the Citibank plaza. The Commission further discussed of adding a running water fountain, a flag pole (carrying both California and San Bruno flags) to host flag ceremony, a brick wall to screen the rear parking lot and the possibility of an actual building. The Commission recommended to have drought tolerant plants in the Centennial Plaza along with more bench or seating walls. The idea of having an off-street space for farmer's market with food trucks was also recommended.
- **Art and Aesthetics Expression:** The Commission recommended to have permeable pavement not only in the sidewalks but also in the parking strip; to have more color to the street as against only grey finishes; to use bike racks and pathways as another

opportunity for artistic expression; and to activate the use of paseos through local art and more lights. Commissioners also commented on the proposed bike rack design is too simple and plain. Elements such as whimsical, artistic and fun are encouraged when considering outdoor furnishing selection.

- **Tree Selection:** The Commission was interested in understanding how tree selection was made, and whether any San Bruno native tree types were considered. Staff explained that selection was based on the City Approved Tree list and the idea is to maintain a sense of unity since Chinese Elms were recently planted at the Aperture site. Other native tree types can be added to the Plan as accent tree so long such trees, at its full maturity, will not conflict with nearby streetlight, signage and buildings.

3. Comments not recommended by staff:

The following list includes comments and questions that were raised by Planning Commission but not addressed in the draft Plan. However, staff does not recommend pursuing addressing these comments at this time since such topics have not been fully evaluated and can substantially delay the Plan's final adoption schedule.

- **Transportation:** The Commission suggested to have a pick-up and drop-off loading zone for ride-sharing companies near the Caltrain Station on San Mateo Avenue.
- **Wayfinding and Signage:** The Plan recommended three options of the gateway signs at the north and south entrances of San Mateo Avenue. The Commission recommended to incorporate "Welcome to San Bruno" on the incoming side of the Gateway sign to welcome visitors and "Thank you" on the out bounding side.
- **Branding:** The Plan describes San Mateo Avenue as '*The Avenue*' since it has currently been advertised as such on flag posts in downtown. The Commission did not reach a consensus on whether San Mateo Avenue should be uniformly referred to as "*The Avenue*". However, ideas such as "International Way," "Downtown," or "Avenue of American Dreams" were suggested. The Commission suggested to have the future downtown or City branding to resonate with the statement "the City with a Heart".
- **Budget:** The Commission agreed that although past efforts have failed, it shouldn't discourage staff and the City to continue working towards beautifying the downtown. They appreciated the project and the proposed conceptual design. The Commission emphasized to make every effort to make this project a reality by finding the appropriate source of funding.
- **Parklet Program:** The Commission was interested in finding out if a parklet program exists and how it would work in San Bruno. Staff explains that City does not currently have a Parklet program in place.
- **Downtown Revitalization Effort:** The Commission thought that numerous efforts like the Plan have been launched in the past. However, staff explains that the draft Plan is the first effort in providing a comprehensive design guideline on streetscape improvements for the overall San Mateo Avenue area.

- **Wind study:** The Commission was interested in understanding if a wind study was prepared as part of the draft Plan. Staff explained that a wind study was not prepared.
- **Downtown Façade Program:** The Commission was interested in understanding if the City currently has a downtown façade program. Staff explained that there isn't such program in place currently.
- **Recology / Food Waste Collection:** The Commission asked how and where restaurants are currently disposing food waste including oil and compost and how garbage cans are located and picked up on San Mateo Avenue. Since Recology was not present at the Planning Commission hearing, staff responded that staff would follow up and report back.
- **Funding of the Streetscape Plan:** The Commission was interested in learning whether the implementation of Streetscape Plan can be funded through the Development Impact Fees (DIF). Staff explained that as identified in the San Bruno Development Impact Fee Nexus Study, the San Mateo Streetscape plan implementation was listed as part of the 'Transportation Improvements' and can be potentially funded through the DIF.
- **Historic Preservation Ordinance.** The Commission was interested in learning if the City currently has a Historic Site Preservation Ordinance. Staff explained that San Bruno currently does not have a Historic Preservation Ordinance in place.
- **Bike Lane:** The Commission asked if there will be a bike lane as part of the final design. Consultant explained that having a dedicated bike lane on San Mateo Avenue is not feasible without having to either reduce the sidewalk width or remove one travel lane. Since the scope of this project is to maintain the curb alignment as much as possible, a proposed *Sharrow* (a shared vehicle/bicycle lane) bike lane was proposed.
- **Bus Stop:** The Commission asked if Samtrans buses will stop in the travel lane. Consultant confirmed that the Plan does not include any dedicated bus pull out areas and buses will stop in the travel lane.
- **Sidewalk at 111 San Bruno Project:** The Commission was concerned whether the recent sidewalk installation for the Aperture project and soon-to-be constructed project at 111 San Bruno are consistent to the Plan and would not be re-installed in the future. Staff explained that since the draft Plan is yet to be adopted, sidewalk improvement for these projects were constructed pursuant to the City standards instead of the draft Plan.
- **Construction Cost Contingency:** The Commission asked if 35% construction cost contingency, as noted in the draft Plan, is within the industry standard. Consultant confirmed that the proposed 35% contingency is within the industry standard.